

City of Kelowna
Regular Council Meeting
AGENDA



Monday, May 12, 2014
1:30 pm
Council Chamber
City Hall, 1435 Water Street

Pages

1. Call to Order

This meeting is open to the public and all representations to Council form part of the public record. A live audio feed is being broadcast and recorded by CastaNet and a delayed broadcast is shown on Shaw Cable.

2. Confirmation of Minutes

4 - 5

Regular PM Meeting - May 5, 2014

3. Development Application Reports & Related Bylaws

3.1 Official Community Plan Bylaw Amendment Application No. OCP14-0005 & Rezoning Application No. Z14-0006 - 235 Queensway Avenue, 1470 & 1476 Water Street, Portions of Mill Street & Queensway Avenue, Westcorp Holyrood Inc.

6 - 96

To consider a proposal to change the Official Community Plan Future Land Use designation and to Rezone portions of the subject properties in order to permit the development of an hotel tower including ancillary commercial retail, conference centre, and restaurant facilities.

3.1.1 Bylaw No. 10962 (OCP14-0005) - 235 Queensway Avenue, 1470 & 1476 Water Street, Portions of Mill Street & Queensway Avenue, Westcorp Holyrood Inc.

97 - 98

Requires a majority of all members of Council (5).

To give Bylaw No. 10962 first reading in order to change the Land Use Designations of part of the properties from the Park - Major Park & Open Space Designation to the MXR - Mixed Use (Residential/Commerical) Designation.

3.1.2 Bylaw No. 10963 (Z14-0006) - 235 Queensway Avenue, 1470 & 1476 Water Street, Portions of Mill Street & Queensway Avenue

99 - 100

To give Bylaw No. 10963 first reading in order to rezone the subject properties from the C7 - Central Business Commercial and P3 - Parks and Open Space zones to the C7lp/rls - Central Business Commercial (liquor primary/retail liquor sales) zone.

- 3.2 Rezoning Application No. Z13-0016, 984 Dehart Road, Sherwood Mission Developments & Dr. A. Rezansoff 101 - 165

The applicant is proposing to rezone a portion of the land from the A1 - Agriculture 1 zone to the RU1 - Large Lot Housing zone to facilitate a future 55 lot residential subdivision. The proposal requires an OCP amendment to extend the Single/Two Unit Residential (S2RES) future land use designation north into the area designated for Multi-Unit Residential, Low Density (MRL), in exchange for area that was previously Single/Two Unit Residential for Multi-Unit Residential, Low Density (MRL).

- 3.2.1 Bylaw No. 10964 (OCP13-0011) - 984 Dehart Road, Sherwood Mission Developments Ltd. 166 - 167

Requires a majority of all members of Council (5).

To give Bylaw No. 10964 first reading in order to change portions of the Future Land Use Designation of the subject property from the Multi-Unit Residential Low Density designation to the Single/Two Unit Residential designation and from the Single/Two Unit Residential designation to the Multi-Unit Residential Low Density designation.

- 3.2.2 Bylaw No. 10965 (Z13-0016) - 984 Dehart Road, Sherwood Mission Developments Ltd. 168 - 169

To give Bylaw No. 10965 first reading in order to rezone the subject property from the A1 - Agriculture 1 zone to the RU1 - Large Lot Housing zone.

4. Non-Development Reports & Related Bylaws

- 4.1 Center of Gravity 2014 170 - 174

To provide Council with an update on the plans for the 2014 Center of Gravity Festival as requested by Council at the October 28, 2013 Council meeting.

- 4.2 2014 Outdoor Events 175 - 195

To provide Council with an update on the 2014 event schedule.

- 4.3 Vernon Creek Risk Assessment 196 - 211

The purpose of this report is to identify the current condition of the Vernon Creek flume and associated risks as directed by Council at the December 12, 2013 meeting, SR Request 278675.

- 4.4 Small Shop Promotion Days 212 - 214

To obtain approval from Council to provide free parking in the downtown area on two (2) dates in 2014.

4.5 Regional Growth Strategy 215 - 267

To provide the Regional District of Central Okanagan the City of Kelowna response to the referral of the Regional Growth Strategy Bylaw.

5. Bylaws for Adoption (Non-Development Related)

5.1 Bylaw No. 10950 - Five Year Financial Plan, 2014-2018 268 - 272

To adopt Bylaw No. 10950.

5.2 Bylaw No. 10952 - Tax Structure Bylaw, 2014 273 - 273

To adopt Bylaw No. 10952.

5.3 Bylaw No. 10948 - Annual Tax Rates Bylaw, 2014 274 - 276

To adopt Bylaw No. 10948.

5.4 Bylaw No. 10949 - DCC Reserve Fund Expenditure Bylaw, 2014 277 - 277

To adopt Bylaw No. 10949.

5.5 Bylaw No. 10951 - Sale of City-Owned Land Reserve Fund Expenditure Bylaw, 2014 278 - 278

To adopt Bylaw No. 10951.

6. Mayor and Councillor Items

7. Termination



City of Kelowna Regular Council Meeting Minutes

Date: Monday, May 5, 2014
Location: Knox Mountain Meeting Room (#4A)
 City Hall, 1435 Water Street

Council Members Present: Mayor Walter Gray and Councillors Colin Basran, Andre Blangleil, Maxine DeHart, Gail Given, Robert Hobson, Mohini Singh, Luke Stack and Gerry Zimmermann

Staff Present: City Manager, Ron Mattiussi; Deputy City Clerk, Karen Needham; Director, Real Estate, Derek Edstrom*; Manager, Subdivision, Agricultural & Environmental Services, Todd Cashin*; Manager, Real Estate, Jeff Hancock*; Manager, Property Management, John Saufferer*; and Council Services Coordinator, Sandi Horning

(* denotes partial attendance)

1. Call to Order

Mayor Gray called the meeting to order at 9:50 a.m. and noted that today is Kelowna 109th Birthday.

2. Confirmation of Minutes

Moved By Councillor Singh Seconded By Councillor Stack

R301/14/05/05 THAT the Minutes of the Regular AM Meeting of April 28, 2014 be confirmed as circulated.

Carried

3. Resolution Closing the Meeting to the Public

Moved By Councillor Hobson/Seconded By Councillor Zimmermann

R302/14/05/05 THAT this meeting be closed to the public pursuant to Section 90(1) (e) and (j) of the Community Charter for Council to deal with matters relating to the following:

- Acquisition, Disposition, or Expropriation, of Land or Improvements; and
- Third Party Information.

Carried

4. Adjourn to Closed Session

The meeting adjourned to a closed session at 9:51 a.m.

5. Reconvene Open Session

The meeting reconvened to an open session at 11:03 a.m.

6. Issues Arising from Correspondence & Community Concerns

6.1. Mayor Gray, re: Issues Arising from Correspondence

Mayor Gray:

- Advised that he did not have anything specific to raise with Council.

7. Termination

The meeting was declared terminated at 11:03 a.m.

Mayor

/slh



Deputy City Clerk

DRAFT

REPORT TO COUNCIL



Date: 4/28/2014
RIM No. 1250-30
To: City Manager
From: Urban Planning, Community Planning & Real Estate (JM)
Application: OCP14-0005 / Z14-0006 **Owner:** Westcorp Holyrood Inc
Inc. No. A31452
Address: 235 Queensway Avenue, 1470 & 1476 Water Street, Portions of Mill Street and Queensway Avenue **Applicant:** Westcorp Development
Subject: Official Community Plan Amendment and Rezoning Applications
Existing OCP Designation: MXR - Mixed Use (Residential/Commercial)
PARK - Major Park & Open Space
Proposed OCP Designation: MXR - Mixed Use (Residential/Commercial)
P3 - Parks and Open Space
Existing Zone: C7 - Central Business Commercial
C7lp/rls - Central Business Commercial (Liquor Primary/
Retail Liquor Sales)
Proposed Zone: C7lp/rls - Central Business Commercial (Liquor Primary/
Retail Liquor Sales)

1.0 Recommendation

THAT Official Community Plan Bylaw Amendment No. OCP 14-0005 to amend Map 4.1 of the Kelowna 2030 - Official Community Plan Bylaw No. 10500, by changing the Future Land Use designation of:

1. Part of the Mill Street Road Right-of-Way, being approximately 596m² in area, located adjacent to 235 Queensway Avenue and 262 Bernard Avenue, Kelowna, BC
2. Part of the Queensway Avenue Road Right-of-Way, being approximately 168m² in area, located adjacent to 235 Queensway Avenue and 1470 Water Street, Kelowna, BC

from the PARK - Major Park & Open Space designation to the MXR - Mixed Use (Residential/Commercial) designation, as shown on Map "A" attached to the Report of Land Use Management Department dated April 28, 2014, be considered by Council;

AND THAT Council considers the Public Information Session public process to be appropriate consultation for the purpose of Section 879 of the *Local Government Act*, as outlined in the Report of the Land Use Management Department dated April 28, 2014;

AND THAT Rezoning Application No. Z14-0006 to amend City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of:

1. Lot 1, District Lot 139, ODYD, Plan 2698, located at 1470 Water Street, Kelowna, BC
2. Lot 2, District Lot 139, ODYD, Plan 2698, located at 1476 Water Street, Kelowna, BC
3. Part of the Mill Street Road Right-of-Way, being approximately 596m² in area, located adjacent to 235 Queensway Avenue and 262 Bernard Avenue, Kelowna, BC
4. Part of the Queensway Avenue Road Right-of-Way, being approximately 168m² in area, located adjacent to 235 Queensway Avenue and 1470 Water Street, Kelowna, BC

from the C7 - Central Business Commercial and P3 - Parks and Open Space zones to the C7lp/rls - Central Business Commercial (liquor primary / retail liquor sales) zone, as shown on Map "B" attached to the Report of the Land Use Management Department dated April 28, 2014, be considered by Council;

AND THAT the Official Community Plan Bylaw Amendment Bylaw and the Zone Amending Bylaw be forwarded to a Public Hearing for further consideration;

AND THAT final adoption of the Zone Amending Bylaw be considered in conjunction with Council's consideration of a Development Permit and Development Variance Permit for the subject property;

AND THAT final adoption of the Zone Amending Bylaw be considered subsequent to the submission of a plan of subdivision consolidating the subject properties and closed road into one title;

AND THAT final adoption of the Zone Amending Bylaw be considered subsequent to the requirements of the Development Engineering Branch and the Ministry of Transportation and Infrastructure being completed to their satisfaction;

AND THAT final adoption of the Zone Amending Bylaw be considered subsequent to the applicant entering into an encroachment agreement with the City to accommodate canopies, and a land agreement to accommodate the kaleidoscope projection, as described in the report from the Urban Planning Department, dated April 28, 2014;

AND FURTHER THAT final adoption of the Zone Amending Bylaw be considered subsequent to the conclusion of the road closure process, as described in the report from the Urban Planning Department, dated April 28, 2014.

2.0 Purpose

To consider a proposal to change the Official Community Plan Future Land Use designation and to Rezone portions of the subject properties in order to permit the development of an hotel tower including ancillary commercial retail, conference centre, and restaurant facilities.

3.0 Urban Planning Department

Urban Planning staff are supportive of the proposed Rezoning and OCP amendment application to facilitate the development of a signature hotel tower and supporting uses on the subject property.

Both the OCP and the Downtown Plan support tower development on the site as part of a broader effort to encourage densification and increased activity in Kelowna's downtown. The policy direction provided in these documents supports development of up to 19 storeys in the form of a signature building with a high quality of design.

The applicant team has come forward with a very strong proposal that meets the needs of the proponent, while responding creatively and thoroughly to the site's contextual challenges. The result is an elliptical 24 storey tower containing 214 hotel units set within a five storey podium that contains vehicle parking wrapped by at-grade retail, a conference centre, and rooftop amenity areas.

While each face of the podium responds to its own unique context, it is the development's direct interface with Kerry Park that receives the greatest attention. Its massing steps down from 5 storeys to 1 storey with tiered outdoor patios focused on a rotunda feature intended to function as a "lantern" at night on the waterfront. Uses at grade invite a high level of pedestrian activity, which will create an engaging environment for patrons of the development and for park users.

The impacts of the development on Kerry and Stuart Parks will be positive, and will support the objectives of the OCP and Downtown Plan. The proposal is built to the westernmost lot line, therefore construction activity will have temporary construction impacts on portions of Kerry Park. Also, the shadowing from the proposed development has an effect on the adjacent parkland. The impact of the shadowing is worsened by the additional five (5) storeys of height requested by the applicant. Staff is working with the applicant team to provide mitigation for these impacts through improvements to Kerry Park in accordance with the City's Kerry Park Concept Plan. Negotiations continue with staff with the aim of coming to agreement prior to Council consideration of the Development Variance Permit.

As noted above, the height of the tower exceeds what was previously considered for the site by 5 storeys, for a total of 24 storeys. In addition, three other very minor variances are requested by the applicant. Staff are not opposed to these requests; they will be considered together and in greater detail should Council support the land use.

4.0 Proposal

4.1 Background

In 2009 Council tasked staff with the development of a plan to guide the development of the downtown into the future. Working with diverse interest groups over several years, the Downtown Plan was adopted by Council on February 27, 2012. The overall vision of the plan is a downtown that "will include areas where citizens choose to live, shop, play and congregate and where businesses choose to do business and where developers choose to develop". To achieve this, the focal points of the plan are:

1. Making downtown more pedestrian friendly
2. Increasing activity downtown
3. Increasing the number of people living downtown
4. Reducing crime/increasing safety
5. Making it easier to park

Within the context of its broader goals for the success of Kelowna's downtown, the plan specifically contemplates building height. The Downtown Building Heights Map provides general maximum height guidance on a lot-by-lot basis, and expresses a form of development that generally increases height as buildings move away from the lake. On this map, the subject properties are noted as suitable for a maximum of 19 storeys in height. The policy direction from this plan has been transferred into the Official Community Plan and has been refined there. The subject site is specifically discussed in the OCP, which provides the following as a guideline to be used in the consideration of future development on the site:

“The former Willow Inn site at the corner of Queensway Avenue and Mill Street is significant given the site’s proximity to the waterfront and its high visibility. In order to achieve approval for up to 19 storeys on this site, any proposed development should be required to demonstrate that it gives careful consideration to view impacts from other parts of downtown, is a signature landmark building and that it meets a high standard of design excellence regarding aesthetics and building performance.”

Using this direction, the applicant has engaged a series of architectural and landscape consultants to design the development to a quality that reflects the site’s prominence. Together, the applicant team has been very pro-active in working with City staff to ensure that issues are identified and requirements addressed as early as possible.

As a component of their development, the applicant has requested that a portion of Queensway Avenue and the majority of Mill Street be closed and consolidated with their site (see attached Road Closure Plan). Should Council support the land use applications, staff will bring forward the road closure request for Council consideration. According to the applicant, the additional roadway lands are needed for the site in order to accommodate parking, which cannot be provided below grade due to the high water table. The developer has proposed to compensate the City for the land at market value.

Due to a technical provision of the Zoning Bylaw, roads take on an equal share of the zoning of abutting land uses. In this instance, half of the Mill Street road right-of-way is zoned for commercial use, while the other half is zoned for park use. The same logic applies to the OCP Future Land Use designation, with half of the road right-of-way being designated for mixed use, and the other half for park. As a result of this, the proponent must apply to rezone and to amend the OCP for half of the Mill Street road right-of-way and a small portion of the Queensway Avenue road right-of-way such that the entire development site is zoned C7lp/rls - Central Business Commercial (Liquor Primary/Retail Liquor Sales) and designated MXR - Mixed Use (Residential/Commercial) in the OCP (see attached Maps ‘A’ and ‘B’).

The development will interface directly with Kerry Park, which has recently undergone a concept site planning exercise with City staff and a landscape consultant. The Kerry Park Concept Plan was completed in September of 2013. However, no funding for its implementation has been included in the 2020 Capital Plan (refer to Section 3.0 discussion).

In accordance with Council Policy No. 367, the applicant has completed a Public Information Session (see attached Community Consultation Summary). The session was held on Wednesday, March 12, 2014 from 3pm - 7pm at the Rotary Centre for the Arts. In preparation for the session, the applicant took out daily ads in local newspapers for two weeks in advance, and went door-to-door in the downtown area providing flyers to businesses. Three large format development signs were also posted on the subject property, each of which identified the date, location and time for the information session. The applicant also maintains a website for the project.

Over the course of the information session, the applicant recorded the attendance of 188 members of the public. It is thought that this is an underestimation of attendance, as not all attendees signed in. Attendees were provided with a clipboard that held a survey for them to complete. 129 surveys were returned. Of the surveys returned, the responses were overwhelmingly positive, with only 13 negative responses submitted. Responses noting support for the project were principally focused on the form and character of the proposed development, and on the increased level of activity that the project would bring. The responses submitted not in support noted concerns about height, proximity to lake, and whether the development is accessible for all socio-economic groups in Kelowna.

Finally, the proposal was reviewed by the City's staff-led Advisory Design Team at its meeting of March 13, 2014. Overall, the feedback from staff on the design of the proposal was overwhelmingly positive, and no significant changes were suggested.

4.2 Project Description

The applicant is proposing to develop a signature hotel tower on the subject property, including ancillary conference centre, commercial retail, and restaurant uses. Both the tower and podium components of the development are critical for different reasons, and each is discussed in detail separately below. This is followed by an analysis of the impacts of the development and the request for a road closure.

The Tower:

The design of the development has been driven by several key factors, of which its context is one of the most critical. Within the larger area of downtown Kelowna, from Clement Avenue in the north to Harvey Avenue to the south, the skyline of Kelowna features a number of towers, both existing and approved but not constructed. At the north side of downtown, the Waterscapes/Sky tower is the tallest existing tower at 26 storeys in height. The Discovery Point, Dolphins, and Grand Hotel towers are adjacent to this at 26, 17, and 11 storeys respectively. Within the central area of the downtown, tower height is somewhat lower, with the Downtown Lofts at 11 storeys, and the Madison at 15 storeys. A notable exception to this is the recently approved Monaco towers, which are designed at 18 and 26 storeys. Moving further south, an approved development on the 400 block of Bernard Avenue is 26 storeys in height.

The proposed tower is 24 storeys in height, and contains of 214 rooms. As viewed from a distance, the proposed tower would be generally in keeping with the height of towers existing or approved, and would act as a bookend of sorts, anchoring Kelowna's downtown. This proposal does exceed, by 5 storeys, the maximum height of development considered for the site in the OCP and the Downtown Plan. In support of this request, the applicant noted that "the additional 5 storeys were needed in order to reach the number of hotel rooms required to make the hotel efficient and economically viable as a project."

The physical design of the tower is also crucial to the success of the development. Both the Downtown Plan and the OCP encourage the development of slender tower forms, as opposed to lower, bulkier buildings, with the aim of protecting privacy, reducing visual impact and shadowing. The applicant team has explored a variety of building forms in an effort to meet this direction. The team has arrived at a tower design whose central form is an ellipse, which allows the tower to be oriented such that it presents a very narrow silhouette when viewed from the east (downtown) or west (lake). The north and south elevations of the tower are accented by a staggered pattern of balconies, which also assist in breaking down the monolithic form of the ellipse.

The tower will contain the vast majority of hotel units. Nearer the base, the uses of the tower begins to shift towards more general hotel uses, including the lobby, hospitality suite, fitness centre and spa, and hotel offices. The core of the tower consists of a staircase and three elevators. Rooms surround this central core, with a range of between 2-13 hotel units per floor.

A white composite panelling and glazing will form the principal materials used on the tower. Major engineered wood elements will accent the tower at its top and base. Balconies on the tower will feature coloured glass privacy screens, and the underside of the balconies will be finished in wood.

The Podium:

The hotel tower is set within a 5 storey podium that contains a core of vehicle parking wrapped by commercial retail and conference uses. When viewed at the finer scale of the blocks immediately surrounding the site, the impact of the podium component of the development is more significant. While the tower is most impactful when viewed from a distance and its footprint only occupies a portion of the site, the podium covers the entire site. Indeed, it is the podium that meets adjacent public space directly, and it is the portion of the building with which members of the public will interact most directly. With this in mind, the podium must unify and enhance the urban fabric of its immediate vicinity.

The massing of existing buildings in the general vicinity of the subject site is typically in the range of 1-6 storeys. Buildings along Bernard Avenue in particular are of a limited height, reflecting their heritage character. Development in these areas presents a consistent street wall, with most buildings having no setback from property lines. In some cases, and particularly along Bernard Avenue, storefronts are relatively narrow, and often feature residential or office uses on upper floors. Moving north of Bernard, buildings tend to be less narrow, single-use structures more suited to office uses and reflective of later phases of downtown development.

The west side of the proposed building interacts directly with Kerry Park. Where there is presently a road (Mill Street) separating the site from Kerry Park, the proposal aims to bring the podium of the development to meet the eastern limit of Kerry Park. As the “keystone” to the downtown waterfront, Kerry Park is very well used by residents and visitors and is home to regular festivals and activities in the summertime. The park contains “the Sails” and “Ogopogo” public art pieces, and also provides the only access/egress for the new Downtown Marina. While each face of the podium is important, it is perhaps the interface of the development with Kerry Park - with its heightened level of activity - that is the most crucial.

Both the form and finishing of the podium structure respond well to the varied needs of their surroundings. This challenge is rendered all the more difficult, as the podium must also provide all the parking necessary for the hotel and associated uses. To achieve this, five levels of parking are wrapped by a commercial retail, restaurant, coffee shop, office, conference centre and hotel uses.

The podium interface with Kerry Park (west elevation) receives the greatest level of attention, using curving forms that “reach out” to the park and that gradually decrease in height from 5 to one storey. The ground floor is highly active, featuring a coffee shop to the south, and restaurant, rotunda and bar moving along the building face to the north. Rooftops along this face are also vibrant spaces, providing outdoor rooftop deck areas for the various hotel uses in the upper levels of the podium.

Moving on to the Queensway Avenue face of the building (north elevation), the podium transitions from an activated pedestrian space, to a design that responds to more of the functional needs of the hotel, including the main entry/lobby, vehicle entry and exit points, and the entry for the parkade. Despite these more functional needs, the architecture of the building maintains a human scale, and echoes the curvilinear designs from the west elevation. The tower meets the ground at northwest corner of the building, which contains the main entry and lobby for the hotel. This is accented by a large porte-cochère/entrance canopy that uses engineered wood to reflect the shapes of the Downtown Marina buildings and the light standards on Bernard. Above the main floor, hotel and conference centre uses look north onto Stuart Park with outdoor patios providing increased animation and building articulation.

The east face of the podium building (east elevation) changes yet again and aims to bring the small scale retail environment from Bernard Avenue onto Water Street. Multiple commercial

retail units front directly onto Water Street at grade, with apartment/hotel units located above. The conference centre on the upper level completes this building face with an interesting architectural feature that is called the “kaleidoscope” by the applicant. The kaleidoscope is a small portion of the building enclosed in glass that projects from the building face at the conference level over the sidewalk. This is a small, but unique space that allows hotel users to interact with passersby downtown in a unique way.

The south elevation of the building is the most utilitarian, as it faces directly onto the lane, and is generally not visible from most vantage points. This side of the building contains service access to the back of house operations of the hotel. The internal parkade structure faces onto the lane, so the applicant team has proposed using long, rectangular planting boxes to soften the building face and to provide visual interest.

Overall, the scale of the podium fits well within its immediate context, providing critical building functions in a manner that respects its surroundings. While unified in an overall design concept, each building face adapts to its setting, whether that is a single storey coffee shop spilling onto Kerry Park, or a compact retail unit facing Water Street. As a contrast to the tower, the materials palette of the podium uses elements, such as engineered wood, wood-like panels, and stone, that serve to “ground” the development. These heavier grounding elements are mixed with large glazed surfaces that promote transparency and enhance surrounding public environments.

Development Impacts:

The subject property is a prominent site near the waterfront of the downtown core. The site is presently underdeveloped, containing a parking lot and a single storey commercial building fronting Water Street. The proposed development represents a major, positive advancement from this use. Notwithstanding the fact that a tower development was anticipated for the site by both the Downtown Plan and the OCP, the proposal will have impacts on Kelowna from multiple fronts. As noted previously, the Downtown Plan outlines some of the impacts that tower development can have and suggests issues that should be thoroughly examined when considering tower development, including: contextual fit into the surrounding urban fabric, view impacts, shadowing of the public realm, overlook and privacy impacts on neighbouring buildings, impacts on the overall skyline, distance between adjacent tall buildings, and impacts on adjacent or nearby heritage structures. Each of these is addressed below.

Contextual fit and skyline: Both in its tower and podium designs, the proposal makes a significant positive effort to respond to its varied context, whether that be through the introduction of at-grade retail fronting Water Street, or through the use of materials that reflect the Okanagan landscape. The tower also fits well within the Kelowna skyline, as indicated previously.

View impacts: Reducing the view impact of the proposal was one of the driving factors behind the elliptical shape of the tower. This shape presents its narrowest face along the east-west axis, which preserves view corridors both to and from the lake.

Shadowing: Even though the tower is not the component of the building most directly experienced by pedestrians, it does have an impact on the pedestrian realm through shadowing. Shadowing is an important consideration in the design and placement of towers, as the cumulative impact of tower development can mean that the streets and public spaces below have limited sun exposure. The applicant has conducted a shadow analysis to identify the impact of the building at varying times of day over different seasons. According to this analysis, the shadow impact of the development will be directed mostly towards the Queensway jetty and the southern portions of Stuart Park.

Overlook and privacy: The only interface challenge presented by the development is where the proposed building interfaces with the existing Kelly O'Bryan's restaurant. In this case, the applicant team worked with the ownership of Kelly O'Bryan's in order to guarantee the views and privacy of Kelly O'Bryan's patrons. To achieve this, the proposal steps the building down to a single storey. Staff do not anticipate any privacy concerns for any other nearby buildings.

Building separation: The aim of this criterion is to ensure that tall buildings are separated and staggered such that they do not form a "wall", eliminating view corridors and severely shadowing the public realm. In this case, this is the first tower in the vicinity, so the criterion will likely be more applicable as additional towers are developed in the future in accordance with the Downtown Plan.

Heritage Impact: Bernard Avenue contains a number of significant heritage buildings; however, none of these is directly impacted by the proposal.

Moving beyond the above areas of interest, one of the most significant areas of change is the interface of the building with Kerry Park. Where Mill Street presently divides commercial development from the park, the proposal will see the closure of the majority of Mill Street, bringing active uses into direct contact with Kerry Park. Ensuring that the development interfaces successfully with Kerry Park is an important challenge for the project. The approach taken by the applicant has been to provide a very high level of activity fronting the park, "reaching out" into the park to blur the line between public and private, and indoor and outdoor space. As a result, both the look and feel of the park will change. It will become a more active, vibrant space with residents and visitors migrating to and from the hotel and park-front commercial uses. Several tiers of rooftop decks will overlook the park.

In addition to visual and park interface changes, the applicant has provided a study that suggests that the project will provide significant and ongoing economic benefit to the city (see attached Economic Impact Analysis). According to the report, the construction phase of development will result in a total of 721 jobs created, and a total investment of \$112.2 million. In terms of the ongoing operating benefits of the hotel, it is expected to 299 total jobs and \$17.7 million annually.

The applicant has also provided a Transportation Impact Analysis (TIA), which evaluates the impact of the proposal (including the closure of Mill Street) on the City's transportation network. Even with the closure of Mill Street, downtown intersections are predicted to operate within capacity. It should be noted that the closure of Mill Street will result in the loss of 22 parking stalls. The applicant has committed to securing an equal number of stalls within the development for public parking use.

4.3 Site Context

The subject site consists of five lots, as well as an area of road to be closed, totaling approximately 4,448 m² of land area. The site is located in the Waterfront District of Kelowna's downtown, between Water and Mill Streets and Bernard and Queensway Avenues. The site is bounded on the south by a service lane.

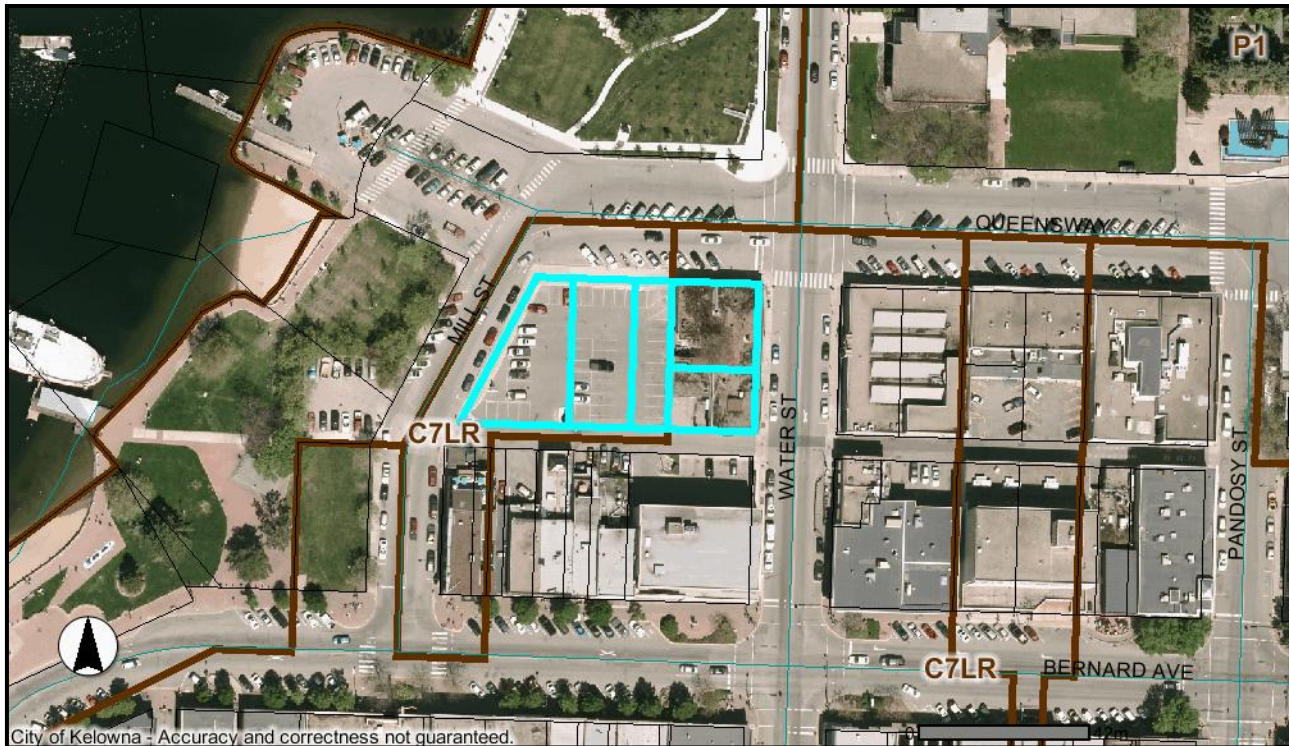
Presently, the site contains an at-grade parking lot and a single storey commercial building fronting Water Street. However, the site has a rich history dating back to 1928, when the original Willow Inn Hotel was opened. The hotel remained a focal point for waterfront activity downtown for decades to come. After years of decline, the hotel was finally demolished in 2008/2009.

The site contains multiple zones, with the majority being C7 or C7lp/rls. Only a small portion of the road right-of-way is zoned P3. Consistent with this, the OCP designation for the majority of the site is MXR, with only a small portion of road right-of-way being designated PARK.

Specifically, adjacent land uses are as follows:

Orientation	Zoning	Land Use
North	P3 - Parks and Open Space P1 - Major Institutional	Stuart Park City Hall
East	C7 - Central Business Commercial	BC Tree Fruits Queensway Transit Terminal Sturgeon Hall, Bordello's
South	C7 - Central Business Commercial C7lp/r/s - Central Business Commercial (Liquor Primary/Retail Liquor Sales)	Bank of Montreal Bernard Ave retail & food primary (various)
West	P3 - Parks and Open Space	Kerry Park

Subject Property Map: Downtown Hotel Proposal



4.4 Zoning Analysis Table

Zoning Analysis Table		
CRITERIA	C7lp/r/s ZONE REQUIREMENTS	PROPOSAL
Existing Lot/Subdivision Regulations		
Lot Area	200 m ²	4,476 m ²
Lot Width	6.0 m	94.51 m
Lot Depth	30.0 m	78.38 m
Development Regulations		
Total Units	-	214

Floor Area Ratio	9.0	7.3
Height	22.0 m	24 storeys / 83.995 m ^①
Front Yard	0.0 m	0.0 m
Side Yard (west)	0.0 m	0.0 m
Side Yard (east)	0.0 m	0.0 m
Rear Yard	0.0 m	0.0 m
Setback above 15.0 m: - Abutting a street	3.0	0.0 m* ^②
Setback above 22.0 m: - From an internal lot line - From a lane	15.0 m 10.0 m	Exceeds 0.0 m (for 675mm)* ^③
Other Regulations		
Minimum Parking Requirements	250 stalls	242 stalls + 8 cash-in-lieu
Bicycle Parking	13 Class I 14 Class II	17 Class I 18 Class II
Private Open Space	555 m ²	4,365.76 m ²
Loading Space	8 spaces	3 spaces ^④
<p>① Indicates a requested variance to the maximum height of buildings and structures from 22.0m permitted to 83.995m proposed.</p> <p>② Indicates a requested variance to the minimum building setback above 15.0m abutting a street from 3.0m required to 0.0m proposed. This variance is triggered by 8 minor encroachments into the required setback. These are illustrated in the attached Variance Analysis.</p> <p>③ Indicated a requested variance to the minimum building setback from a lane for portions of a building over 22.0 m in height from 10.0 m permitted to 0.0 m proposed. This variance is triggered by a portion of the parkade structure that encroaches into the setback by a total of 675mm. This is illustrated in the attached Variance Analysis.</p>		

5.0 Current Development Policies

5.1 Kelowna Official Community Plan (OCP)

Development Process

Compact Urban Form.¹ Develop a compact urban form that maximizes the use of existing infrastructure and contributes to energy efficient settlement patterns. This will be done by increasing densities (approximately 75 - 100 people and/or jobs located within a 400 metre walking distance of transit stops is required to support the level of transit service) through development, conversion, and re-development within Urban Centres (see Map 5.3) in particular and existing areas as per the provisions of the Generalized Future Land Use Map 4.1.

Ensure appropriate and context sensitive built form.²

Public Space.³ Integrate safe, high-quality, human-scaled, multiuse public spaces, such as parks, plazas and squares, as part of development or redevelopment within Urban and Village Centres.

Ensure that Urban Centres develop as vibrant commercial nodes.⁴

Encourage uses and commercial ventures that promote local tourism.⁵

Visitor Accommodation.⁶ Consider allowing visitor accommodation along the shore zone provided that such a use protects the riparian area, would be compatible with the neighbourhood and site context, and public enjoyment

¹ City of Kelowna Official Community Plan, Policy 5.3.2 (Development Process).

² City of Kelowna Official Community Plan, Objective 5.5 (Development Process).

³ City of Kelowna Official Community Plan, Policy 5.8.1 (Development Process).

⁴ City of Kelowna Official Community Plan, Objective 5.25 (Development Process).

⁵ City of Kelowna Official Community Plan, Objective 5.26 (Development Process).

Sustainable Prosperity.⁷ Assign priority to supporting the retention, enhancement and expansion of existing businesses and post secondary institutions and the attraction of new businesses and investment identified as bringing sustainable prosperity to Kelowna.

5.2 Kelowna Downtown Plan

Building Heights.⁸ Allow building heights to reach a maximum of the heights noted on Map 3 (unless existing zoning provides for greater heights). To achieve those heights, Council may consider variances from the heights set out in the Zoning Bylaw, provided that the additional height (beyond that provided in the Zoning Bylaw) results in the creation of affordable housing or yields other significant community benefits and is appropriate from the perspective of the following considerations:

- Contextual fit into the surrounding urban fabric
- Shadowing of the public realm
- View impacts
- Overlook and privacy impacts on neighbouring buildings
- Impacts on the overall skyline
- Distance between adjacent tall buildings
- Impacts on adjacent or nearby heritage structures
- Building form and massing to mitigate negative impacts of tall building.

Tower Separation.⁹ Require, where height variances are sought, that a minimum separation distance of 36.5 m (120 ft.) be provided between adjacent towers where there are floor plates larger than 697 sq. m (7,500 sq. ft.) and a minimum separation distance of 30.5 m (100 ft) will be sought between towers where floor plates are less than 697 sq. m. (7500 sq. ft.).

Tall Buildings/Block.¹⁰ Limit the number of taller buildings on any long blocks (typ. 270m long) to 5 towers per block.

Willow Inn Site.¹¹ Allow up to 19 storeys on the former Willow Inn site at the corner of Queensway Avenue and Mill Street upon demonstration that the proposed development gives careful consideration to view impacts from other parts of downtown, is a signature landmark building and that it meets a high standard of design excellence.

Retail Corridors.¹² Encourage retail uses to locate on designated Retail Corridors as noted in red on Map 5. Encourage a second strong retail anchor at the east end of Bernard Avenue to complement Safeway and to support Bernard Avenue as the primary Downtown retail corridor. Discourage new financial institutions and offices on the *ground* floor of buildings within the retail area show on Map 5 (ATMs are not included in this policy). Financial institutions and offices are better situated as ground floor uses outside of the retail area or on floors above-grade within the retail area.

6.0 Technical Comments

6.1 Building & Permitting Department

⁶ City of Kelowna Official Community Plan, Policy 5.26.2 (Development Process).

⁷ City of Kelowna Official Community Plan, Policy 8.1.1 (Economic Development).

⁸ City of Kelowna Downtown Plan, Policy 12.

⁹ City of Kelowna Downtown Plan, Policy 13.

¹⁰ City of Kelowna Downtown Plan, Policy 14.

¹¹ City of Kelowna Downtown Plan, Policy 15.

¹² City of Kelowna Downtown Plan, Policy 30.

Comments to be provided during Development Permit process.

6.2 Development Engineering Department

See attached Development Engineering Memoranda, dated April 28, 2014.

6.3 Bylaw Services

No comments received.

6.4 Fire Department

See attached letter, dated February 21, 2014.

6.5 Interior Health Authority

See attached letter, dated March 17, 2014.

6.6 FortisBC (electric)

Due to the size of the development there is potential for offsite upgrade requirements. Further, previous comments provided with respect to the proposed road closure area remain accurate (email forwarded below). Again, there are significant underground primary facilities servicing Kerry Park and various other properties in the area, including several owned by the City of Kelowna which will have to be moved to accommodate this development. The applicant is responsible for costs associated with any change to the existing service as well as the provision of appropriate land rights where required.

In the interim, FortisBC Inc. (electric) requests appropriate land rights to protect those facilities and services to neighbouring properties affected by the proposed development.

It should be noted that additional land rights issues may arise from the design process but can be dealt with at that time, prior to construction.

6.7 Telus

TELUS will provide underground facilities to this development. Developer will be required to supply and install conduit as per TELUS policy.

6.8 Shaw

No Concern.

6.9 Ministry of Transportation

Approval from the Ministry of Transportation was received on April 24, 2014.

6.10 RCMP

No Concerns.

7.0 Application Chronology

Date of Application Received: February 17, 2014

Advisory Design Team: March 13, 2014

Public Notification & Consultation: Public Information Session held on March 12, 2014

Report prepared by:

James Moore, Planner II

Reviewed by: Ryan Smith, Urban Planning Manager

Approved for Inclusion: D. Gilchrist, Div. Dir. Of Community Planning & Real Estate

Attachments:

Subject Property Map

Map A - Proposed OCP

Map B - Proposed Zoning

Project Rationale

Road Closure Plan

Site Plan

Conceptual Elevations

Landscape Plan

Context/Site Photos

Renderings

Shadow Analysis

Variance Analysis

Economic Impact Analysis, prepared by Grant Thornton LLP, dated January 30, 2014

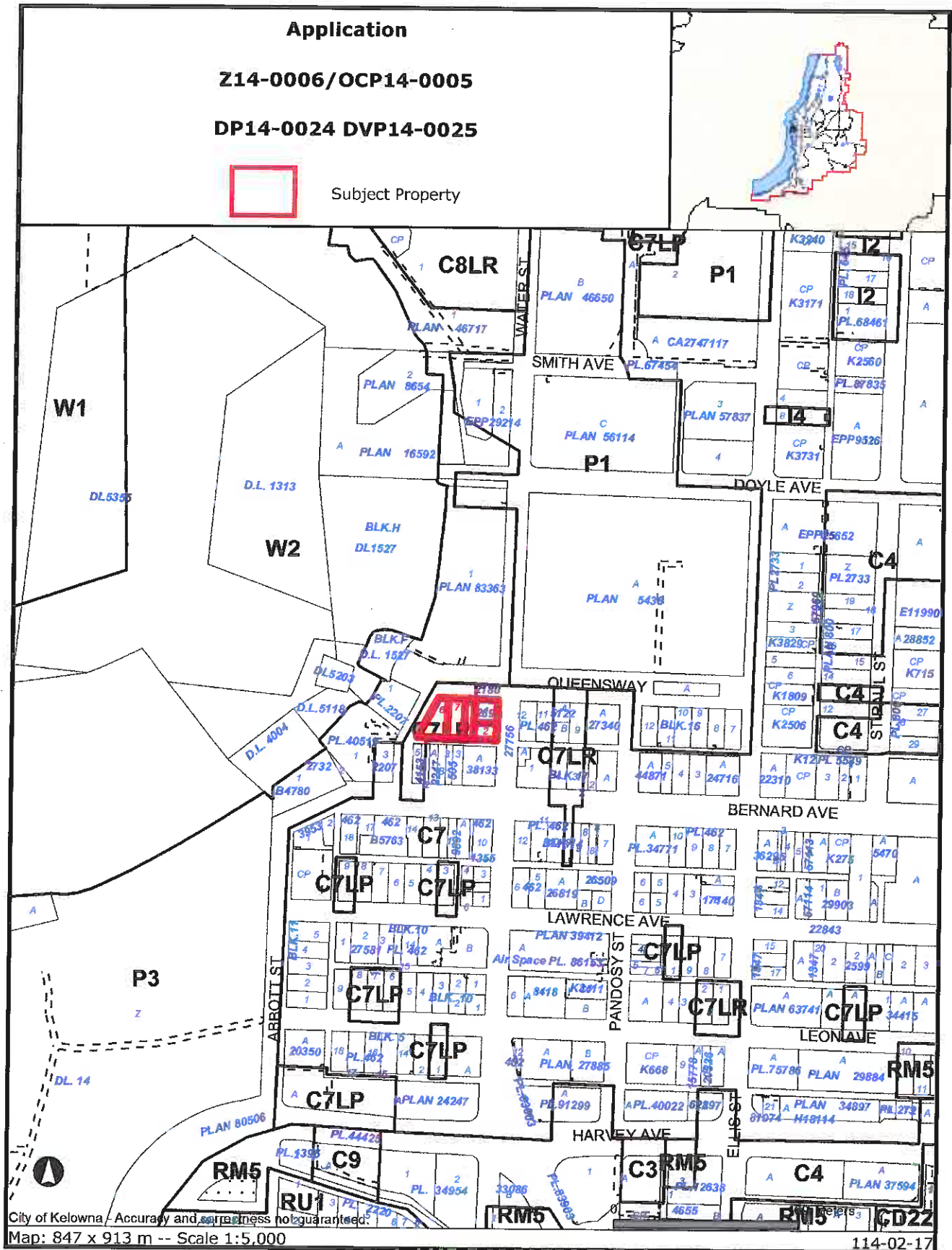
DRAFT - Transportation Impact Analysis

Community Consultation Summary

Development Engineering Memoranda, dated April 28, 2014

Letter from the Interior Health Authority, dated March 17, 2014

Letter from the Kelowna Fire Department, dated February 21, 2014

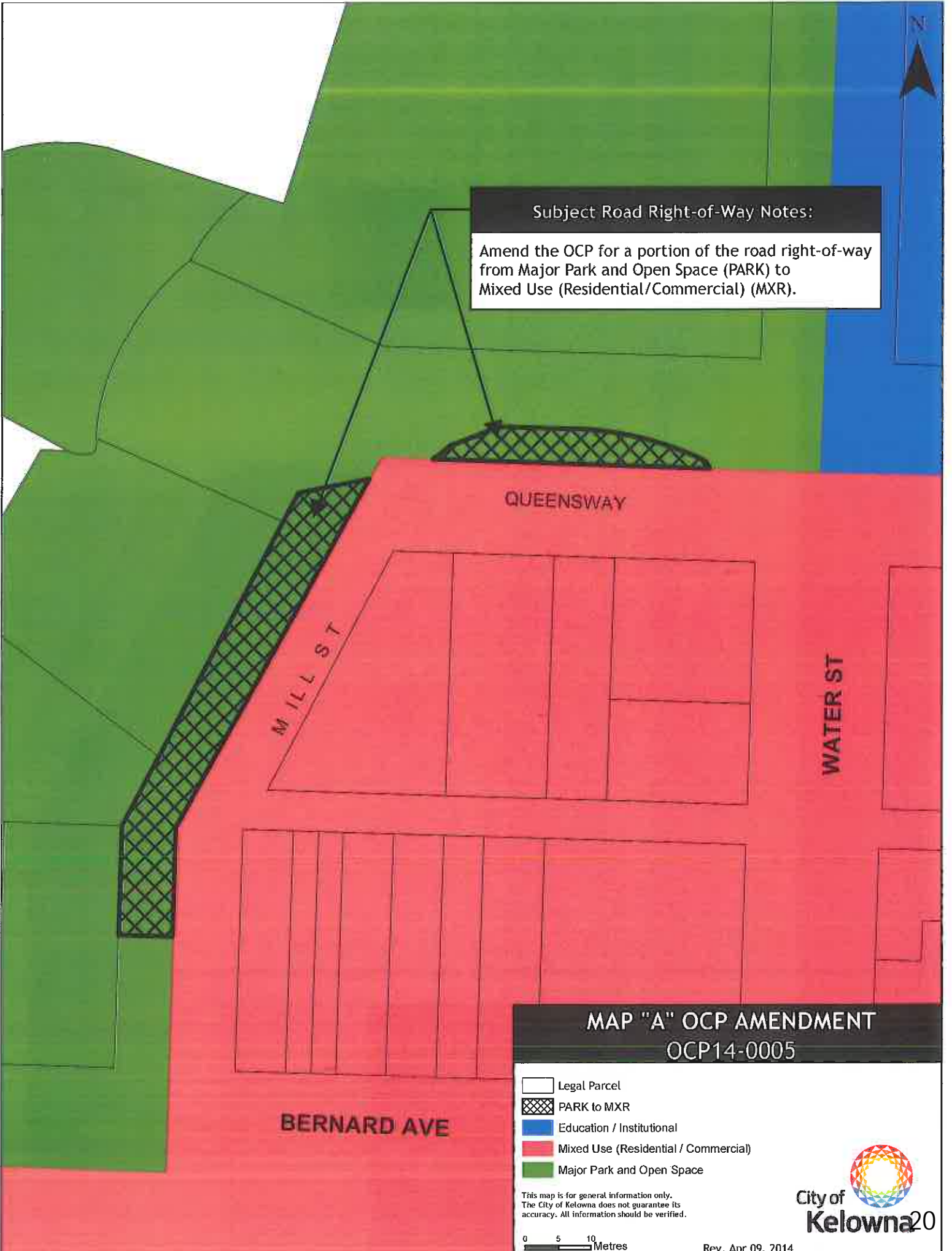


Certain layers such as lots, zoning and dp areas are updated bi-weekly. This map is for general information only. The City of Kelowna does not guarantee its accuracy. All information should be verified.



Subject Road Right-of-Way Notes:

Amend the OCP for a portion of the road right-of-way from Major Park and Open Space (PARK) to Mixed Use (Residential/Commercial) (MXR).








QUEENSWAY

MILL ST

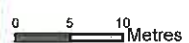
WATER ST

BERNARD AVE

**MAP "A" OCP AMENDMENT
OCP14-0005**

-  Legal Parcel
-  PARK to MXR
-  Education / Institutional
-  Mixed Use (Residential / Commercial)
-  Major Park and Open Space

This map is for general information only.
The City of Kelowna does not guarantee its accuracy. All information should be verified.



Rev. Apr 09, 2014



1414

P3

1430

1435

Subject Properties Notes:

Rezone the subject properties from C7 Central Business Commercial to C7lp/rls Central Business Commercial (Liquor Primary/Retail Liquor Sales).

Subject Road Right-of-Way Notes:

Rezone portions of the road right-of-way from P3 Parks and Open Space to C7lp/rls Central Business Commercial (Liquor Primary/Retail Liquor Sales).

QUEENSWAY

230

1466

MILL ST

1470
C7

WATER ST

1473

235
C7lp/rls
235

1480

235

235

1476

1481

Subject Road Right-of-Way Notes:

Rezone a portion of the road right-of-way from C7 Central Business Commercial to C7lp/rls Central Business Commercial (Liquor Primary/Retail Liquor Sales).

248





294

C7

310-328

BERNARD AVE

MAP "B" PROPOSED ZONING
File #Z14-0006

-  C7 to C7lp/rls
-  P3 to C7lp/rls
-  Subject Properties
-  Legal Parcel

This map is for general information only. The City of Kelowna does not guarantee its accuracy. All information should be verified.

0 5 10 Metres

Rev. Apr 09, 2014





Project Rationale for the Proposed Downtown Hotel

The former Willow Inn property (currently a temporary parking lot), located adjacent to Kerry Park, is one of the most highly visible and important sites in the city of Kelowna. Because of its proximity to the sweet spot of our downtown, it represents a tremendous opportunity to bring more life and activity to the waterfront and downtown – to once again provide a connection between the city and the lake.

In 1928, the Willow Inn hotel opened on the downtown waterfront to much fanfare and acclaim. It quickly became a popular choice for visitors, as well as a thriving community venue where residents gathered to socialize. It became an important hub of the downtown, expanding over the years to include the Willow Inn Lodge and the Ferry Dock coffee shop. Today, this site remains an ideal location for a downtown hotel because of its views to the lake, marina, and Kerry, Stuart and City Parks – in fact, no other downtown site can rival it. What was done in 1928 demonstrated great vision and foresight in meeting the needs of the community. Once again it is time to bring that kind of forward thinking and pride to this site. This development application for an iconic downtown hotel recognizes the extreme importance of the opportunity and envisions a project that will return this location to its place as a destination and popular hub on the waterfront.

With a design that takes its inspiration from the city's Bernard Streetscape and the recently constructed marina buildings, the project proposes a 206 room boutique hotel with 17,000 sf of international quality conference space, 5,000 sf of restaurant, 4,800 sf of retail, and 2,400 sf of coffee shop. The development proposes a high standard of architectural design and the sensitive integration with the downtown waterfront. With an iconic presence, the project is envisioned as a significant community asset, one that will achieve one of the key principles that guided the Downtown Plan Charrette process, namely to "Enhance Kelowna's identity nationally and internationally and enhance the identity of downtown as Kelowna's Principle Centre."

Why does the hotel application propose the purchase of portions of Mill St. and Queensway Ave.?

When design work commenced, it quickly became apparent that adequate parking could not be accommodated and screened using the current site, especially given the high water table that makes going underground unfeasible. Expansion of the site to accommodate a hotel development would require purchase of Mill Street to the west. This was thought reasonable and logical because Mill St.'s main purpose, access to the Willow Inn site, was not necessary for the new hotel. Traffic engineering opinion also suggested that the continuation of Mill Street was not required for traffic flow purposes because Mill St. represented little more than a second

connection between Bernard Ave. and Water St. The parking challenge, however, proved to have some very positive consequences in terms of urban design and goals for the waterfront because, if the hotel was located directly adjacent to the park, rather than separated from it by a road, the hotel could provide some much needed animation and definition to Kerry Park. A pedestrian realm would be created between the hotel and the waterfront park, and hospitality uses along the edge of the park would provide ideal connections and integration with the park.

Purchase of Mill St. would also provide the opportunity to create an interesting and attractive transition between the hotel and Kelly O'Bryan's, one that would not include a highly visible service lane. After consulting with the owner of Kelly O'Bryan's, a low coffee shop building was designed, with careful attention to preserve views to the lake from Kelly O'Bryan's 2nd floor outdoor patio. At grade, the small remnant of road adjacent to Kelly O'Bryan's could also present a wonderful opportunity to create a sidewalk patio (similar to what is seen along the new Bernard Ave. streetscape) and pedestrian and green space plaza that would further activate and animate the park edge.

Why is the hotel important from a planning perspective?

With the Kelowna Yacht Club already under construction and the final phase of Stuart Park soon to follow, the proposed hotel represents one of the final pieces of the puzzle in revitalization of the downtown waterfront. The iconic hotel will provide a key connection point along the waterfront - from the wetlands of Brandt's Creek in the north to City Park on the south. This project will also be a significant catalyst for further development in Kelowna's downtown. The site provides the perfect location to achieve a vibrant downtown waterfront hotel, a use that is highly desirable in the downtown core, and with limited opportunities in Kelowna to enjoy a meal while viewing the lake, the new hospitality uses will be welcomed by both residents and visitors.

Some key principles of the OCP which are included within the project are the strong sense of authenticity reflective of the distinct urban design character of Kelowna, the high quality of structure, and strong integration with the surrounding context. The architectural materials, finishes and details have been carefully selected and designed to harmonize with the Kelowna context, while at the same time raise the benchmark for future development by creating a fresh, vibrant and iconic architectural design.

Through the incorporation of stepping forms, creative massing, varying geometry, rich natural materials and an emphasis on human scale, the design creates an interesting, pedestrian friendly interface with adjacent streets and park spaces. As a catalyst for an enjoyable waterfront experience, the design promotes public safety through active engagement of outdoor patios and walkways with the internal programming of the adjacent hotel lobby, restaurant, coffee shop, and overlook from the layered terraces of the podium, all of which will be highly animated with people.

How many parking stalls on Mill St. and Queensway will be lost in order to accommodate this development proposal? How many parking stalls will be provided in the project, and how was this number determined?

Twenty two stalls will be lost on Mill and Queensway as a result of the new hotel. These stalls will be rebuilt within the parkade structure of the hotel and a covenant on the property's title will guarantee that they remain available for public use.

According to the zone requirements of the City of Kelowna, 250 parking stalls are required. The proposed project provides 242, 8 short of the zone target. As part of the application submission, a traffic and parking analysis has been undertaken by Bunt & Assoc., a transportation consulting firm with extensive experience in Kelowna and its downtown core. According to their analysis, the number of stalls required to accommodate full occupancy of the hotel and its associated uses is 175. The findings of their report state that the project will provide 67 more stalls than are actually needed. This surplus parking will assist with the public parking needs of the downtown core.

What determined the size of the project?

There were a number of key considerations which led to the ultimate size and shape of the development. The project needed to be financially viable, the size of the project needed to make sense from a demand perspective, and the site needed to be able to accommodate the required number of parking stalls. Equally important, the project needed to respect the sensitivities, aspirations and hopes for this important location in our city. The downtown is "the anchor for the city and attracts people. It forms the reputation of the city and as such should be the best it could possibly be." (Excerpted from the Downtown Plan).

In terms of financial viability, the hotel needed to have a minimum of 200 rooms, and, in terms of demand, event planners, Tourism Kelowna, and representatives from other hotels told us that 200 rooms was the correct number required if the new hotel was to successfully fulfil a needed support role in the downtown core. With two downtown hotels within walking distance of each other, having a combined room count of approximately 600 rooms, Kelowna will be in a much better position to attract larger conferences to our city. We believe there will be excellent opportunities for synergies between the hotels.

According to the Downtown Plan, 19 storeys can be considered on the Willow Inn site "upon demonstration that the proposed development gives careful consideration to view impacts from other parts of downtown, is a signature landmark building and meets a high standard of design excellence."

The architectural form of the hotel was developed with a specific architectural rationale related to the necessity to achieve balance between form, massing and the various hotel uses. The design of the rooms and the potential of the floor plates resulted in 206 rooms on top of the podium, for a total of 24 storeys. If the tower floor plate were to increase in size, the tower would be lower, but it would be bulky and the elegance of correct proportion would be lost. A slender elliptical shape was chosen as this narrows the perception of the form achieving an appropriate balance between mass and height. The tower presents a very narrow face to the lake and to the east, minimizing impact to the lake views from tall buildings that will be built in the future behind it.

There was an earlier investigation into providing 200 rooms in two shorter towers, but the two tower scenario had much more significant visual impact, both from the street level and from the surrounding buildings, especially given that the site size did not allow for good separation of the towers. One tall slender tower was deemed to be the most architecturally and visually appealing solution, with much less visual impact from other buildings, from the water, and from the street level.

Striking the right balance between all the varying objectives was a key driver in achieving a design that would fulfil the planning requirements of the City of Kelowna, the need for economic feasibility, the aspirations of the community for the waterfront, the vision for a landmark development on this important site, and the expectations of guests wanting a top class hotel experience.

This site is a former hotel site, already zoned C7. What approval processes does this development application need to go through with the City of Kelowna?

- a) Development Permit Application for the form and character of the overall proposal
- b) Development Variance Permit for a height variance, a parking variance and two variances related to the podium
- c) Proposed road closures of portions of Mill Street and Queensway
- d) Proposed purchase from the City of portions of closed road
- e) Rezoning and OCP amendment of a portion of the road closure area
- f) Landscaping and servicing improvements on public and private property

The former Willow Inn site does not require rezoning. The rezoning applies only to the western half (one lane) of Mill St. that Westcorp is proposing to consolidate into the site. Technically, zoning in the City of Kelowna extends to the middle of each road because roads do not have a specific zone attributed to them. In this case, the P3 zoning of Kerry Park is attributed to the western half of Mill St. Throughout all of Kelowna, commercial and residential zones extend onto the roadway regardless of the fact that the commercial and residential uses do not. Since, technically, the west half of Mill St. is zoned P3 (because Mill St. is beside Kerry Park), a rezoning is required in order to consolidate the roadway into the Willow Inn site. It should be clearly understood, however, that there is no reduction of Kerry Park required by this development.

What role will the conference space play?

The 17,000 sf of conference space in the new hotel will accommodate 300+ for dinner. The state-of-the-art rooms will be international quality, have expansive lake views, and access to rooftop patios overlooking Kerry and Stuart Parks. There are few conference spaces in the North America, or the world, that will be able to rival this location or experience. New conference business will be attracted to Kelowna as a result of this new venue, and local residents will have fabulous new facilities in which to host business events, local galas, and first class Okanagan weddings.

What benefits will the proposed hotel provide to the downtown and Kelowna as a whole?

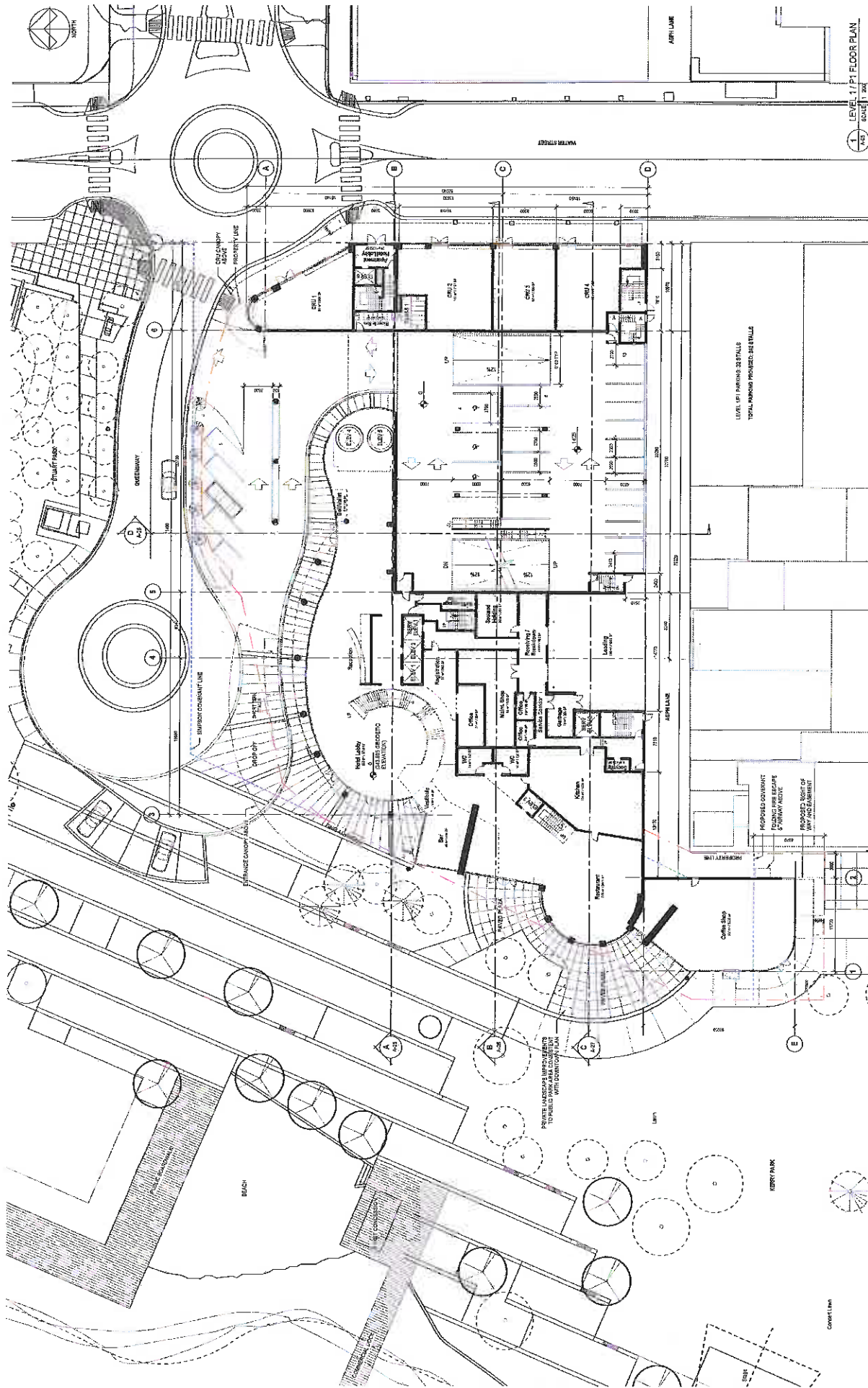
In terms of economic impact, the direct capital investment to develop the hotel is expected to be in excess of \$65M, will lead to over 720 (direct + indirect) construction jobs, and provide an economic output of over \$112M. Over the long term, nearly 300 (direct + indirect) on-going positions will be created by the proposed hotel, over 100 of which will be in the new hotel. In terms of impact on surrounding businesses, it is forecast that the average daily spend of each hotel guest will be \$297 per day. This equates to \$20.8M spent annually, much of which is expected to be spent in Kelowna's downtown. In addition, the hotel will have many positive social impacts for residents and visitors of Kelowna because of how the hotel and hospitality uses will transform the feeling of Kerry Park and the waterfront. The downtown core will become increasingly more vibrant as the civic heart of the city.

To quote from the Downtown Plan, “For Kelowna residents, the downtown is the heart and hub of the city. It is the key to Kelowna's unique and authentic identity and to attracting residents, tourists and investment.” Comments received from Downtown Plan survey correspondents strongly indicate the importance of a dynamic downtown environment for the citizens of Kelowna. In particular, the waterfront was identified as a major attraction which contributes to the “Kelowna Brand”.

The proposed hotel will increase commercial activity on the waterfront, stimulate year-round use and create a vibrant and energetic public realm which supports both existing and new open spaces. This will have the added benefit of responding to public concern for an increased sense of safety in this area.

With a striking elliptical form set within a podium massing that steps down and gestures to Kerry Park, Bernard Avenue and the waterfront, together with a material palette that draws from the natural beauty of the Okanagan Valley and builds on the local vernacular, the new downtown hotel project will significantly reinforce the identity of the city and contribute to the emerging “Kelowna Brand”.

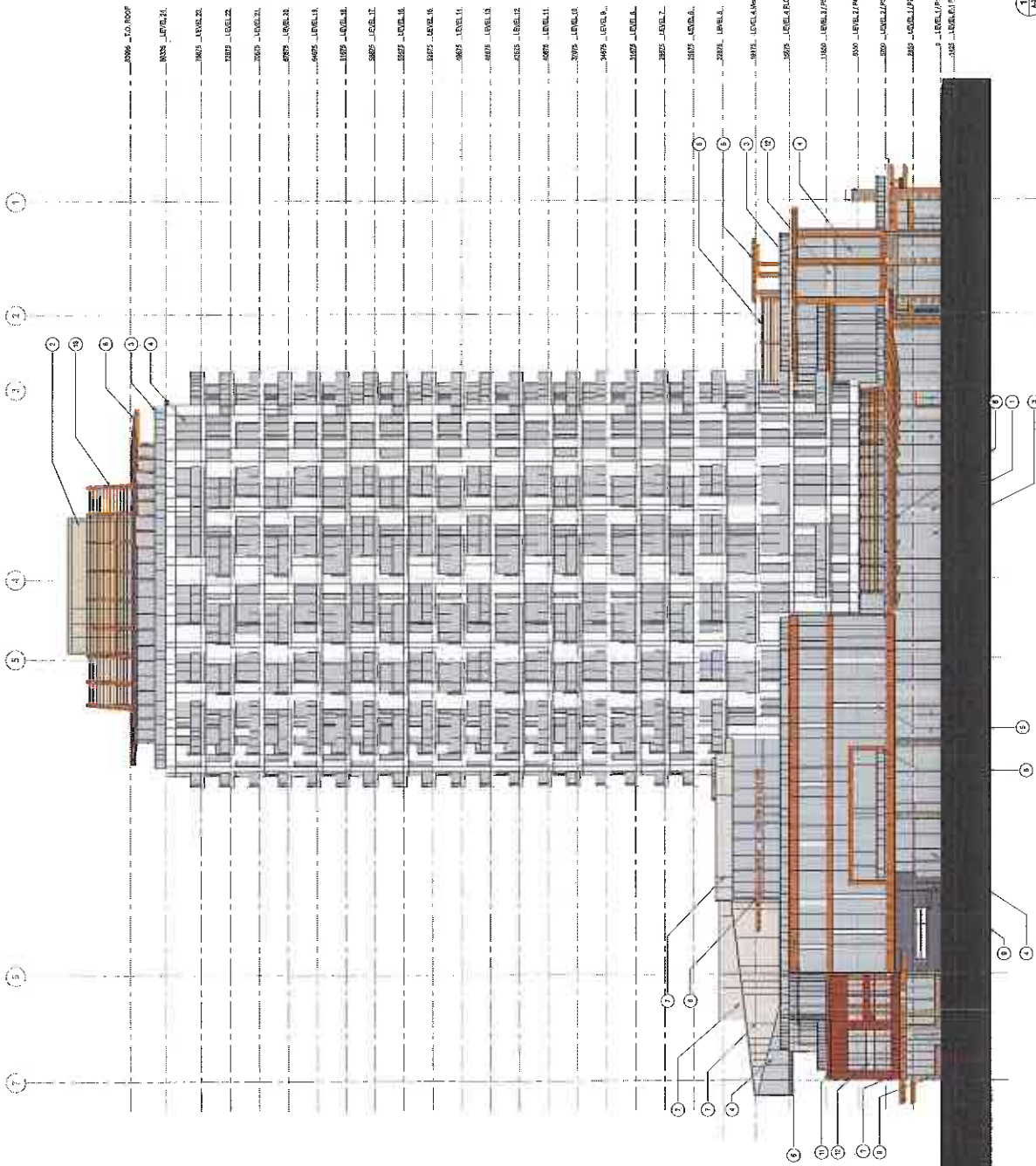
As described by one survey respondent, “Vibrant downtowns are social hubs that breathe energy and ideas into a place.” That is the kind of downtown that this project aspires to!



A-05
2014/02/04
PROJECT 160013

LEVEL 1/P1 FLOOR PLAN
Kelowna Downtown Hotel
Kelowna, BC





- 1 - Cement Composite Paneling System - Colour White
- 2 - Cast in Place Concrete - Clear Sealed
- 3 - Insulated Double Glazing Clear Float Glass
- 4 - Insulated Double Glazing - (Singleard Superneutral 64 or sim.)
- 5 - Opaque Privacy Screen - Colour Amber/Yellow
- 6 - Laminated fir canopy beams - Colour Light Brown
- 7 - Metal/Grey Composite Paneling System
- 8 - Composite Paneling System - Colour Light Brown
- 9 - Sandstone Veneer - (Cultured Stone or sim.) - Colour Beige
- 10 - Aluminium Window Clear Anodized
- 11 - Prefinished Metal Flashing - Colour Light Grey
- 12 - Painted steel - Colour Dark Grey
- 13 - Painted Steel - Colour Light Brown
- 14 - Painted hollow metal door - Painted Grey
- 15 - Overhead metal door - Painted Grey

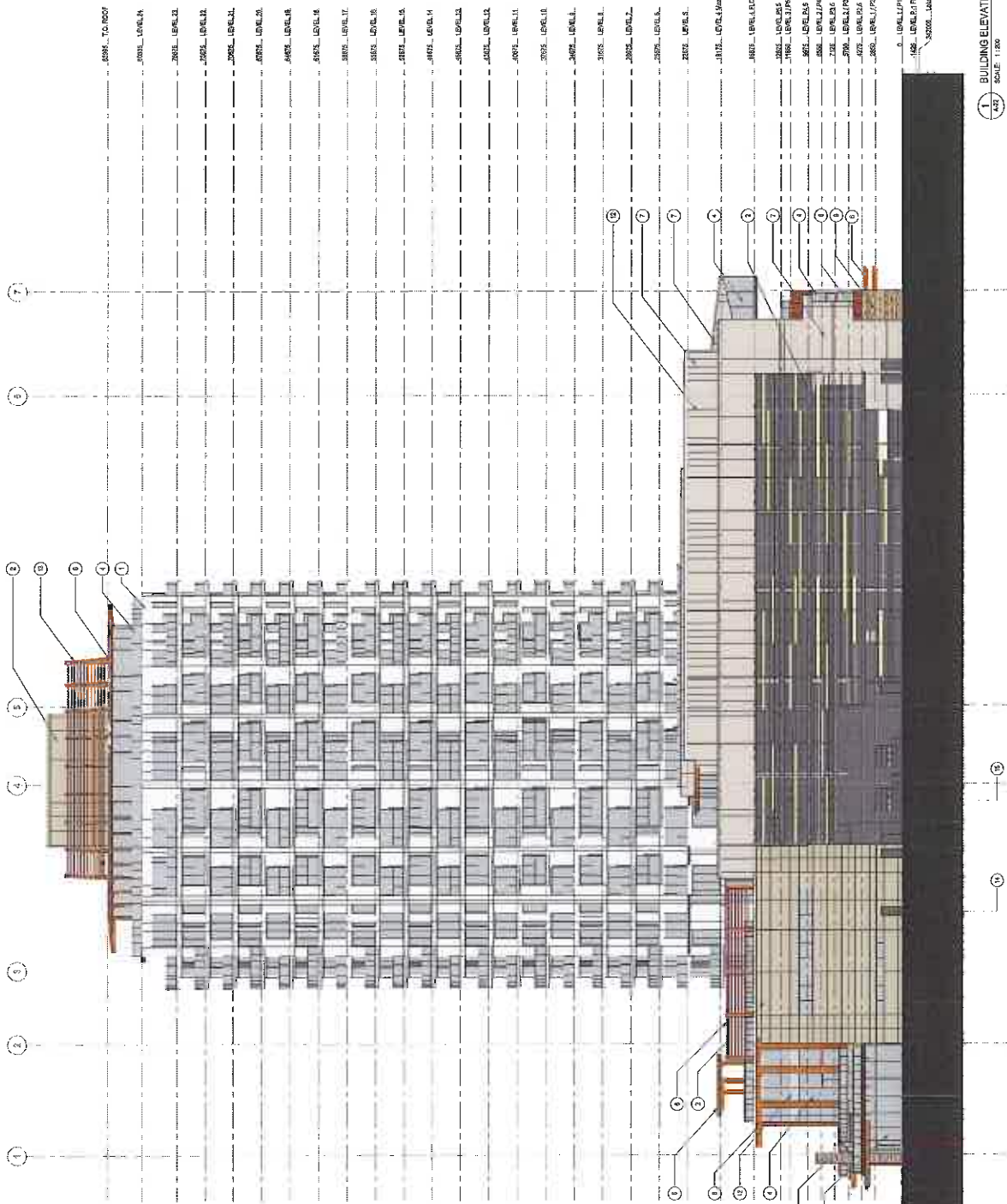
1 BUILDING ELEVATION - NORTH
SCALE: 1:200



A-21
2014/02/04
PROJECT 160013

BUILDING ELEVATION - NORTH
Kelowna Downtown Hotel
Kelowna, BC





- 1 - Cement Composite Paneling System - Colour White
- 2 - Cast In Place Concrete - Clear Stained
- 3 - Insulated Double Glazing Clear Float Glass
- 4 - Insulated Double Glazing - (Sunguard Supraneutral 54 or sim.)
- 5 - opaque Privacy Screen - Colour Amber/Yellow
- 6 - Laminated fir canopy beams - Colour Light Brown
- 7 - Metallic Gray Composite Paneling System
- 8 - Composite Paneling System - Colour Light Brown
- 9 - Sandstone Veneer - (Cultured Stone or sim.) - Colour Beige
- 10 - Aluminum Window Clear Anodized
- 11 - Prefinished Metal Flashing - Colour Light Grey
- 12 - Painted steel - Colour Dark Grey
- 13 - Painted Steel - Colour Light Brown
- 14 - Painted hollow metal door - Painted Grey
- 15 - Overhead metal door - Painted Grey

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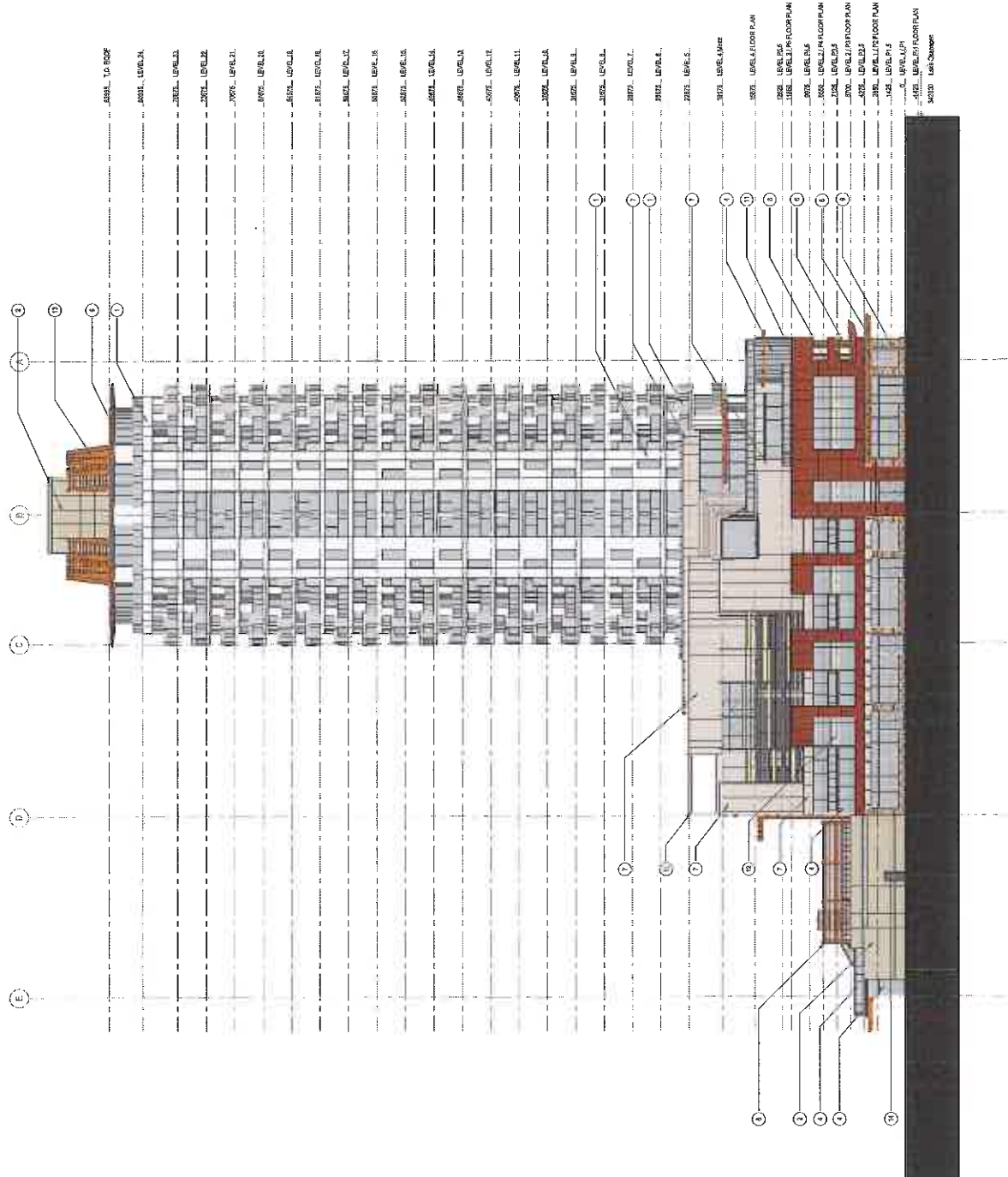
1 AS2
BUILDING ELEVATION - SOUTH
SCALE: 1:20



A-22
2014/02/04
PROJECT 160013

BUILDING ELEVATION - SOUTH
Kelowna Downtown Hotel
Kelowna, BC





- 1 - Cement Composite Paneling System - Colour White
- 2 - Cast in Place Concrete - Clear Sealed
- 3 - Insulated Double Glazing Clear Float Glass
- 4 - Insulated Double Glazing - (Sunguard Superspectral 54 or sim.)
- 5 - Opaque Privacy Screen - Colour Amber/Yellow
- 6 - Laminated fir canopy beams - Colour Light Brown
- 7 - Metal/ Gray Composite Paneling System
- 8 - Composite Paneling System - Colour Light Brown
- 9 - Sandstone Veneer - (Cultured Stone or sim.) - Colour Beige
- 10 - Aluminum Window Clear Anodized
- 11 - Prefinished Metal Flashing - Colour Light Grey
- 12 - Painted steel - Colour Dark Grey
- 13 - Painted Steel - Colour Light Brown
- 14 - Painted hollow metal door - Painted Grey
- 15 - Overhead metal door - Painted Grey

1 BUILDING ELEVATION - EAST
A23 SCALE 1:200

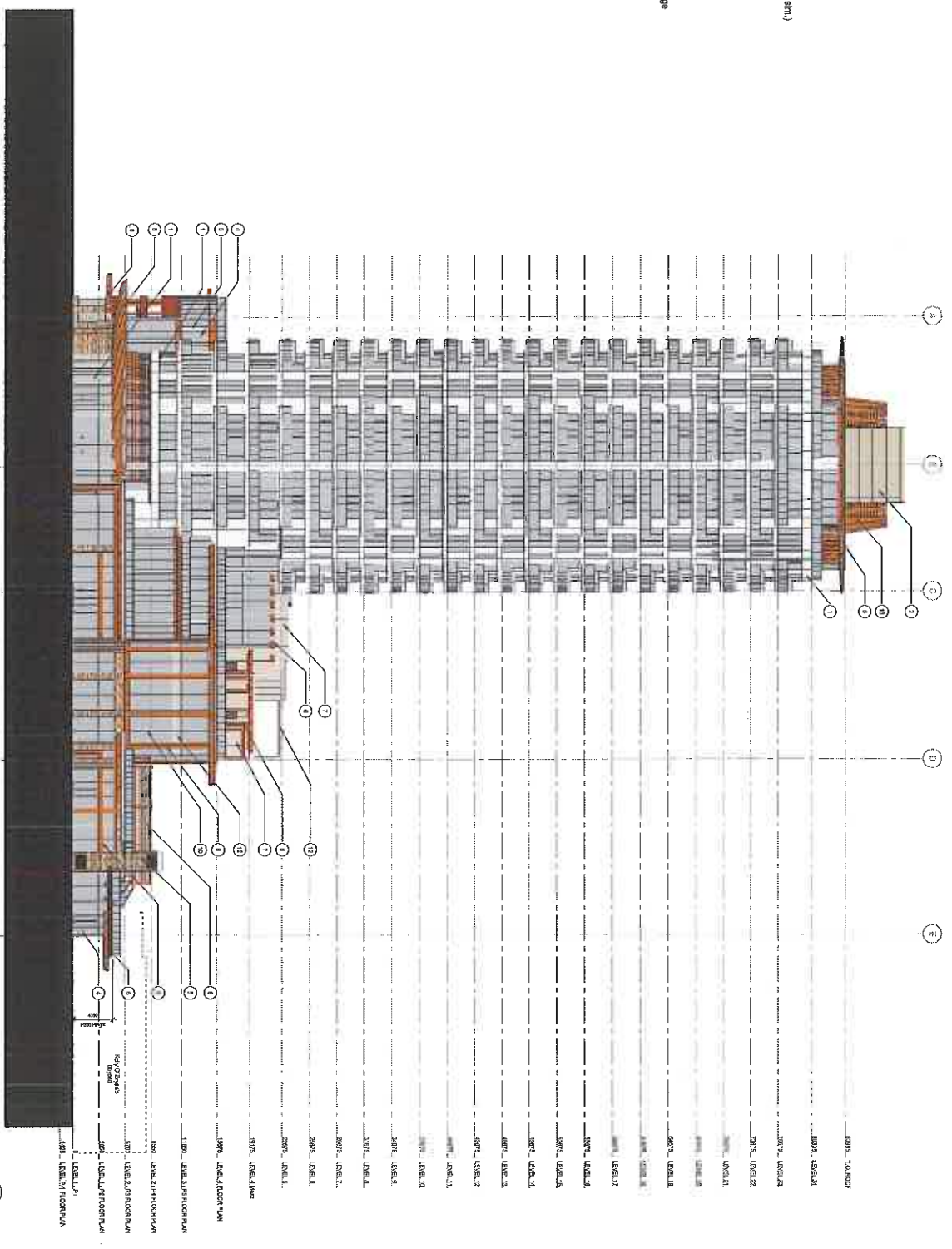


A-23
2014/02/04
PROJECT 160013

BUILDING ELEVATION - EAST
Kelowna Downtown Hotel
Kelowna, BC



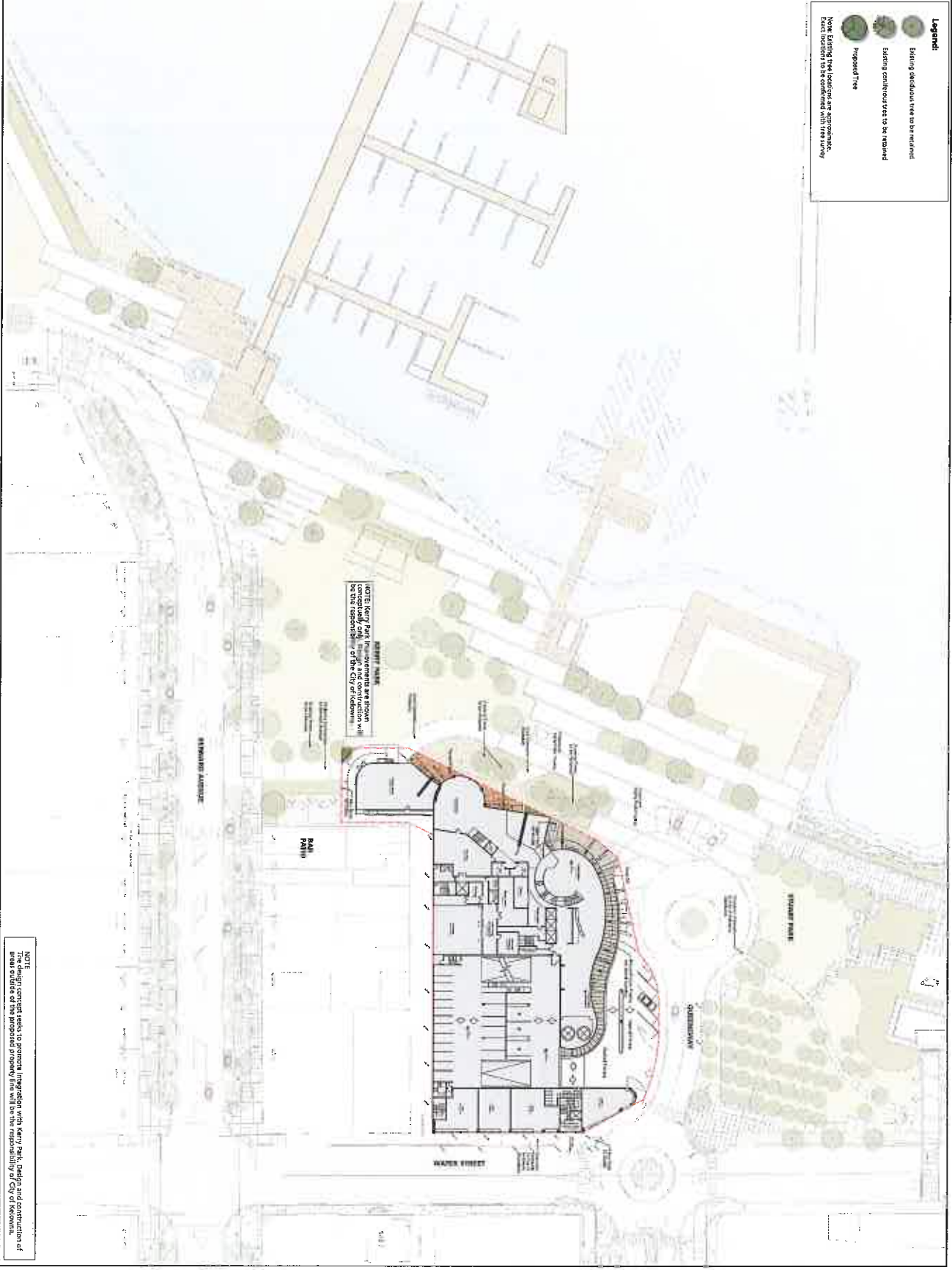
- 1 - Cement Composite Paneling System - Colour White
- 2 - Cast In Place Concrete - Clear Sealid
- 3 - Insulated Double Glazing Clear Float Glass
- 4 - Insulated Double Glazing - (Singlepaned Supramerual 54 or sim.)
- 5 - Opaque Privacy Screen - Colour Amber/Yellow
- 6 - Laminated fir canopy beams - Colour Light Brown
- 7 - Metallic Grey Composite Paneling System
- 8 - Composite Paneling System - Colour Light Brown
- 9 - Sandstone Veneer - (Cultured Stone or sim.) - Colour Beige
- 10 - Aluminium Window Clear Anodized
- 11 - Perforated Metal Flashing - Colour Light Grey
- 12 - Painted steel - Colour Dark Grey
- 13 - Painted Steel - Colour Light Brown
- 14 - Painted hollow metal door - Painted Grey
- 15 - Overhead metal door - Painted Grey



Legend:

- Existing deciduous trees to be retained
- Existing coniferous trees to be retained
- Proposed trees

Note: Existing tree locations are approximate. Exact locations to be confirmed with tree survey.



P+A

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1. Issued for DP 02/02/2014
 Revision No. Date

GENERAL RESERVE
 The City of Kelowna, in the name of a
 Plan Area & Assessor Inc. and in the name of
 the City of Kelowna, hereby grants to the
 applicant a license to use the land for the
 purposes stated in the plan, subject to the
 conditions of any other plan, which the plan
 shall be deemed to be a part of this plan.
 The license shall be valid for the period of
 10 years from the date of this plan.

Westcorp

PROJECT TITLE
 Kelowna
 Mill Street Hotel

DRIVING TITLE
 Landscape
 Context Plan

Project No: 14-001
 Date: 14/01/14
 Drawn By: J.M./J.P.
 Checked By: KP
 Scale: 1:100
 Project No: 14-001

L-0.0



View looking towards site from Mill St and Bernard Ave



View from Bernard Ave



View looking south from Stuart Park



View from Queensway and Water St.



View from Kary Park



View from Water St, looking North



View looking down Queensway towards Marina



View of Marina and site from water.



Aerial view of site in relation to Marina



Marina glulam and decking



Marina decking



Marina roof and structure



Context Photos
Kelowna Downtown Hotel
Kelowna, BC

A-04
2014/02/04
PROJECT 160013



VIEW DESCRIPTION

This view of the project illustrates how the proposed tower and podium interface with Kerry Park with the rotunda 'lantern' form stepping down to create a backdrop and focal point in scale with the park. Viewed from above, primarily by hotel rooms and possibly from a distance by future tower developments, the green roof of the centre core centre serves as an attractive view from hotel rooms with its outside room patio and pool, with its outside relaxing area and bar adds to the enjoyment of the park and waterfront. The elliptical tower form stands as iconic landmark element visually and actively connecting downtown with the waterfront as a single cohesive urban composition.





VIEW DESCRIPTION

The proposed hotel tower and podium integrate with the Bernard Ave block and surrounding context while still adding their own distinctive character to the downtown Kelowna experience. This integration is achieved by means of the decreased scale of the podium terraces stepping down towards Bernard Avenue and the adjacent Kelly O'Bryan's Pub. The final terrace is scaled so as to be at the same height or lower than the second storey Kelly O'Bryan's patio, providing a respectful integration with the adjacent property owner. The variety of form, lecture, material and colour of the podium provides an additional level of character to the proposed hotel, and were selected as being in keeping with the Okanagan palette. The rounded addresses Mary Park, complementing the form of the tower and providing a landmark on the pedestrian edge, as well as a 'lantern' at night.



VIEW DESCRIPTION

The view highlights the variety of hotel scale and form that addresses Kerry Park, from the elegance and sturdiness of the tower, to the roundness which provides the element around which the various hotel forms are composed. The verticality of the fireplace chimney is the anchor point for the scale transition towards Bernard Ave and Kelly O'Bryan's. The fireplace itself provides the feeling of an outdoor living room on the park. The reduced scale, as well as the materials used, harmonizes with the existing heritage architecture. The park pedestrian walkway is completely integrated with the podium, creating a vibrant outdoor experience which provides a lively backdrop to the grassy stage area. The activity on the hotel podium terraces overlooking the park enhances this degree of integration.





VIEW DESCRIPTION

Set back from Kerry Park and integrated with the highly activated and transparent podium, the slender, elliptical lower form anchors the development and marks the entry to the hotel at the foot of Queensway Avenue. The pattern of alternating balconies and the top of the tower - articulated by the trellis features, provide visual interest and reinforce the landmark quality of the proposed development. The stepping form of the podium brings the scale of the development down to the park with the lantern rounda creating a highly visible local point. The coffee shop to the south, accentuated by the stone fireplace element, forms a 'living room' further integrating the development with Kerry Park. Wood accents, terraces and trellis features create warmth and activate the development with a uniquely Kelowna sense of place.



View from the Water
 Kelowna Downtown Hotel
 Kelowna, BC

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 2014/02/04
 PROJECT 160013





VIEW DESCRIPTION

This view dramatically highlights the warmth, presence and inviting feel of the hotel and the Kerry Park edge. The extensive hotel lobby glazing, which will open up during summer, blurs the transition between the indoor and outdoor activities of the hotel and park. The wood theme structures of the ports cocoon complement the architecture of the marina close by on the waterfront. This view also shows how the scale of the hotel begins to transition down to the park as it turns to corner toward Bernard Avenue.



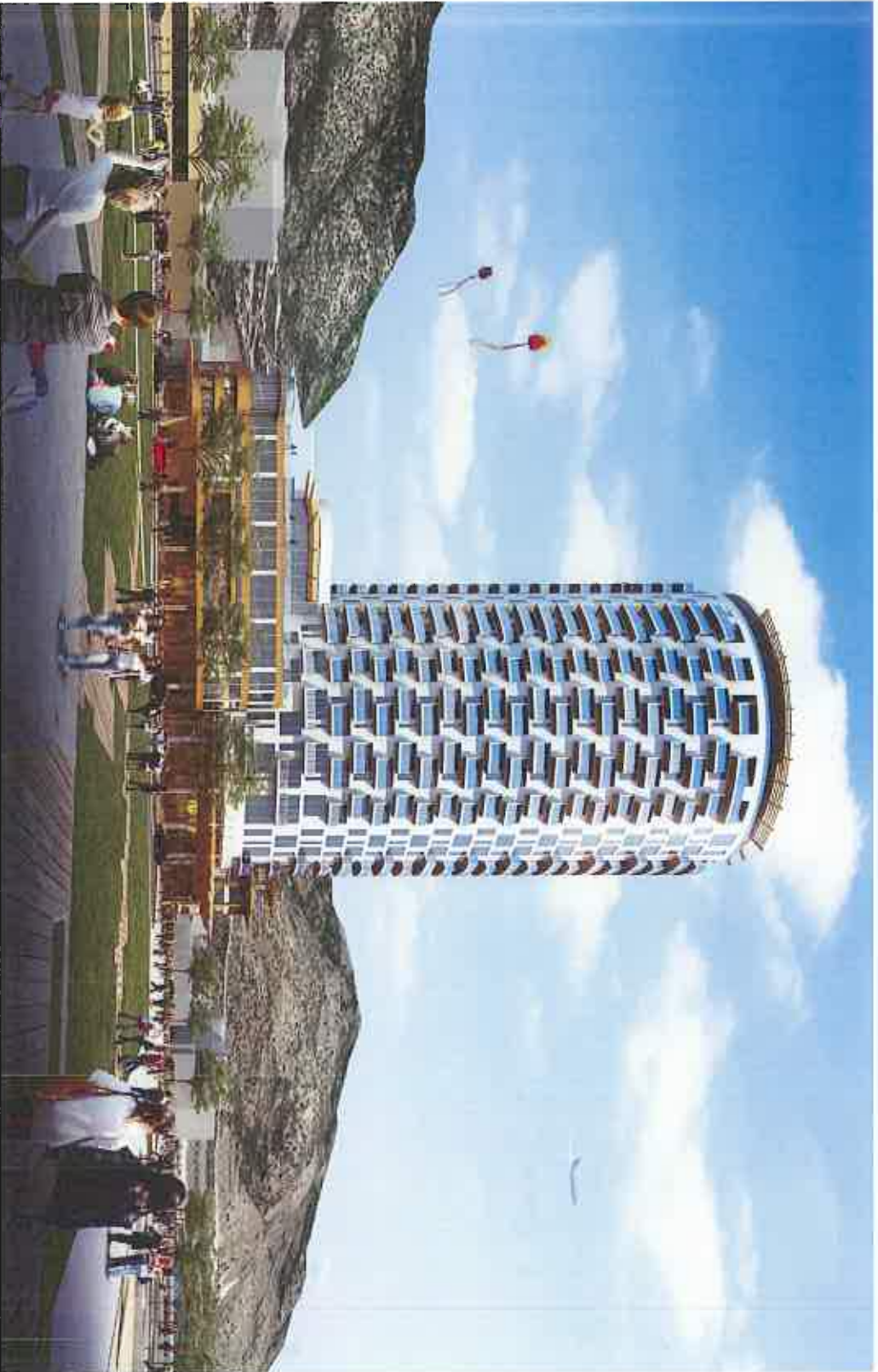
View toward the Hotel Lobby
 Kelowna Downtown Hotel
 Kelowna, BC

A-35
 2014/02/04
 PROJECT 160013



VIEW DESCRIPTION

The textured balcony patterns of the landmark tower rising from the highly activated and transparent podium will form a lively backdrop to the south end of Stuart Park opposite the new Yeart Club under construction to the north. Together, the two projects will reinforce Stuart Park as a significant urban public open space on the waterfront. At grade a dramatic cantilevering port coorene constructed of wood treame beams will mark the entrance to the hotel and create a focal point on the turnabout proposed for the terminus of Queensway Avenue. Midway on the podium a long horizontal balcony with wood finishes will further activate Stuart Park together with a large rooftop terraces serving the conference center.





VIEW DESCRIPTION

Viewed from City Hall and the northeast corner of Queensway and Water Street, highly articulated forms and architectural features create an urban presence, extending the commercial core of the City north along Water Street. A strong, mixed-use element featuring retail and apartment hotel units creates a highly active and transparent base, echoing the lantern element facing Kerry Park and the waterfront, a slender rotunda feature gestures towards City Hall at the corner of the intersection and marks the transition to the conference center. The rotunda transforms vertically to create an interesting view point from the conference terrace level on the podium rooftop. A dramatic cantilever feature soars over Water Street, creating a unique point of interest both from the exterior as well as from the interior at the conference level where prefunction spaces will have a 'kaleidoscope' view of the city. Patios and decks amass the facade, while rounded edges and warm materials soften the building, making it part of the landscape and a uniquely Kelowna experience.



View from City Hall corner of Queensway and Water St.



View from Queensway & Water St.
 Kelowna Downtown Hotel
 Kelowna, BC

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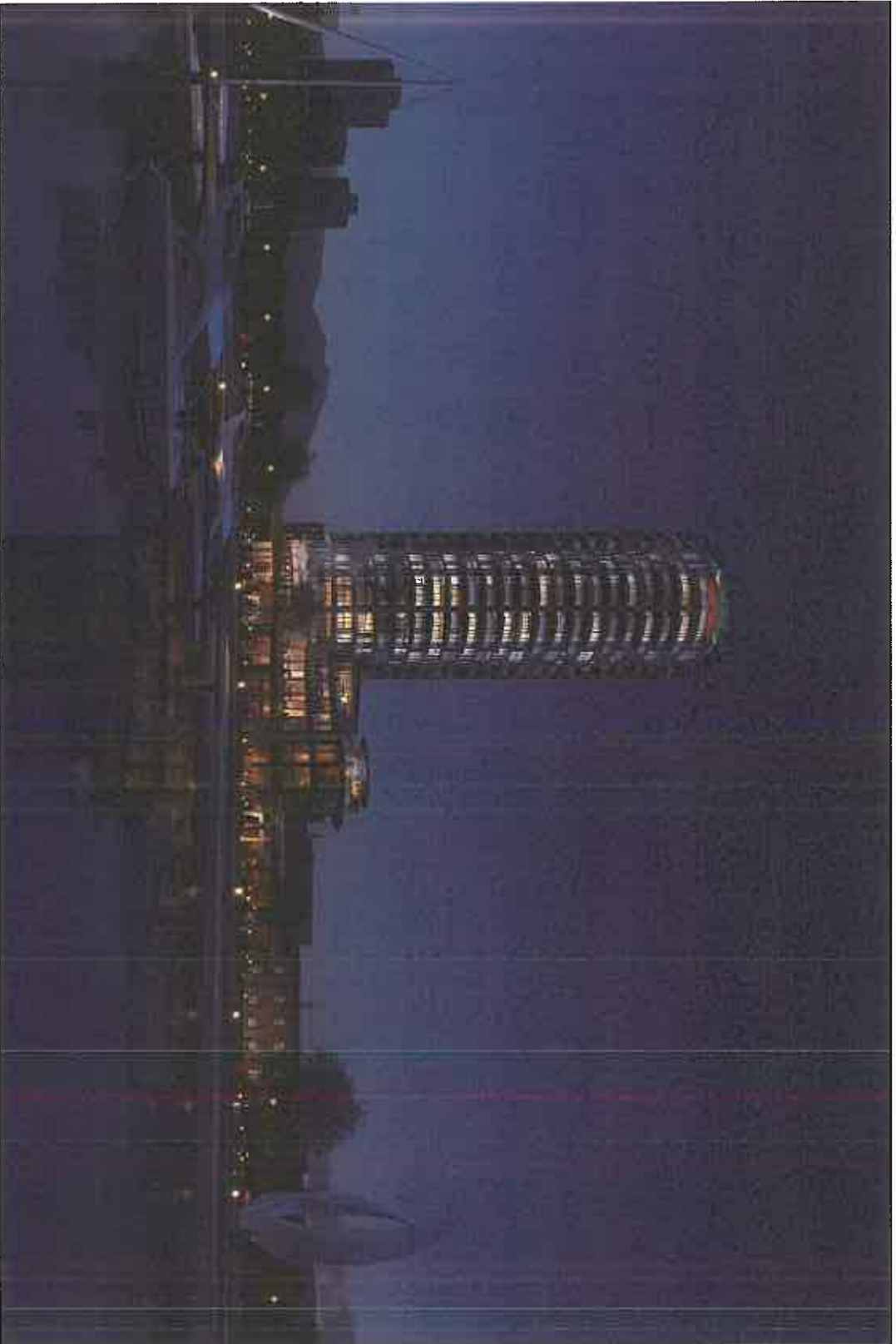
VIEW DESCRIPTION

Viewed from the city scale, the slender tower form dramatically terminates the Queensway view corridor. For the pedestrian the podium grounds the tower, providing a warm, interesting, human scale experience as the pedestrian approaches the hotel. The rounda at the corner of Water and Queensway complements the elliptical tower form seen in the background and at an urban scale is a formal nod towards City Hall.



VIEW DESCRIPTION

Viewed from the lake and the Downtown Marina at dusk and in the evening, the elegant and slender curving form of the tower with the majestic trellis featured at the top, together with the curving form of the rounda facing Kerry Park will be accentuated by soft light creating a lantern effect similar to the small buildings on the lake supporting the new marina. The soft curves and indirect lighting will complement the Sails sculpture and the gently sloping hills in the distance washed by the setting sun. The composition of form, light and materials together with the fireplace feature anchoring the low form of the coffee house adjacent to Kelly O'Bryan's will transform Kerry Park into an outdoor living room. A living room animated by the sounds of music and concerts in the Park on summer evenings.

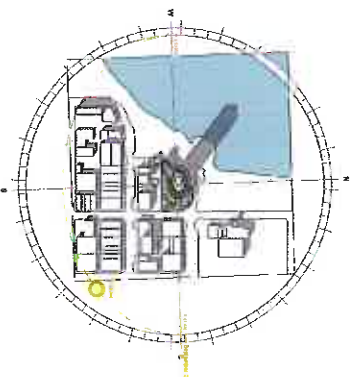
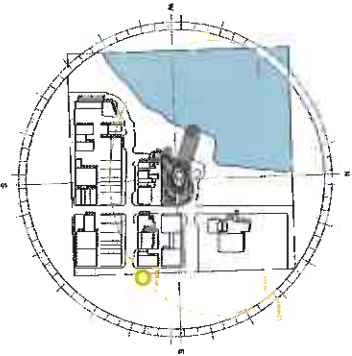
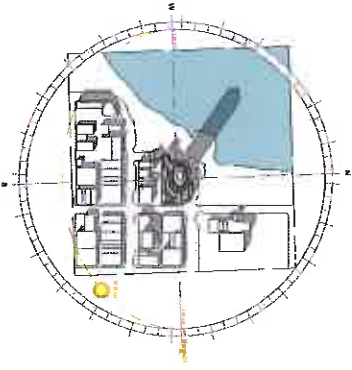


MARCH 21

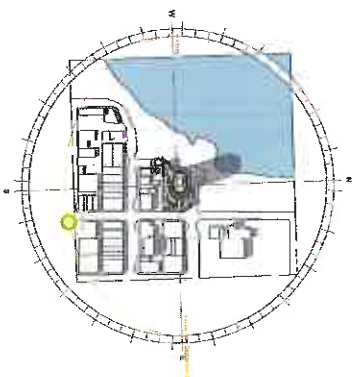
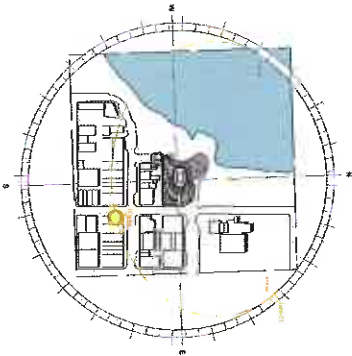
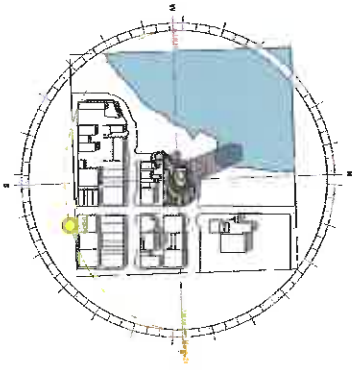
JUNE 21

SEPT 21

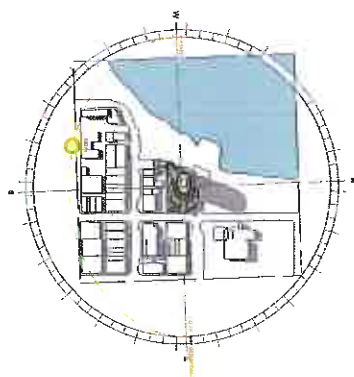
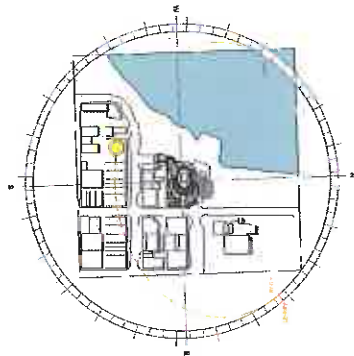
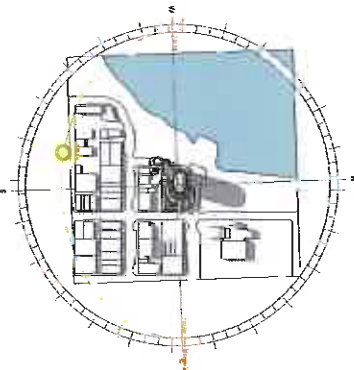
10 AM



12 PM



2 PM



Shadow Analysis
 Kelowna Downtown Hotel
 Kelowna, BC

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 2014/02/04
 PROJECT 160013



References and Assumptions

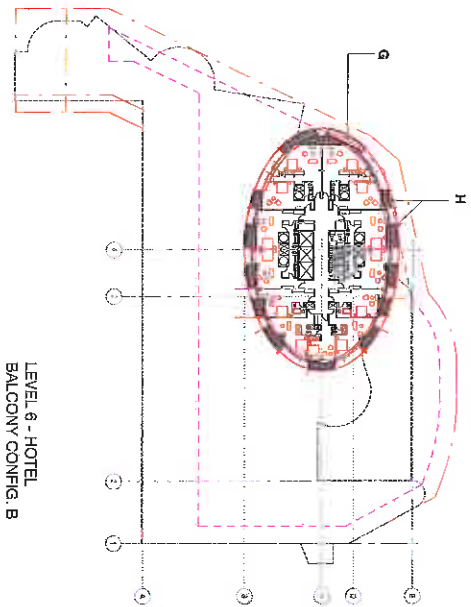
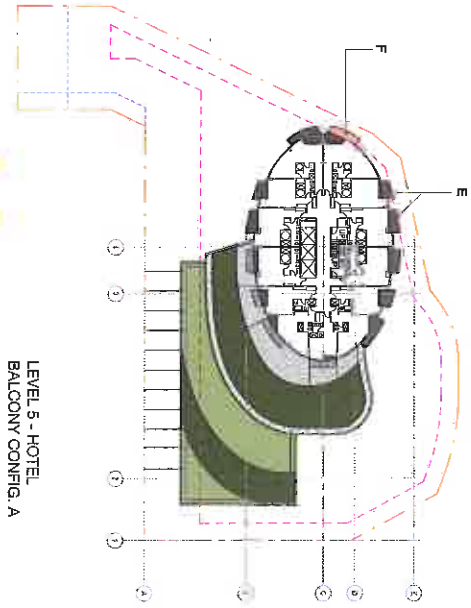
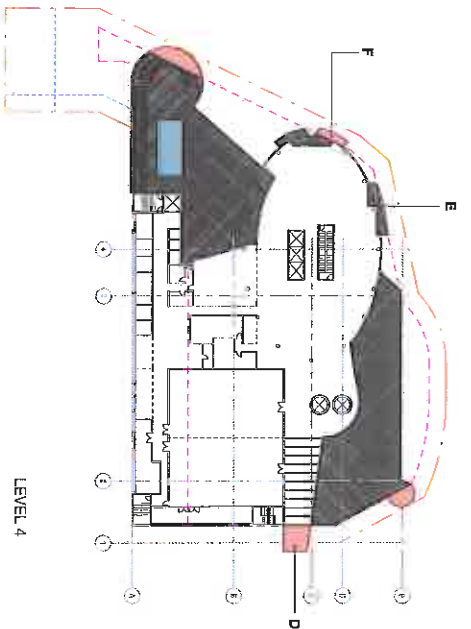
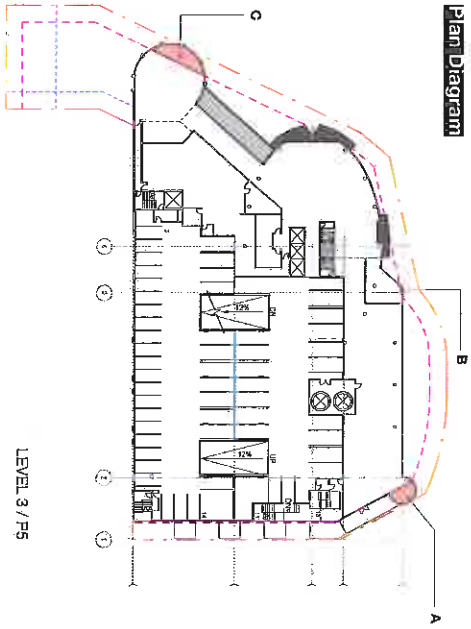
 Areas representing encroachment into 3.0m setback (See bylaw reference below)

Assumptions: Variances are based on proposed property lines as illustrated.

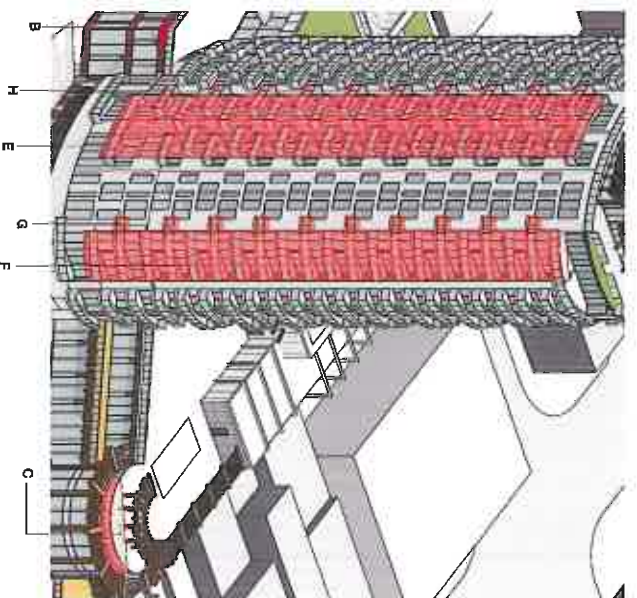
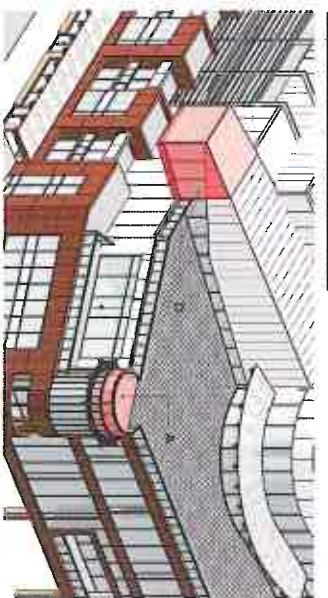
Bylaw Reference: Consolidated Zoning Bylaw No. 8000, C7 - Central Business Commercial, 14.7.5 Development Regulations, Section (e)
 "Any portion of a building above 15 metres in height must be a minimum of 3.0m from any property line abutting a street, as shown on C7 - Diagram B attached to this bylaw.
 The above setbacks will be measured from the nearest exterior building face, exclusive of unenclosed balconies."

C7 - Proposed Setback Variances above 15m

Plan Diagram



Proposed Setback Variances



C7 - Proposed Setback Variances above 15m
 Kelowna Downtown Hotel
 Kelowna, BC

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 2014/02/04
 PROJECT 160013



C7 - Proposed Setback Variances above 22m

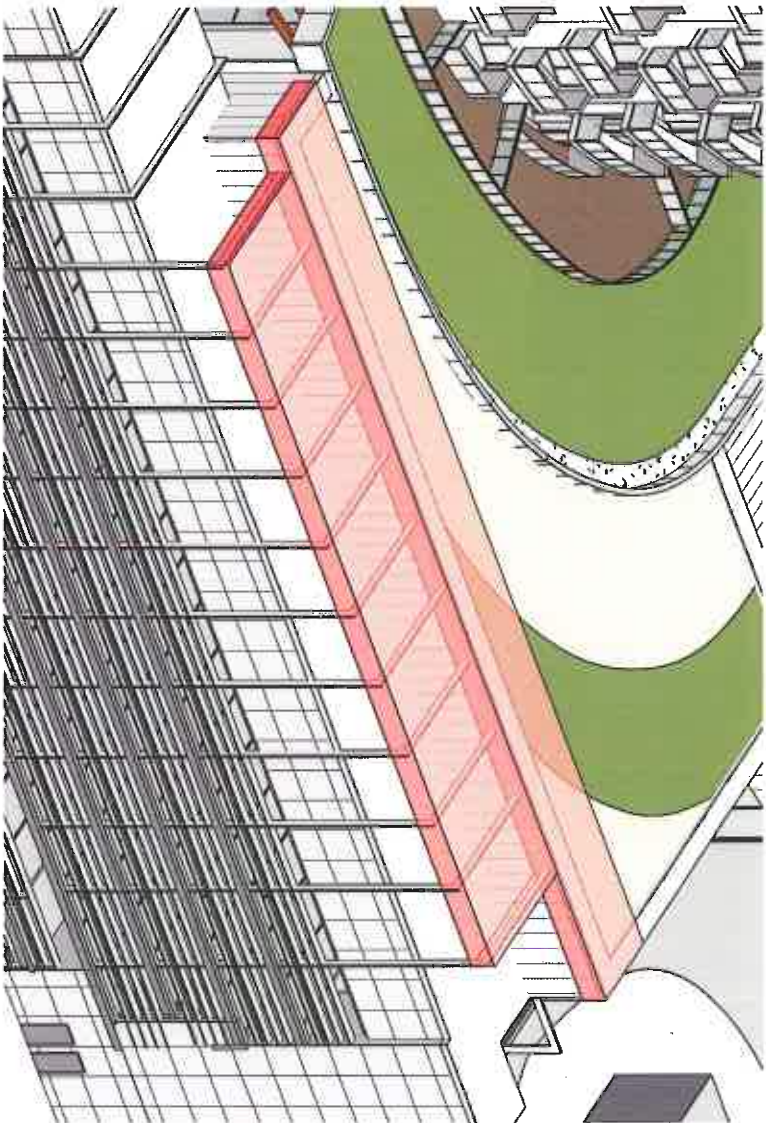
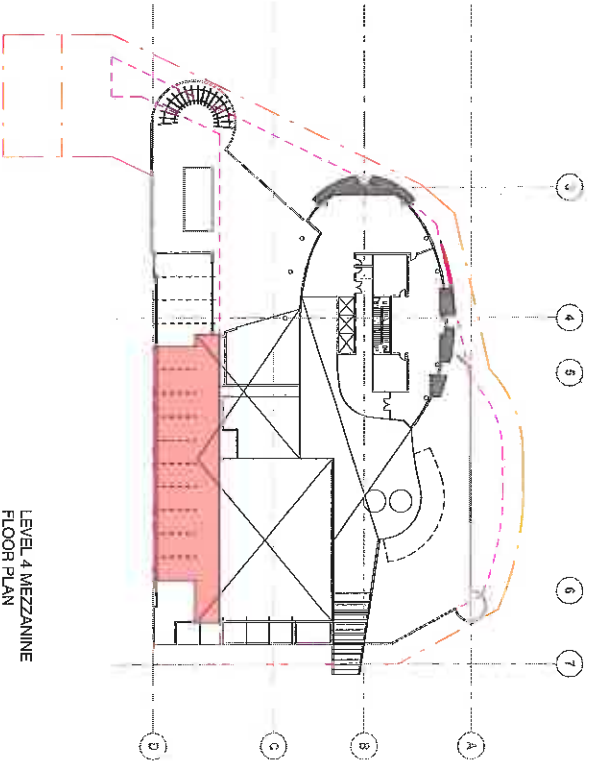
References and Assumptions

 Areas representing encroachment into 10.0m setback (See bylaw reference below)

Assumptions: Variances are based on proposed property lines as illustrated.

Bylaw Reference: **Consolidated Zoning Bylaw No. 8000, C7 - Central Business Commercial, 14.7.5 Development Regulations, Section (i):**
 "Any portion of a building above 22.0m in height must maintain a minimum setback of:
 (i) 15.0m from any property line abutting another property line,
 (ii) 10.0m from any property line abutting a lane,
 (iii) 30.0m between more than one tower on the same property.
 The above setbacks will be measured from the nearest exterior building face, exclusive of unenclosed balconies."

Plan Diagram



C7 - Proposed Setback Variances above 22m
 Kelowna Downtown Hotel
 Kelowna, BC

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 2014/02/04
 PROJECT 160013



C7 - Proposed Variances: Building Height Downtown Plan - Comparative Analysis: Building Height

References and Assumptions

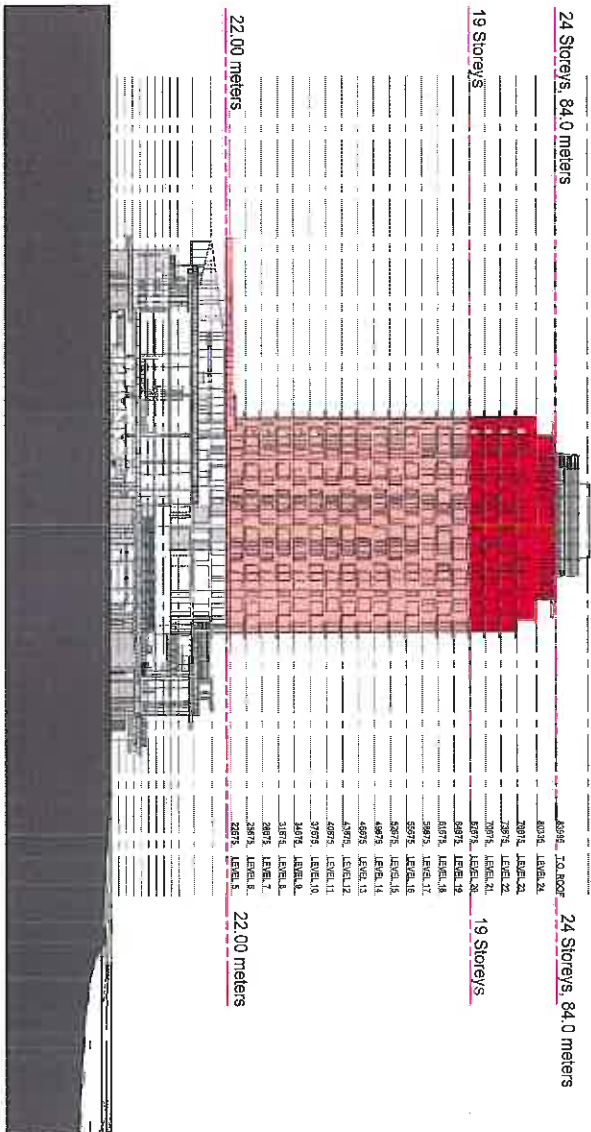
- Areas representing encroachment into 22.0m maximum building height.
- Areas representing encroachment above 19 Storey in building height.

Bylaw Reference: **Consolidated Zoning Bylaw No. 9000, C7 - Central Business**

Commercial, 14.7.8 Development Regulations, Section (a):
"The maximum height is 22.0 m except in Area 1 and Area 2, as shown on C7 - Diagram A and Diagram C, attached to this Bylaw."

City of Kelowna
Official Community Plan:
my Downtown, Feb. 2011, POLICIES, Section 15, Willow Inn Site:
"Allow up to 19 storeys on the former Willow Inn site at the corner of Queensway Avenue and Mill Street upon demonstration that the proposed development gives careful consideration to view impacts from other parts of downtown, is a signature landmark building and that it meets a high standard of design excellence."

Elevation Diagram



C7 Proposed Building Height Variances

- ① Proposed Tower Height for Hotel Tower:
to vary the maximum building height for Tower from 22 m to 81.6m



C7 - Proposed Variances: Building Height
Kelowna Downtown Hotel
Kelowna, BC

A-42
2014/02/04
PROJECT 160013





Grant Thornton

An instinct for growth™

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Attn: Gail Temple, Director of Development

January 30, 2014

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Dear Gail

Re: Economic impact of the proposed downtown hotel

As requested, we have prepared an update of the economic impact calculations, from the March 2010 Economic Impact Study Report.

The following points summarize why a new full service hotel for Downtown Kelowna should be considered:

- **The City of Kelowna continues to grow as a regional centre** for the Okanagan and as a tourist destination within BC. However, there are opportunities to further develop the area as a premier tourism and conference destination. To re-establish the downtown area as a vibrant part of the city, a number of key facilities and amenities are required; one of which is a high-quality hotel, with meeting and conference space.
- **Strong demand for accommodation.** The accommodation market in Kelowna has performed well over the last three years, with growth of 9.7% in annual occupancy and 5.1% in average room rates.
- **Opportunities for a new hotel downtown.** There is an opportunity to target a segment of the hotel market (a higher end market) which is not adequately served, in Kelowna. In addition, Kelowna is currently missing out on larger meetings and conferences because it does not have the room base in one hotel or in adjacent hotels to accommodate all attendees.
- **Challenges for the developer.** Although market research indicates strong demand for quality hotel accommodation in Kelowna, there are three key challenges facing the developer:
 - i There are very few good sites available for a high quality hotel development in Kelowna (the subject site is the “best”);
 - ii The development costs will be significant (±\$65 million); and,

- iii Financing for new hotels is extremely difficult to access in today's market environment, and as the size and cost of the project increase, so does the cost of borrowing.
- **Positive economic impact**, for Kelowna and Okanagan region.
 - i **The one-time employment and economic impacts** to be generated during the construction phase are as follows:

Construction Phase Impacts: \$66.45 Million Development Costs

Total Impacts	Employment		GDP	Output
	Jobs	(FTEs)	(\$ Million)	(\$ Million)
Direct	336	252	12.74	66.45
Indirect	250	187	13.02	33.71
Induced	135	101	6.45	11.96
Total	721	540	32.41	112.12

- ii **The annual operation of the hotel is expected to generate employment and economic benefits on an ongoing basis.** The estimated, annual employment and economic impacts are presented below.

Annual Operating Impacts

Total Annual Impacts	Employment		GDP	Output
	Jobs	(FTEs)	(\$ Million)	(\$ Million)
Direct	218	164	6.2	11.2
Indirect	57	42	2.8	4.5
Induced	24	18	1.1	2.0
Total	299	224	10.1	17.7

- iii Visitor expenditures in Kelowna by guests of the proposed hotel are expected to total \$20.8 million, annually or \$297 per day per guest. These expenditures are anticipated to generate additional employment opportunities and tax revenues.

If you have any questions which arise from the enclosed information, please feel free to contact us.

Yours sincerely



Doug Bastin, CMC
Partner, Grant Thornton Consulting

Mill Street Hotel, Kelowna, BC Transportation Impact Assessment

Draft Report

Prepared for
Westcorp

Date
March 7, 2014

Prepared by
Bunt & Associates

Project No.
4652.08

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1. INTRODUCTION

Westcorp is proposing the redevelopment of 235 Queensway Avenue, 1470, Water Street and 1476 Water Street in the downtown area of Kelowna BC. Proposed is the construction of a hotel with supporting amenities such as a restaurant, a coffee shop, commercial units and conference space. Bunt & Associates has prepared a Transportation Impact Assessment for the mix of land uses proposed. The location of the site is illustrated in **Exhibit 1.1**. The Study Area is shown in greater detail in **Exhibit 1.2**.

For the purposes of this study, it is assumed that the development will consist of 214 hotel rooms, 680 m² (7,319 square feet) of restaurant and cafe space, 1,684 m² (18,126 square feet) of conference space, and four Commercial Retail Units that total 378 m² (4,069 square feet).

This Transportation Impact Assessment Study will review the following:

- The Study Area's existing road network and transportation infrastructure;
- Estimates of the development's traffic generation and the subsequent impact to the adjacent road network;
- Site design, including multi-modal access; loading and pick-up/ drop off requirements;
- On-site parking supply and parking demand forecasts; and,
- Transportation demand management strategies.

Exhibit 1.1: Site Location and context

Exhibit 1.2 Detailed Site Location

2. BACKGROUND

The City of Kelowna is actively revitalizing its downtown, which includes various changes to the transportation network. Alterations to the local road network include the revitalization of Bernard and Queensway Avenues, and the closure of Mill Street to vehicular traffic. The locations of these changes are shown in Exhibit 1.2.

The Ministry of Transportation and Infrastructure (MoTI) approved the closure of Mill Street as part of Kelowna's Downtown Revitalization CD-21 zone area, which is bounded by: Harvey Avenue, Abbott Street, Water Street and Queensway Avenue, commonly referred to as "Waterfront Village Redevelopment". The City of Kelowna approved the closure of Mill Street in November of 2012. The Mill Street closure is meant to increase the walkability of the local area and to expand and promote the adjacent lakefront public park space.

Queensway Avenue, which is to the immediate north of the site, is scheduled for redevelopment in the summer of 2014. Alterations include the addition of traffic circles at its Water Street intersection as well as at Queensway Avenue's west termination. The redevelopment of Queensway Avenue will result in a loss of on-street parking spaces; these spaces will be compensated with the addition of parkade parking spaces, in particular the proposed Interior Health parkade at 1430 Ellis Street.

Further changes include the addition of 1,000 Interior Health employees with an office building at Ellis Street & Doyle Avenue together with a new parkade facility on Ellis Street. Also, the existing parking lot at the west end of Queensway Avenue is identified as a redevelopment site for Kelowna's visitor centre.

3. EXISTING CONDITIONS

3.1 Existing Street Network

The following provides a description of the existing Study Area street network, where roadway function, design characteristics and intersection controls, in particular, are discussed within the following report.

- Mill Street & Queensway Avenue (eastbound stop control);
- Mill Street & Bernard Avenue (southbound stop control);
- Water Street & Doyle Avenue (east and westbound stop control);
- Water Street & Queensway Avenue (currently stop control, modelled as single lane roundabout);
- Water Street & Lane (eastbound stop control); and,
- Water Street & Bernard Avenue (signalized).

The development site, as illustrated in Exhibit 1.2, is bounded by the existing Mill Street alignment to the west, Queensway Avenue to the north, Water Street to the east, Bernard Avenue to the south and a laneway along its south edge.

Mill Street is a two way road. It has a four lane cross section with a travel lane in each direction and metered parking along each side. It borders Kerry Park to its west, which is part of the lakefront walkway.

Queensway Avenue is a two way road. It has an approximate 25 metre width. Between Water Street and Pandosy Street, Queensway Avenue is comprised of (from its south edge to north edge) angled parking, a eastbound travel lane, a median, angled parking for westbound vehicles, a westbound travel lane then a parallel parking lane along the north edge. East of Pandosy Street, Queensway Avenue presently operates as a transit bus loop; the main bus exchange for buses heading to the Westside, Dilworth, Mission and Rutland. West of Mill Street, Queensway Avenue becomes a parking lot for the waterfront area and adjacent parks. The Queensway bus loop is scheduled for redevelopment in 2014. In the future plans, Queensway Avenue will only accommodate cars between Water and Pandosy, one-way, eastbound. Similarly, between Pandosy Street and the lane south of Queensway Avenue, it will be one-way to cars southbound, though buses can travel northbound. Parking on the south side of Queensway Avenue will be eliminated, though it will be retained on the north. Buses are to enter the roundabout from the east.

Bordering the south end of the study site, Bernard Avenue is an east/west route connecting to City Park. Bernard operates as a two-way street with non-metered angled parking along both curbs. Bernard Street is undergoing development, and will change from a four to a three lane cross section. This redevelopment of the street will allow for wider sidewalks and other pedestrian improvements, including increased street furniture and vegetation. The Bernard Avenue development is currently scheduled for completion in June 2014.

The laneway which directly borders the site (to the south of the site) functions as a one-way eastbound route. The lane is primarily used for loading activity and parking.

The Mill Street & Queensway Avenue intersection currently operates under eastbound stop control, with a zebra pedestrian crossing on the east leg of the intersection.

The Mill Street & Bernard Avenue intersection operates under southbound stop control, with a zebra pedestrian crossing for north and southbound pedestrian movements (west and east legs of the intersection).

The Water Street & Queensway Avenue intersection currently operates under east and west leg stop control. There is a pedestrian crossing at each leg of the intersection, and zebra markings on the north and south legs. As mentioned above, this intersection planned for conversion to a single lane traffic roundabout later in 2014.

The Water Street & Bernard Avenue intersection operates under signalised control, with pedestrian crossing push buttons at each leg.

The Lane to the south of the site operates with two way travel; however the adjacent Lane to the east of Water Street operates with just one-way eastbound travel.

3.2 Existing Traffic Volumes and Operations

Existing Traffic volumes were obtained from AM and PM Peak hour counts collected by the City of Kelowna and augmented with a spot traffic counts conducted by Bunt & Associates. The study intersections, collection data and source are summarised in **Table 3.1**.

Table 3.1: Study Intersections and Data Source

Intersection	Survey Date	Source
Water Street & Doyle Avenue	July 18 th , 2012	City of Kelowna
Water Street & Queensway	November 10, 2011	City of Kelowna
Water Street & Site Lane	February 6 th , 2012	Bunt Spot Count
Bernard Avenue & Water Street	November 10, 2011	City of Kelowna
Bernard Avenue and Mill Street	November 10, 2011	City of Kelowna

These peak hour conditions were factored up to match the July 2012 summer volumes, in order to provide a conservative estimate of peak summer period traffic operations. The non-summer period traffic volumes were factored up by 12% and 58%, for the AM and PM respectively.

The 2012 summer traffic volumes were factored up by a 1.4% annual growth rate to represent 'existing summer 2013' volumes.

The Weekday AM and PM peak hour periods were analyzed. Saturday volumes were not included in the analysis as review of MOTI data sets from the area (south end of Okanagan Lake Bridge) from the past few years indicate that Saturday peak hour volumes are significantly lower than Weekday PM peak hour volumes.

The operation of the study area intersections were examined using the described summer period peak traffic volumes and Synchro 6 software to assess existing traffic conditions for the Weekday AM and PM peak hour periods.

Analysis is based on the procedures outlined in the 2000 Highway Capacity Manual (HCM). The reported performance criteria includes a measure of the traffic volume to capacity (v/c) ratio, and a traffic delay based Level of Service (LOS) measure at each study intersection. A v/c ratio less than 0.90 indicates acceptable traffic conditions, with sufficient capacity to accommodate demands; a v/c ratio between 0.90 and 0.95 indicates a near-capacity traffic condition with considerable delays and vehicle queuing. A v/c ratio over 0.95 indicates that traffic conditions are close to saturated or saturated, and traffic demand exceeds the available capacity. The LOS rating is based on average vehicle delay and ranges from "A", which represents minimal delay conditions; to "F", which represents congestion and/ or considerable delays. For urban areas, a LOS of "E" or better and a V/C ratio of 0.90 or less generally represent acceptable traffic performance.

Tables 3.2 and 3.3 present a capacity analysis of the examined intersections during Weekday AM and PM existing (2013) peak hour conditions. It is noted that the Water Street & Queensway Avenue intersection was modelled as a roundabout due to the approved plans to implement a roundabout at this location.

Table 3.2: Existing (2013) Traffic Conditions - Weekday Peak AM

Intersection	Overall Intersection			Critical Movement			
	Average Delay (sec.)	V/C	LOS	Movement	V/C	LOS	Delay (sec.)
Mill & Bernard (Southbound Stop Control)	2.7	-	A	Southbound	0.09	A	9.4
Water & Queensway (Roundabout Control)		0.34	A	Northbound	0.34	-	-
Water & Lane (Eastbound Stop Control)	0.3	-	A	Eastbound	0.02	B	11.8
Water & Bernard (Signalized)	9.3	0.37	A	Eastbound	0.40	B	10.5
Doyle & Water (East and Westbound Stop Control)	2.1	-	A	Westbound	0.15	C	17.3

Table 3.3: Existing (2013) Traffic Conditions - Weekday Peak PM

Intersection	Overall Intersection			Critical Movement			
	Average Delay (sec.)	V/C	LOS	Movement	V/C	LOS	Delay (sec.)
Mill & Bernard (Southbound Stop Control)	5.9	-	C	Southbound	0.49	B	15.0
Water & Queensway (Roundabout Control)	-	0.57	B	Northbound	0.57	-	-
Water & Lane (Eastbound Stop Control)	0.4	-	A	Eastbound	0.06	C	17.4
Water & Bernard (Signalized)	11.5	0.60	B	Southbound	0.68	B	14.2
Doyle & Water (East and Westbound Stop Control)	7.7	-	B	Westbound	0.63	E	43.8

The intersection of Queensway Avenue & Water Street was also analyzed using Sidra Intersection 6, a micro-analytical, network analysis tool. Sidra Intersection allows for a more accurate analysis of single-lane and multi-lane roundabouts via its combined (hybrid) geometry and gap-acceptance modelling approach in order to take into account the effect of roundabout geometry on driver behaviour.

The Sidra output for Water Street and Queensway is summarized in **Table 3.4**

Table 3.4: Existing (2013) Queensway Avenue & Water Street: Sidra Output

Period	Overall Intersection			Critical Movement			
	Average Delay (sec.)	V/C	LOS	Movement	V/C	LOS	Delay (sec.)
AM	5.5	0.30	A	Westbound	0.06	B	10.6
PM	6.3	0.50	A	Northbound	0.43	A	5.5

As suggested by Tables 3.2, 3.3, and 3.4 all Study Area intersections currently (Summer 2013) operate within capacity during the most critical Weekday AM and PM peak hour periods. The intersection of Doyle Avenue & Water Street nears capacity for the westbound movement in the PM peak hour period. This is due to the substantial opposing north and southbound volumes, which are present especially in the summer months.

3.3 Walking

Typically, people are willing to walk 5 to 10 minutes for daily trips (transit, shopping, etc), which translates to approximately 400m to 800m in distance.

Exhibit 3.1 shows the extent of downtown Kelowna that is within this 800m area from the proposed site. This larger 800m zone represents a typical distance people are usually willing to walk as a commute or as a component of a commute, to a transit station. Clearly, there is a significant area of downtown Kelowna that lies within a reasonable walking distance for future hotel guests, employees and visitors. A wide range of amenities are also within reasonable walking distance, with pedestrian facilities provided on all streets in vicinity of the site location (crosswalks, paths and sidewalks).

Walk Score is a method of evaluating a location’s walkability by using an algorithm that awards points based on the distance to amenities such as grocery stores, schools, shops, recreation opportunities, banks and restaurants. The Mill Street development site receives a 100 score out of 100 placing it in the ‘Walkers Paradise’ category.

There are various downtown improvement projects, at various stages of approval or completion which will further improve the existing pedestrian environment. These include Stuart Park Phase 2 redevelopment, the Downtown pier/marina and the closure of Mill Street to vehicle traffic as previously discussed.

3.4 Cycling

Exhibit 3.1 also shows the cycle route facilities in the context of the development site. The site is well-connected with an off-road, paved bike lane which runs along the lakefront. Additionally, it is within 800m to the main north-south cycle corridor, the Richter Street bike lane and the Cawston Avenue off-road paved, east-west cycling route.

3.5 Transit

The proposed site is located approximately 200m from the Queensway Bus Exchange. The existing Queensway Exchange is a significant destination and transfer point in downtown Kelowna. **Table 3.5** summarizes the bus transit routes it currently services. It is also the terminus for the RapidBus service implemented as phase I of the RapidBus project.

Table 3.5: Bus Transit Service

Bus Route	Destination	Duration	AM Peak Frequency	PM Peak Frequency	SAT Frequency
1	Mission Recreation Exchange	25	15	15	30
2	Ellis St & Cambridge Ave	5	15-55	60	60
6	NA				
7	Orchard Park Mall	30	17	15-30	30
9	Orchard Park Mall	20	NA- Shopper Shuttle	NA- Shopper Shuttle	65
10	Fitzpatrick & Findlay Rd	40	15	15	30
11	Craig Rd & McCurdy Rd	30	15	15	60

Exhibit 3.1 Pedestrian and Cycling Access

4. DEVELOPMENT PLAN

4.1 Proposed Development Content

The proposed development's main land use consists of hotel units. The hotel is developed with supporting amenities including a restaurant, a coffee shop, conference space and a spa. The development also includes 4 small retail units. Eight of the hotel units may service long term stays, for the purpose of this report, due to their size and expected function they are grouped into the hotel component of the development. The proposed development is summarized in **Table 4.1**.

Table 4.1: Proposed Development Content

Land Use	Quantity (sq.m. or units)
Hotel	214 rooms
Restaurant	473 m ²
Coffee Shop	207 m ²
Conference	1,684 m ²
Retail (four units)	378 m ²

The four retail units will front Water Street. The remainder of the land uses including the coffee shop and restaurant will be within the hotel complex. Parking for all components of the development will be within the parkade structure. Five of the hotel parking spaces are within its porte cochere.

4.2 Access

The most recent site plan for the project is illustrated in **Exhibit 4.1**, shows vehicular access to the development and its parkade from Queensway Avenue. Earlier development schemes had a higher portion of residential units which were to be accessed from the rear lane south of the site which connects with Water Street; this is no longer the case alleviating potential issues with left movements to and from the lane and Water Street. The lane will remain open to permit loading activity for the proposed development as well as the existing adjacent commercial land uses to the south of the lane, e.g. Kelly O'Brien's Pub.

The hotel's drop off zone or porte cochere is also located along Queensway Avenue. The porte cochere zone and valet will provide the temporary parking needs of approximately five vehicles, plus curbside loading for passengers.

Exhibit 4.1 Site Plan and access

5. TRAFFIC IMPACT ANALYSIS

5.1 Development Trip Generation

Traffic generation was estimated using Institute of Transportation Engineers (ITE) trip generation rates in conjunction with observed vehicle assignment patterns. Trip generation estimates of the proposed development calculated with ITE trip rates are provided in **Table 5.1**.

ITE rate 310 (Hotel) was used instead of the Resort Hotel category to account for the development's conference facility. A 20% discount was applied to the ITE hotel rates to account for the downtown location of the hotel. This 20% deduction is consistent with the parking allocation of the development where parking bylaw reductions are permitted for Kelowna's downtown region.

The proposed restaurant and coffee shop were grouped together and vehicle trips estimate by applying the ITE trip rate reported for quality, sit down style restaurant land use. The predicted 30 or so vehicle trips in the PM peak hour period is consistent with parking demand estimates for this type of use within a hotel setting (9 parking spaces with an approximately three to four total in and out trips per parking spaces per peak hour). In the AM peak hour period approximately 3 trips are estimated. This is considered realistic for a weekday morning as most of the patrons are expected to be guests of the hotel, or people who work in the near vicinity.

Although the Saturday peak hour period is not examined in this traffic impact analysis, the trip generation estimates for the Saturday mid-day peak hour period are presented for comparison purposes.

Table 5.1: Development Site Trip Generation

Use	Size (Sq.ft.) or (Units)	Peak Hour	Source	Trip Rate (per 1,000 sq.ft or per unit)	% Trips IN	% Trips OUT	# Trips IN	# Trips Out	Total Peak Hour 2-Way Trips
Hotel Rooms	214 units	AM	ITE (310)	0.42	59%	41%	53	37	90
		PM		0.48	51%	49%	61	42	103
		Sat		0.58	56%	44%	69	55	124
Hotel Restaurant & Coffee Shop	7,319 sq.ft. (680m ²)	AM	ITE (931)	0.41	50%	50%	2	1	3
		PM		3.75	67%	33%	18	9	27
		Sat		5.41	59%	41%	23	17	40
Hotel Conference	18,126 sq.ft. (1684m ²)	AM	Included in Hotel Rate*	-	-	-	0	0	0
		PM		-	-	-	0	0	0
		Sat		-	-	-	0	0	0
Commercial Retail Units	4,069 sq.ft. (378m ²)	AM	ITE (826)-	0.70**	66%	33%	2	1	3
		PM		2.71	44%	56%	5	6	11
		Sat		2.71***	50%	50%	6	5	11
Total		AM					57	39	96
		PM				84	57	141	
		Sat				98	77	175	

*ITE 310 (Hotel) description, "supporting facilities such as meeting and banquet rooms or convention facilities".

** There is no AM rate for ITE 826, therefore rate was derived from ITE Shopping Centre 820.

*** There is no Saturday rate for ITE Specialty Retail 826 therefore the PM rate was used as an approximation.

As presented in Table 5.1 the proposed development is expected to generated approximately 96, 141 and 175 total two-way trips per Weekday AM, Weekday PM, and Saturday peak hours, respectively.

5.2 Future Traffic Conditions

Future traffic analysis examined the traffic impact of the development during Opening Day (2015) and Opening Day plus 10 year (2025). A 1.4% annual growth rate was applied to existing volumes to forecast future background traffic volumes. The assumed 1.4% rate results in future Year 2030 traffic volumes on Water Street similar to those predicted by the EMME regional transportation model used by the City of Kelowna.

5.2.1 Future Road Network Changes

As mentioned prior, the Water Street & Queensway Avenue intersection is to be converted to a single lane roundabout. All background and total future scenarios model this intersection with the roundabout configuration. All future traffic scenarios were also modelled assuming Mill Street is closed to vehicular traffic. Existing traffic using Mill Street was largely reassigned through the Bernard Avenue & Water Street intersection.

5.2.2 Trips Distribution and Assignment

The assumed vehicle trip distribution for the proposed development is based mainly on existing vehicle volume patterns in this part of the downtown area. Expected distribution to and from the hotel's access is based on distribution assumptions summarized in **Table 5.2**. **Tables 5.3 to 5.10** present the forecasted Background 2015, 2025 Weekday AM and PM peak hour operating conditions.

Table 5.2: Estimated Distribution of Site Generated Traffic

Direction To/From	AM Peak Hour Outbound/Inbound	PM Peak Hour Outbound/ Inbound
North	42%/39%	42%/36%
South	28%/30%	35%/21%
East	19%/11%	13%/18%
West	11%/20%	10%/25%

Table 5.3: 2015 Background Traffic Conditions - Weekday Peak AM

Intersection	Overall Intersection			Critical Movement			
	Average Delay (sec.)	V/C	LOS	Movement	V/C	LOS	Delay (sec.)
Water & Queensway (Roundabout)	-	0.28	A	Northbound	0.34	-	-
Water & Lane (Eastbound Stop Control)	0.3	-	A	Eastbound	0.02	B	12.0
Water & Bernard (Signalized)	9.4	0.38	A	Eastbound	0.41	B	10.7
Doyle & Water (East and Westbound Stop Control)	2.1	-	A	Westbound	0.13	C	17.9

Table 5.4: 2025 Background Traffic Conditions - Weekday Peak AM

Intersection	Overall Intersection			Critical Movement			
	Average Delay (sec.)	V/C	LOS	Movement	V/C	LOS	Delay (sec.)
Water & Queensway (Roundabout)	-	0.39	A	Northbound	0.39	-	-
Water & Lane (Eastbound Stop Control)	0.4	-	A	Eastbound	0.03	B	12.9
Water & Bernard (Signalized)	9.8	0.42	A	Eastbound	0.48	B	11.5
Doyle & Water (East and Westbound Stop Control)	2.3	-	A	Westbound	0.17	C	21.1

Table 5.5: 2015 Total Traffic Conditions - Weekday Peak AM

Intersection	Overall Intersection			Critical Movement			
	Average Delay (sec.)	V/C	LOS	Movement	V/C	LOS	Delay (sec.)
Water & Queensway (Roundabout)	-	0.37	A	Northbound	0.37	-	-
Water & Lane (Eastbound Stop Control)	0.3	-	A	Eastbound	0.03	C	12.4
Water & Bernard (Signalized)	9.6	0.40	A	Eastbound	0.44	B	8.7
Doyle & Water (East and Westbound Stop Control)	2.2	-	A	Westbound	0.15	C	19.3

Table 5.6: 2025 Total Traffic Conditions – Weekday Peak AM

Intersection	Overall Intersection			Critical Movement			
	Average Delay (sec.)	V/C	LOS	Movement	V/C	LOS	Delay (sec.)
Water & Queensway (Roundabout)	-	0.43	A	Northbound	0.43	-	-
Water & Lane (Eastbound Stop Control)	0.4	-	A	Eastbound	0.03	B	13.5
Water & Bernard (Signalized)	10.1	0.46	A	Eastbound	0.51	B	12.1
Doyle & Water (East and Westbound Stop Control)	2.5	-	A	Westbound	0.20	C	23.1

Table 5.7: 2015 Background Traffic Conditions – Weekday Peak PM

Intersection	Overall Intersection			Critical Movement			
	Average Delay (sec.)	V/C	LOS	Movement	V/C	LOS	Delay (sec.)
Water & Queensway (Roundabout)	-	0.55	B	Southbound	0.55	-	-
Water & Lane (Eastbound Stop Control)	0.4	-	A	Eastbound	0.06	C	18.0
Water & Bernard (Signalized)	11.7	0.62	B	Southbound	0.70	B	16.0
Doyle & Water (East and Westbound Stop Control)	8.2	-	B	Westbound	0.65	E	72.0

Table 5.8: 2015 Total Traffic Conditions - Weekday Peak PM

Intersection	Overall Intersection			Critical Movement			
	Average Delay (sec.)	V/C	LOS	Movement	V/C	LOS	Delay (sec.)
Water & Queensway (Roundabout)	-	0.61	B	Southbound	0.61	-	-
Water & Lane (Eastbound Stop Control)	0.4	-	A	Eastbound	0.07	C	19.7
Water & Bernard (Signalized)	12.5	0.64	B	Southbound	0.75	B	17.9
Doyle & Water (East and Westbound Stop Control)	9.9	-	B	Westbound	0.75	F	94.2

Table 5.9: 2025 Background Traffic Conditions - Weekday Peak PM

Intersection	Overall Intersection			Critical Movement			
	Average Delay (sec.)	V/C	LOS	Movement	V/C	LOS	Delay (sec.)
Water & Queensway (Roundabout)	-	0.64	B	Southbound	0.64	-	-
Water & Lane (Eastbound Stop Control)	0.5	-	A	Eastbound	0.10	C	22.4
Water & Bernard (Signalized)	14.2	0.73	B	Southbound	0.83	C	22.5
Doyle & Water (East and Westbound Stop Control)	19.7	-	C	Westbound	1.04	F	187.0
				Eastbound	0.77	F	94.5

Table 5.10: 2025 Total Traffic Conditions – Weekday Peak PM

Intersection	Overall Intersection			Critical Movement			
	Average Delay (sec.)	V/C	LOS	Movement	V/C	LOS	Delay (sec.)
Water & Queensway (Roundabout)	-	0.71	C	Southbound	0.71	-	-
Water & Lane (Eastbound Stop Control)	0.6	-	A	Eastbound	0.11	D	25.2
Water & Bernard (Signalized)	16.8	0.78	B	Southbound	0.90	C	30.6
Doyle & Water (East and Westbound Stop Control)	25.6	-	C	Westbound	1.32	F	305.2
				Eastbound	0.67	F	69.0

The Intersection of Water Street & Queensway Avenue was further analyzed using Sidra software, as in the existing conditions, to confirm its operation performance. The summary of the highest volumes scenario, 2025 PM, is shown in **Table 5.11**.

Table 5.11: 2025 Background & Total Traffic Conditions – Queensway & Water Street, Sidra Analysis

Intersection	Overall Intersection			Critical Movement			
	Average Delay (sec.)	V/C	LOS	Movement	V/C	LOS	Delay (sec.)
PM Background 2025	5.9	0.51	A	Northbound	0.51	A	5.7
PM Total 2025	6.6	0.58	A	Northbound	0.58	A	6.1

In summary, the AM peak hour traffic analysis indicates the background and total traffic operations, for both horizon years, operate well within capacity.

In the PM peak hour traffic analysis the background and total traffic operations, for both horizon years, operate well within capacity, with the exception of Doyle Avenue & Water Street and the southbound movement at Bernard Avenue & Water Street. Mitigation measures for each intersection are provided below.

5.2.3 Bernard Avenue & Water Street Mitigation

Bernard Avenue & Water Street operational issues, forecasted to occur during the 2025 Total condition scenario, are mitigated through optimization of the traffic signal's cycle length and signal splits. With signal optimization the overall performance during the 2025 Total Weekday PM peak hour is forecast to have a V/C ratio of 0.77, an average delay of 15.6 seconds and LOS B. For the most pressurized southbound movement the V/C ratio is forecast at 0.78 with an 18.8 second delay and LOS B.

5.2.4 Doyle Avenue & Water Street Intersection Mitigation

For the Doyle Avenue & Water Street intersection the following mitigation measure options were examined using the AM and PM Background and Total 2025 volumes:

- Four leg stop control;
- Roundabout; and
- Addition of a traffic signal.

The results of the alternative traffic control types at the Doyle/Water intersection are summarized in **Table 5.12** for the Background and Total 2025 forecasted traffic volumes.

Table 5.12: 2025 Background Traffic Conditions – Doyle & Water

Intersection	Overall Intersection			Critical Movement			
	Average Delay (sec.)	V/C	LOS	Movement	V/C	LOS	Delay (sec.)
<i>AM Background 2025</i>							
Four leg Stop Control	13.0	-	B	Northbound	-	B	14.5
Roundabout	-	0.42	A	Northbound	0.42	-	-
Signalization	5.5	0.34	A	Westbound	0.36	C	30.5
<i>AM Total 2025</i>							
Four leg Stop Control	14.0	-	A	Northbound	-	C	15.9
Roundabout	-	0.44	A	Northbound	0.44	-	-
Signalization	5.5	0.36	A	Westbound	0.38	C	30.4
<i>PM Background 2025</i>							
Four leg Stop Control	66.7	-	F	Southbound	-	F	101.3
Roundabout	0.66	-	C	Southbound	0.66	-	-
Signalization	8.2	0.55	A	Westbound	0.40	B	19.0
<i>PM Total 2025</i>							
Four leg Stop Control	81.9	-	F	Southbound	-	F	122.9
Roundabout	0.69	-	C	Southbound	0.69	-	-
Signalization	8.4	0.57	A	Westbound	0.41	B	19.6

Signalization was coded as semi actuated, uncoordinated.

Analysis was again conducted using Synchro 6 software, the roundabout analysis was confirmed using Sidra software. The Sidra analysis confirmed the ability of the proposed roundabout to accommodate the future forecasted scenarios. More specifically, the overall intersection traffic operation was determined to operate with the following V/C ratios and LOS during the background and total 2025 Weekday PM time periods:

- Background 2025: LOS A, V/C - 0.54.
- Total 2025: LOS A, V/C -0.57.

5.3 Traffic Operation Summary

All intersections analysed are expected to continue operating well within capacity into the future development scenarios, with the exception of the Doyle Avenue & Water Street and Water Street & Bernard Avenue intersections.

The Water Street & Bernard Avenue intersection analysis suggested the southbound movement would encounter a V/C ratio of 0.90 in the Total Weekday PM 2025 scenario. Optimization of the signal cycle time and signal phasing provides for a reduction of the predicted V/C ratio for the southbound movement at Water Street & Bernard Avenue by 12% to 0.78.

The Doyle Avenue & Water Street intersection's west leg encounters significant delays as those vehicle wait to turn onto Water Street. This condition is expected to be more prevalent during the peak summer periods. Mitigation measures examined for the Doyle Avenue & Water Street intersection suggest consideration of either a roundabout or the introduction of a traffic signal. This issue was found to be present during the future background scenarios and is only slightly impacted by the proposed development (during the peak hour period the proposed development is expected to add approximately four westbound to southbound vehicle trips per hour).

The proposed introduction of a traffic circle at the Water Street & Queensway Avenue intersection (as currently planned) will help relieve existing intersection delays at this intersection and maintain minimal south leg queues on Water Street (Sidra forecasted 95th percentile existing peak hour northbound queues of approximately 17m).

In summary, the development is shown to result in minimal traffic impact to the adjacent road network for the different time periods and future horizon years considered by the analysis.

6. PARKING

The Mill Street Hotel site plan accommodates 242 on-site parking spaces, the majority of which are to be located in an above grade parking structure with driveway access to Queensway as shown in Exhibit 4.1.

The reconfiguration of Queensway west of Water Street does eliminate existing street parking. The proposed Mill Street hotel development has committed to securing 22 parking spaces within the new parkade for public use to compensate for this loss of existing street parking.

6.1 Zoning Bylaw Parking Requirements

The City of Kelowna Zoning Bylaw sets out the off-street parking supply requirements for a broad range of land use types. Specifically for the C7 zone in the downtown area, the Bylaw parking supply requirements for the proposed development are as listed in **Table 6.1**.

Table 6.1: Bylaw Required Parking Summary

Mill Street Development - City of Kelowna Bylaw Required Parking Summary				
Land Use	Rate	GFA	Req. Spaces	Notes
Restaurant	1.3 per 100m ²	473 m ²	6.2	C7 Zone Parking Requirement
Coffee Shop	1.3 per 100m ²	207 m ²	2.7	C7 Zone Parking Requirement
Conference	1.3 per 100m ²	1,684 m ²	21.9	C7 Zone Parking Requirement
Hotel	1 per room	214 rooms	214	As directed by the City of Kelowna
Retail (four units)	1.3 per 100m ²	378 m ²	4.9	C7 Zone Parking Requirement
		Required	250	Round up as per bylaw
		Provided	242	
		Difference	-8	8 stall parking deficiency

Direct application of the Bylaw parking supply standards indicates that 250 off-street parking spaces are required for the proposed development. The proposed on-site parking supply is 242 spaces which is eight (8) parking spaces short of the Bylaw requirement.

6.2 Parking Demand Analysis

Recommended parking rates for the proposed development are provided below. These recommended rates are based on Bunt & Associates' parking database information for urban development of the type contemplated with the Mill Street redevelopment project.

6.2.1 Transportation Options

Parking demand for any development is significantly influenced by the transportation options available for the residents, employees, visitors and customers. As discussed in Section 3 of this report, the proposed Mill Street redevelopment project is very well situated in the downtown area of Kelowna with convenient access to multiple transit routes as well as pedestrian and cycling connections.

As discussed in Section 3.5 the site is well connected to transit, including connections with the airport (approximately 50 minute connection to downtown using BC Transit routes 23 and 97, these routes run every 15 to 30 minutes 5:30 am to 6:50 pm), and is within walking distance of all major amenities within Kelowna's downtown.

The hotel will also provide patrons with rental car services, which can help reduce private vehicle dependence.

6.2.2 Hotel Parking Demand

The City of Kelowna Zoning Bylaw parking supply requirement for hotel land use is one (1) stall per guest room whether located downtown or in more suburban locations. In our opinion a more appropriate parking rate for the hotel component of the project in this well accessed downtown location is 0.65 spaces per guest room. For the proposed 214 hotels rooms, this translates to a peak parking demand of 139 vehicles which would typically occur during the early morning period as hotel staff arrive before the overnight guests depart.

It is not uncommon for municipalities to differentiate between the downtown area and non downtown areas for certain land use types including hotels, recognizing that guest staying in downtown hotels are somewhat less dependent on automobile transportation for access to/from the facility and during their stay. For instance, the City of Vancouver Parking Bylaw minimum parking supply requirement for downtown hotels is 0.3 stalls per sleeping unit or 60% of the otherwise required 0.5 spaces per sleeping unit for hotels outside of downtown area.

In addition to the availability of taxi and shuttle bus connections to the Kelowna International Airport, it is noted that BC Transit routes 23 and 97 provide public transit access between the airport and downtown at 15 to 30 minute service headways. Moreover, the proposed new hotel is planning to provide an on-site car rental service for guests that would further reduce the need for private vehicle access and parking.

6.2.3 Commercial Uses Parking Demand

On account of the downtown location of the commercial uses planned for the project including the restaurant, coffee shop, the four small retail units along Water Street and the conference centre, the City of Kelowna has confirmed that these uses will be assessed the 1.3 parking spaces per 100m² of floor area C7 Zone Bylaw parking supply rate.

6.2.4 Shared Parking Opportunity

The different uses proposed for the development will have different peak parking demand patterns through the day. In particular, the peak parking associated with the hotel is typically during the early morning period as hotel staff arrives to the hotel before the guests begin to depart. During this hotel peak parking period, the other commercial uses on the site will typically have minimal if any parking activity.

According to the ITE Parking Generation Manual, hotel parking demand during the mid-day period is typically down to about 60-70% of its early morning peak. With an estimated peak parking demand of 139 parked vehicles during the early morning period, the mid afternoon parking demand for the hotel component would decrease to between 95-100 parked vehicles.

Table 6.2 on the following page provides a summary of the anticipated parking demands for the different component uses during the morning, mid afternoon, and evening periods. This analysis indicates that the combined peak parking demand for this proposed collection of uses is anticipated to be 167 vehicles during the early morning period, 133 vehicles during the mid afternoon period, and 159 vehicles during the evening period. Should the parking demand associated with the other commercial uses planned for the project exceed the C7 Zone Bylaw parking supply rates, the parking demand figures would be somewhat higher than these estimates but still well less than the 242 stall parking supply to be provided.

With 242 parking spaces proposed for the development, and assuming the other commercial uses on the site generate parking demand consistent with the Bylaw supply rates, potentially between 75 to 109 surplus parking spaces will be available in the parkade at different times of the day. This surplus parking would be available for use to offload parking pressures elsewhere in vicinity of the development.

Table 6.2: Shared Parking Analysis

Estimated Parking Demand (Parked Vehicles)					
Component Use	GFA	Individual Peak Demand *	Morning	Afternoon	Evening
Restaurant	473 m ²	6	3	6	6
Coffee Shop	207 m ²	3	3	3	3
Conference	1,684 m ²	22	22	22	22
Hotel	214 rooms	139	139	97	125
Retail - 4 units	378 m ²	5	0	5	3
Totals	-	175 vehicles	167 vehicles	133 vehicles	159 vehicles

*Note: * The individual peak parking demands for the component land uses are the C7 Bylaw parking required parking except in the case for the hotel use where the 139 vehicle figure is based on the recommended 0.65 per guest room parking rate described in Section 6.2.2.*

7. LOADING

Loading vehicles will access the site from the rear lane, as shown in **Exhibit 7.1**.

The City of Kelowna Zoning Bylaw as applied directly to the proposed development would require a total of eight (8) loading bays (1 per 2,800 square metres of floor area). Based on our database information for loading requirements for the collection of uses proposed here, we believe that 8 loading bays would be excessive. In our opinion a total of two (2) loading bays would be sufficient and therefore the proposed three (3) loading bay configuration will be more than adequate to service the development.

Using the AutoTURN software package, Bunt conducted a vehicle turning path analysis to confirm the adequacy of the proposed loading court design to accommodate SU9 design vehicles (single unit box truck with an overall length of 30 feet). As shown in **Exhibit 7.1** all three loading bays are capable of accommodating the SU9 design vehicle.

Exhibit 7.1 Loading-AutoTURN Analysis

8. TRANSPORTATION DEMAND MANAGEMENT

The downtown context of the site is ideal for implementation of Transportation Demand Management (TDM) measures as it is located in a highly pedestrian friendly environment, close to transit, other shops and services, and eventually higher density residential development.

A key element of TDM programs for customers and employees of projects such as the proposed development is that alternative travel options be relevant, practical to implement and well advertised. The hotel can play an active role in informing their guests on which services are available locally and the best walking/ cycling routes to get there. This can easily be done in person, during the check in process or on the hotel's web site.

Additional information that should be provided on the hotel's website includes Kelowna transit routes, especially to the airport. Currently there is bus access to/from the airport via route 23 & route 97. This one transfer trip takes approximately 50 minutes and connects the airport directly with the Queensway exchange, which is adjacent to the site.

Given the hotel has a plan to offer a car rental directly onsite, this offers visitors flexibility to access a vehicle just when it is required.

We also believe it is prudent for the hotel to not oversupply parking. An oversupply of parking can encourage unnecessary vehicle use. In addition from a regional perspective a tighter downtown parking supply can help encourage commuters to use other forms of transportation by making the other modes such as transit more financially competitive.

To encourage cycling trips, particularly for employees of the hotel and other commercial uses planned for the project, end of trip bicycle facilities including bike storage as per the requirements of the City of Kelowna Zoning Bylaw, well as shower and change room facilities should be considered.

9. SUMMARY & CONCLUSIONS

9.1 Traffic Impact

The existing road network with the traffic study area presently operates within capacity during the weekday morning and afternoon peak traffic periods.

The proposed Mill Street hotel and commercial development is expected to generate approximately 96, 141 and 175 total two-way trips per Weekday AM, Weekday PM, and Saturday peak hours, respectively.

All intersections analysed with the traffic study area are expected to continue operating well within capacity into the future development scenarios, with the exception of the Doyle Avenue & Water Street and Water Street & Bernard Avenue intersections. Forecast growth in area traffic generally is anticipated to lead to capacity issues at both locations.

The Water Street & Bernard Avenue intersection analysis indicates that the southbound through traffic movement would encounter a volume to capacity (V/C) ratio of 0.90 for the Year 2025 Total Traffic condition (background traffic growth plus site traffic associated with the proposed development). Optimization of the signal time and phasing enabled the V/C of the southbound movement at Water Street & Bernard Avenue to be reduced to 0.78.

The Doyle Avenue & Water Street intersection's west leg encounters significant delays, particularly for eastbound vehicles attempting to turn onto Water Street. This condition is expected to be most acute during the peak summer periods. Mitigation measures examined for the Doyle Avenue & Water Street intersection suggest consideration of either a traffic roundabout or the introduction of traffic signal control. This issue was found to be present during the future background scenarios and is only slightly impacted by the proposed development (during the peak hour period the proposed development is expected to add approximately four westbound to southbound vehicle trips per hour).

The proposed introduction of a traffic circle at the Water Street & Queensway Avenue intersection (as currently planned) will help relieve existing intersection delays at this intersection and maintain minimal south leg queues on Water Street (Sidra forecasted 95th percentile existing peak hour northbound queues of approximately 17m).

In summary, the development is shown to result in little discernible impact to the adjacent road network during the examined time periods.

9.2 Parking and Loading

Direct application of City of Kelowna Zoning Bylaw minimum parking supply requirements yields a minimum parking supply of 250 spaces which is 8 spaces more than the 242 parking spaces to be provided.

The actual peak parking demand anticipated for the development, allowing for the different time of day peak parking demand characteristics of the component uses, ranges from approximately 135 parked vehicles during the mid-day period up to approximately 170 parked vehicles in the early morning period (mainly associated with the hotel use). This analysis would indicate that anywhere from approximately 75 to 110 surplus parking spaces would potentially be available in the parkade at different times of the day to offload parking pressures elsewhere in the downtown. Westcorp has committed to making available a minimum of 22 spaces in the new parkade for public use to accommodate existing street parking on Queensway west of Water Street that will be eliminated with the proposed reconfiguration of this road.

Direct application of the City of Kelowna Zoning Bylaw requirements for loading would result in a total of eight (8) loading bays for the proposed development. Based on our database information for loading use associated with this type of development, in our opinion a total of two loading bays would be sufficient. The proposed three loading bays, accessed from the rear lane, will be more than adequate to service the planned development.

9.3 Transportation Demand Management

The downtown context of the site is ideal for implementation of Transportation Demand Management (TDM) measures as it is located in a highly pedestrian friendly environment, close to transit, other shops and services, and eventually higher density residential development. Aside from providing on-site bicycle storage as per the requirements of the Zoning Bylaw, and planned provision of an on-site car rental service for hotel guests to lessen the requirement for private vehicles or even rental cars checked out from the airport, other TDM initiatives should be considered to encourage employee and guest/customer travel by modes other than private automobile. We recommend the hotel provide guests with the following information, to be available on site and on the hotel's website:

- Local transit information (in particular connections between the hotel and Kelowna airport);
- Nearby cycling routes;
- Lists of amenities within a walking distance; and,
- Available shuttle services such as winery tours.

* * * * *

APPENDIX A

Traffic Volumes

Community Consultation To-Date Summary

Public Consultation Chronology:

Dec 20th, 2012 – City of Kelowna staff and Stakeholder meeting - Initial hotel design concepts were discussed. Invitees included: City of Kelowna planning staff, the owner of Kelly O’Bryan’s; representatives from UBCO, the wine industry, technology sector, UBCO, DKA, and the Chamber of Commerce. Design drivers were refined as a result of the feedback gathered.

2013 – Meetings with adjacent neighbour - numerous one-on-one meetings were held with the owner of Kelly O’Bryan’s, Reg Henry. A letter of support from Mr. Henry accompanies the application.

Apr 17, 2013 – Meeting with DKA Board of Directors - Initial hotel concepts were presented and discussed. Feedback was overwhelmingly positive, especially regarding the interface with Kerry Park. There were questions about parking, and an expressed hope that the project not propose a large deficit in this regard.

Jan 7, 2014 – Meeting with the Chamber of Commerce Board of Directors – preliminary design drawings were presented and discussed. Feedback was quite positive.

Feb 12, 2014 – meeting with DKA Board of Directors – a presentation was given to the Board, their comments were recorded for use on a Hotel Story video, and information booklets were given to all attendees.

Feb 13, 2014 – Press release and renderings were released to the media.

Feb 13, 2014 – Proposed Hotel Information Booklet, Renderings, Design Rationale and Variance Rationale were posted on Westcorp’s website.

Feb 14, 2014 – Open House flyers were sent to 2400 households and businesses in the downtown area, and were hand-delivered to the three block radius around the development site.

Feb 26 - March 12 – Open House Notices were posted in each of the Daily Courier and Capital News publications for this time period. (Daily Courier 13 issues, Cap News 4 issues)

Mar 12, 2014 – Public Information Session/Open House was held at the Rotary Centre for the Arts from 3pm – 7pm. Presentation materials included: Presentation boards, Hotel Story Video, Hotel Fly-Through, 3-D Model, and 400 Information Booklets. Representatives were: Phil Milroy, Gail Temple, Kyle Spence, Mandy Little, and Keri Fields from Westcorp, Michael McDonald and Guy Taylor from Kasian, and Kim Perry from Perry and Associates.

188 persons registered at the session, although estimated attendance was 300 – 400

129 feedback sheets were received

112 were supportive of the project (87%)

- 1 was provisionally supportive (if the height was reduced to 19 storeys) (1%)
- 3 were undecided (2%)
- 13 were not supportive of the project (10%)

Comments from supportive individuals included:

Fabulous. Just what Kelowna needs!

Sincerely hope this opportunity is approved and moves forward rapidly.

Westcorp has done a great job so far – very impressive!

Put a restaurant on the top floor.

I think the project is fantastic. Well Done!

Look forward to seeing the project move forward.

We fully support reconfiguration of Mill St.

Great looking building; will really add value to downtown; exactly the type of project we need to bring Kelowna into the future.

Fantastic development of the space! It injects exactly what Kelowna needs in its downtown core.

Great transitions to water, park and adjacent/existing buildings.

Great project. Kelowna's next step into urban maturity. Looks great!

Thank you for a very informative presentation.

I fully support the project. Good work!

Great addition for the downtown!

Hope it is not a condo hotel. Great. Love it.

Fantastic. The sooner you build, the better.

Nice change for downtown.

Add some condo units.

Very exciting and needed project for downtown. Animates the waterfront. Supports downtown businesses and gives visitors a first class place to stay. All great!

An attractive design with a lot of thought behind it.

This type of hotel offers something different from what currently exists.

Love it!

Excellent proposal. Let's get it done now!

Good plan. Can't happen soon enough.

Will add very well to the skyline and downtown area. I am enthused. Bravo!

Huge improvement and definitely needed to update the waterfront area.

We had hoped there would be some condos. Thanks for the fine wharf.

Kelowna needs this.

A real asset to Kelowna.

Wonderful project. Landmark presence. Thanks.

Great design. Great concept.

Can't wait to see it come to life.

Looking forward to watching it develop and pull more people downtown.

Very excited to see density and great architecture added to Kelowna.

Will bring a lot of great benefits to the downtown core. I'm completely behind this project.

Well planned. Well designed. Can't wait to have a drink on one of the patios!

A design of this calibre will only serve to strengthen Kelowna's profile, burgeoning brand, and position as a Canadian jewel.

Very supportive.

Will transfer what is currently an unexciting parking lot and confusing dangerous roadway into an exciting space that will energize and great improve the appearance of the downtown waterfront.

I think this is a great project that completes the bookend for the waterfront and the Yacht Club.

Comments from undecided individuals included:

Parking may be an issue?

Need separated bike lanes in parks. Too dangerous for bikers, dogs, kids, wheelchairs, etc.

Best location, smart move, but this will affect businesses – accommodations.

Comments from unsupportive individuals included:

Some would say that not saying you are willing to serve all residents explicitly may be discriminatory to those who couldn't afford your views.

Too close to the water; too high.

Would be better as a 6 – 8 storey boutique hotel; height is not appropriate.

Should not build high-rises on the waterfront; Mill St. is needed for traffic and parking.

Hotel is too tall; benefits are for hotel guests and not Kelowna residents.

Do not like height of tower; do not like loss of park and public access.

Don't understand why someone with a heart wouldn't instead want to building something to actually help my neighbour rather than favor to line someone's pockets.

Don't think will be good for downtown especially considering impact on marginalized population.

Not in favour of highrises in the downtown area at all.

Mar 14, 2014 – Hotel Story Video, Fly-Through, and the location of the 3-D Model for viewing were posted on the Westcorp website.

Upcoming meetings/presentations:

Mar 25, 2014 – presentation to EDC Board

Apr 29, 2014 – presentation to Rotary Club at Coast Capri

Jun 4, 2014 – presentation to Ogopogo Rotary Club

We have requested meetings with The Heritage Society and the Kelowna Downtown Knox Mountain Neighbourhood Association.

CITY OF KELOWNA

MEMORANDUM

Date: April 28, 2014
File No.: OCP14-0005
To: Urban Planning (JM)
From: Development Engineer Manager (SM)
Subject: 235 Queensway Ave, 1470 & 1476 Water Street

The Development Engineering comments and requirements regarding this OCP amendment application are as follows:

1. **General.**

- a) All the offsite infrastructure and services upgrades are addressed in the Development Engineering Report under file Z14-0006.



Steve Muenz, P. Eng.
Development Engineering Manager

SS



Interior Health

March 17, 2014

James Moore
Land Use Management Department
1435 Water Street
Kelowna, BC V1Y 1J4

Dear James,

RE: Rezoning Z14-0006/OCP14-0005/DP14-0024 & DVP14-0025
235 Queensway Ave, 1470 & 1476 Water Street

Thank you for the opportunity to respond to this development application from a population health perspective.

This 24 storey, 214 room hotel proposal will offer many benefits to the City and community members.

From a population health perspective the creation and continuation of a pedestrian friendly environment with proximity to parks and pathways;

- enhance the ability of people to engage in physical activity,
- create a greater sense of community,
- increase the sense of safety in this area

All of these represent components of a healthy community and improved health outcomes. Interior Health is in support of this application.

Please contact me if you have any questions or comments.

Sincerely

Pam Moore CPHI (C)
Healthy Built Environment Team

Bus: (250) 980-5077
Fax: (250) 980-5060
Web: interiorhealth.ca

HEALTH PROTECTION
"Less Risk, Better Health"
2nd Floor, 1835 Gordon Drive Kelowna, BC V1Y 3H5



KELOWNA FIRE DEPARTMENT
Fire Prevention Branch
2255 ENTERPRISE WAY
KELOWNA, B.C.
V1Y 8B8

FIRE DEPARTMENT COMMENTS

DATE: February 21, 2014
TO: James Moore
FROM: Gayanne Pacholzuk, Fire Prevention Officer
RE: DP14 0024 235 Queensway

Comments regarding the project:

- Construction fire safety plan is required to be submitted and reviewed prior to construction and updated as required.
- Engineered Fire Flow calculations are required to determine Fire Hydrant requirements as per the City of Kelowna Subdivision Bylaw #7900.
- A visible address must be posted as per City of Kelowna By-Laws
- Sprinkler drawings are to be submitted to the Fire Dept. for review when available.
- A fire safety plan as per section 2.8 BCFC is required at occupancy. The fire safety plan and floor plans are to be submitted for approval in AutoCAD Drawing format on a CD or DVD to facilitate Fire Department pre-planning for this structure. The fire safety plan should clearly detail the unique requirements for this structure. A copy of the sprinkler system owner's certificate is to be included in the fire safety plan.
- Fire Department access is to be met as per BCBC 3.2.5.6
- Fire Stairwells to be marked clearly (including roof access) as per Fire Department requirements. This would be standardized and approved by the Kelowna Fire Department (KFD).
- Fire Department steel lock box (large size) or key tube acceptable to the fire dept. is required by the fire dept. entrance. Kurt's Lock & Safe at 100A - 1021 Ellis Street, Kelowna is the approved supplier for flush mount lock boxes.
- The standpipes connections are to be installed on the transitional landings of the stairwells as per NFPA 14. 3) Fire Stairwells to be marked clearly (including roof access) as per Fire Department requirements. This would be standardized and approved by the Kelowna Fire Department (KFD).
- All requirements of the City of Kelowna Fire and Life Safety Bylaw 10760 shall be met including part 5, 7 and 14 (Additional requirements for High Buildings)
- Fire alarm system is to be monitored by an agency meeting the CAN/ULC S562 Standard.
- Contact Fire Prevention Branch for fire extinguisher requirements and placement.
- Fire department connection is to be within 45M of a fire hydrant - please ensure this is possible and that the FD connection is clearly marked and visible from the street.

CITY OF KELOWNA
MEMORANDUM

Date: April 28, 2014
File No.: Z14-0006

To: Urban Planning (JM)

From: Development Engineering Manager (SM)

Subject: 235 Queensway Ave, 1470 & 1476 Water Street Hotel Development

Development Engineering Department have the following comments and requirements associated with this application. The road and utility upgrading requirements outlined in this report will be a requirement of this development.

The Development Engineering Technologist for this project is Sergio Sartori

General

- a) Provide easements as may be required.
- b) The proposed hotel development triggered a traffic impact assessment (TIA). The recommendations and key findings indicate no additional offsite works are triggered by the development. The TIA does require minor revisions for City Transportation & Mobility approval and must be submitted prior to issuance of the Development Permit.

1. Domestic Water and Fire Protection

- (a) The existing lot (1470) is serviced with small diameter (50mm) water service. The developer's consulting mechanical engineer will determine the domestic and fire protection requirements of this proposed development and establish hydrant requirements and service needs.
- (b) The applicant, at his cost, will arrange for the removal of the existing main within the closed road and the installation of new water main and service.
- (c) The developer must obtain the necessary permits and have all existing utility services disconnected prior to removing or demolishing the existing structures. The City of Kelowna water meter contractor must salvage existing water meters, prior to building demolition. If water meters are not salvaged, the developer will be invoiced for the meters.

2. Sanitary Sewer

- (a) The developer's consulting mechanical engineer will determine the requirements of this proposed development and establish the required size and preferred location of the new service. Only one service will be permitted for this development. The applicant, at his cost, will arrange for the removal of all existing small diameter services (3) and the installation of a new larger service.

- (b) The applicant, at his cost, will arrange for the removal of the existing main within the closed road and the installation of new sanitary main and service as required.
- (c) A downstream flow analysis check is required by a consulting civil engineer to determine the impact of additional flow contributions on the existing pipe system and sewer lift station. If it is determined that upgrades to the existing facilities must be made, additional bonding will be required.

3. Storm Drainage

- (a) The developer must engage a consulting civil engineer to provide a storm water management plan for the site which meets the requirements of the City Storm Water Management Policy and Design Manual. The storm water management plan must also include provision of lot grading plans, minimum basement elevations (MBE), if applicable, and provision of a storm drainage service and recommendations for onsite drainage containment and disposal systems.
- (b) Only one service will be permitted for this development. The applicant, at his cost, will arrange for the removal of the existing main within the closed road, and the installation of new storm main as required as well as one new overflow service.

4. Road Improvements

- (a) Queensway Avenue as a result of the road closure will require complete reconstruction including a Cul De Sac. Construction will include curb and gutter, sidewalk, storm drainage system including catch basins and manholes, pavement, boulevard landscaping /irrigation, street lights as required, and relocation or adjustment of existing utility appurtenances if required to accommodate the upgrading construction.
- (b) Water Street fronting this development site is urbanized. The proposal to construct a roundabout will require realigning the curb, gutter and sidewalk to accommodate this intersection improvement. This will require the curb, sidewalk removal and reconstruction including the re-location or adjustment of existing utility appurtenances to accommodate the upgrading construction. Additional improvements along the remainder of Water Street fronting this development may be required.
- (c) The lane fronting this development is urbanized to a paved standard. The extent of pavement restoration beyond the service trench upgrades may be required. The full width pavement of the lane should be repaved complete with a turnaround at the west end.
- (d) Mill Street as a result of the road closure will require reconstruction including the re-location or adjustment of existing utility appurtenances to accommodate the upgrading construction. The intersection of Mill Street and Bernard Avenue will be a part of the improvements.

5. Subdivision

By registered plan to provide the following:

- (a) Lot consolidation.
- (b) Grant statutory rights-of-way if required for utility services.

6. Electric Power and Telecommunication Services

The electrical services to this development must be installed in an underground duct system, and the building must be connected by an underground service. It is the developer's responsibility to make a servicing application with the respective electric power, telephone and cable transmission companies to arrange for these services which would be at the applicant's cost.

7. Street Lighting

Street lighting including underground ducts must be installed on all roads fronting on the proposed development. The cost of this requirement is included in the roads upgrading item.

8. Engineering

Road and utility construction design, construction supervision, and quality control supervision of all off-site and site services including on-site ground recharge drainage collection and disposal systems, must be performed by an approved consulting civil engineer. Designs must be submitted to the city engineering department for review and marked "issued for construction" by the city engineer before construction may begin.

9. Design and Construction

- (a) Design, construction supervision and inspection of all off-site civil works and site servicing must be performed by a Consulting Civil Engineer and all such work is subject to the approval of the City Engineer. Drawings must conform to City standards and requirements.
- (b) Engineering drawing submissions are to be in accordance with the City's "Engineering Drawing Submission Requirements" Policy. Please note the number of sets and drawings required for submissions.
- (c) Quality Control and Assurance Plans must be provided in accordance with the Subdivision, Development & Servicing Bylaw No. 7900 (refer to Part 5 and Schedule 3).
- (d) A "Consulting Engineering Confirmation Letter" (City document 'C') must be completed prior to submission of any designs.
- (e) Before any construction related to the requirements of this subdivision application commences, design drawings prepared by a professional engineer must be submitted to the City's Works & Utilities Department. The design drawings must first be "Issued for Construction" by the City Engineer. On examination of design drawings, it may be determined that rights-of-way are required for current or future needs.

10. Servicing Agreements for Works and Services

- (a) A Servicing Agreement is required for all works and services on City lands in accordance with the Subdivision, Development & Servicing Bylaw No. 7900. The applicant's Engineer, prior to preparation of Servicing Agreements, must provide adequate drawings and estimates for the required works. The Servicing Agreement must be in the form as described in Schedule 2 of the bylaw.

- (b) Part 3, "Security for Works and Services", of the Bylaw, describes the Bonding and Insurance requirements of the Owner. The liability limit is not to be less than \$5,000,000 and the City is to be named on the insurance policy as an additional insured.

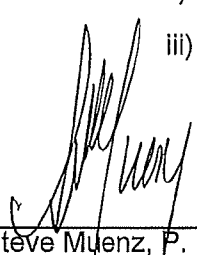
11. Geotechnical Report

As a requirement of this application the owner must provide a geotechnical report prepared by a Professional Engineer qualified in the field of hydro-geotechnical survey to address the following:

- (a) Area ground water characteristics.
- (b) Site suitability for development, unstable soils, etc.
- (c) Drill and / or excavate test holes on the site and install piezometers if necessary. Log test hole data to identify soil characteristics, identify areas of fill if any. Identify unacceptable fill material, analyse soil sulphate content, identify unsuitable underlying soils such as peat, etc. and make recommendations for remediation if necessary.
- (d) List extraordinary requirements that may be required to accommodate construction of roads and underground utilities as well as building foundation designs.
- (e) Additional geotechnical survey may be necessary for building foundations, etc.

12. Charges and Fees

- a) Development Cost Charges (DCC's) are payable
- b) Fees per the "Development Application Fees Bylaw" include:
 - i) Street/Traffic Sign Fees: at cost if required (to be determined after design).
 - ii) Survey Monument, Replacement Fee: \$1,200.00 (GST exempt) -- only if disturbed.
 - iii) Engineering and Inspection Fee: 3% of construction value (plus GST).



Steve Muenz, P. Eng.
Development Engineering Manager

SS

CITY OF KELOWNA

BYLAW NO. 10962

**Official Community Plan Amendment No. OCP14-0005 -
Westcorp Holyrood Inc. No. A31452
235 Queensway Avenue, 1470 & 1476 Water Street, Portions of Mill
Street and Queensway Avenue**

A bylaw to amend the "*Kelowna 2030 - Official Community Plan Bylaw No. 10500*".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT Map 4.1 - **GENERALIZED FUTURE LAND USE** of "*Kelowna 2030 - Official Community Plan Bylaw No. 10500*" be amended by changing the Generalized Future Land Use designation of:
 - a) part of the Mill Street Road Right-of-Way, being approximately 596m² in area, located adjacent to 235 Queensway Avenue and 262 Bernard Avenue, Kelowna, BC; and
 - b) part of the Queensway Avenue Road Right-of-Way, being approximately 168m² in area, located adjacent to 235 Queensway Avenue and 1470 Water Street, Kelowna, B.C.,

from the PARK - Major Park & Open Space designation to the MXR - Mixed Use (Residential/Commercial) designation as per Map A attached to and forming part of this bylaw.
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this

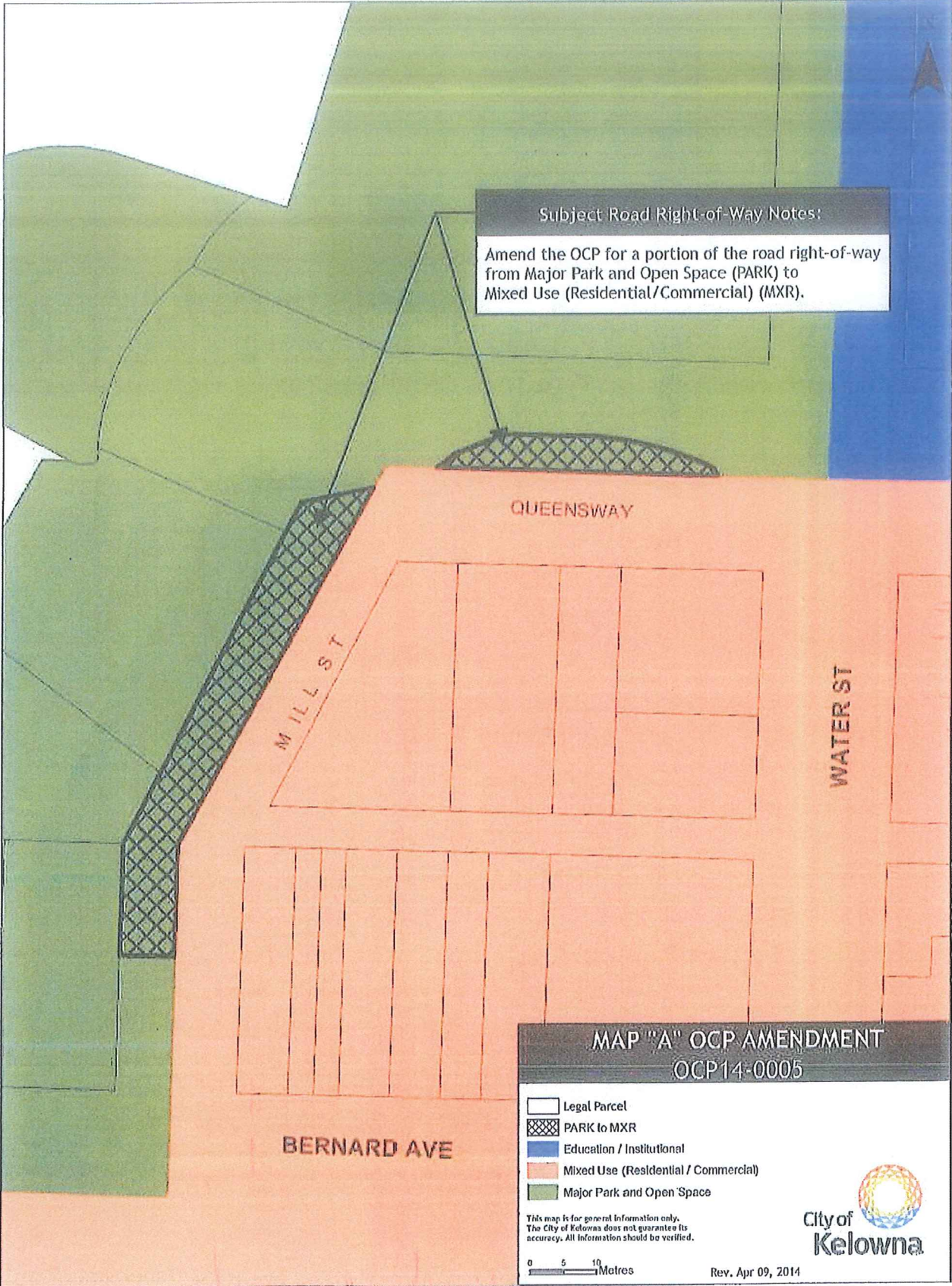
Considered at a Public Hearing on the

Read a second and third time by the Municipal Council this

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk



Subject Road Right-of-Way Notes:
 Amend the OCP for a portion of the road right-of-way from Major Park and Open Space (PARK) to Mixed Use (Residential/Commercial) (MXR).

QUEENSWAY

MILL ST

WATER ST

BERNARD AVE

MAP "A" OCP AMENDMENT
OCP14-0005

-  Legal Parcel
-  PARK to MXR
-  Education / Institutional
-  Mixed Use (Residential / Commercial)
-  Major Park and Open Space

This map is for general information only. The City of Kelowna does not guarantee its accuracy. All information should be verified.

0 5 10 Metres

Rev. Apr 09, 2014



CITY OF KELOWNA

BYLAW NO. 10963

Z14-0006 -

Westcorp Holyrood Inc. No. A31452

235 Queensway Avenue, 1470 & 1476 Water Street, Portions of Mill Street and Queensway Avenue

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of Lots 1 & 2, District Lot 139, ODYD, Plan 2698 located on 1470 & 1476 Water Street and part of the Mill Street Road Right-of-Way, being approximately 596m² in area, located adjacent to 235 Queensway Avenue and 262 Bernard Avenue and part of the Queensway Avenue Road Right-of-Way, being approximately 168m² in area, located adjacent to 235 Queensway Avenue and 1470 Water Street, Kelowna, BC. from the C7 - Central Business Commercial and P3 - Parks and Open Space zones to the C7lp/rls - Central Business Commercial (liquor primary/retail liquor sales) zone as per Map B attached to and forming part of this bylaw.
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this

Considered at a Public Hearing on the

Read a second and third time by the Municipal Council this

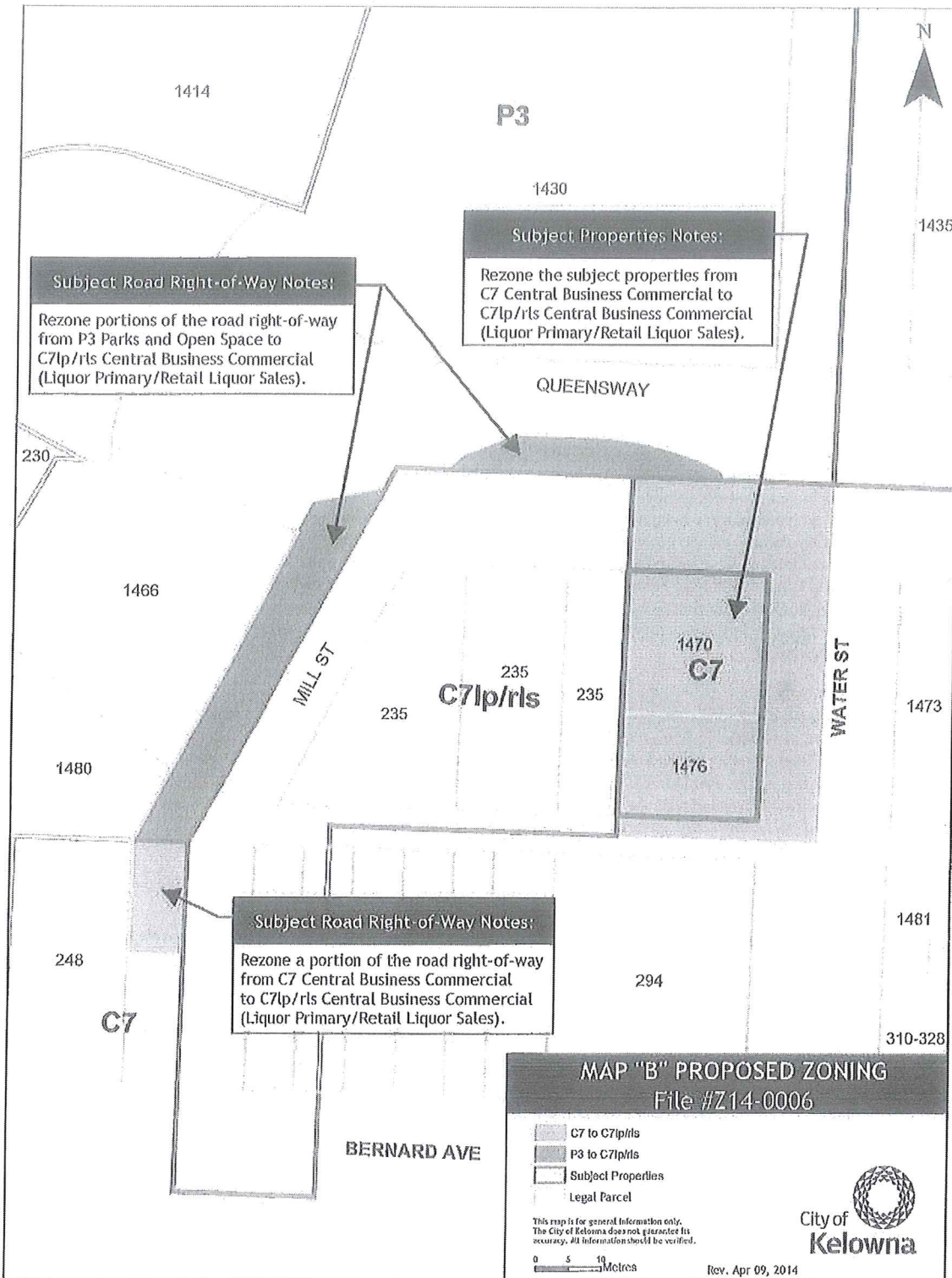
Approved under the Transportation Act

(Approving Officer-Ministry of Transportation)

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk



REPORT TO COUNCIL



Date: 4/28/2014
RIM No. 1210-21
To: City Manager
From: Subdivision, Agriculture & Environment Services (MS)
Application: OCP13-0011
Z13-0016
Address: 984 Dehart Road
Owners: Sherwood Mission Developments
Dr. A. Rezansoff
Applicant: New Town Planning Services Inc.
Subject: Official Community Plan (OCP) Amendment and Rezoning Application

Existing OCP Designation: Single / Two Unit Residential (S2RES) / Multi-Unit Residential Low Density (MRL)
Proposed OCP Designation: Single / Two Unit Residential (S2RES) / Multi-Unit Residential Low Density (MRL)
Existing Zone: A1 - Agriculture 1
Proposed Zone: RU1 - Large Lot Housing

1.0 Recommendation

THAT Official Community Plan Bylaw Amendment No. OCP13-0011 to amend Map 4.1 of the Kelowna 2030 - Official Community Plan Bylaw No. 10500, by changing a portion of the Future Land Use designation of Lot 1, Section 31, TWP 29, ODYD Plan KAP62654, located at 984 Dehart Road, Kelowna, BC, from the Multi-Unit Residential Low Density (MRL) designation to the Single/Two Unit Residential (S2RES) designation, and a portion of Single/Two Unit Residential (S2RES) be changed to Multi-Unit Residential Low Density (MRL), in accordance with 'Map A', be considered by Council;

AND THAT Rezoning Application No. Z13-0016 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of part of Lot 1, Section 31, TWP 29, ODYD Plan KAP62654, located at 984 Dehart Road, Kelowna, BC, from the A1 - Agriculture 1 zone to the RU1 - Large Lot Housing zone, in accordance with 'Map B' be considered by Council;

AND THAT the Official Community Plan Amendment Bylaw and Zone Amending Bylaw be forwarded to a Public Hearing for further consideration;

AND THAT final adoption of the zone amending bylaw be considered subsequent to the requirements of Development Engineering Branch and Parks Services, being completed to their

satisfaction, as well as a Farm Protection Development Permit, and a Natural Environment Development Permit.

2.0 Purpose

The applicant is proposing to rezone a portion of the land from the A1 - Agriculture 1 zone to the RU1 - Large Lot Housing zone to facilitate a future 55 lot residential subdivision. The proposal requires an OCP amendment to extend the Single/Two Unit Residential (S2RES) future land use designation north into the area designated for Multi-Unit Residential, Low Density (MRL), in exchange for area that was previously Single/Two Unit Residential for Multi-Unit Residential, Low Density (MRL).

3.0 Subdivision, Agriculture & Environment Services

The property at 984 Dehart was removed from the Agricultural Land Reserve (ALR) in 1989 and since the mid-1990s has been subject to significant infilling and watercourse disruption and relocation. In 1998, a Section 219 Restrictive Covenant was placed on the property requiring geotechnical, hydrogeological and drainage investigation and plans prior to development. Fill has been placed over peat, making the current soil conditions unsuitable and/or uncertain for road and foundation construction. Geotechnical concerns involve the presence of undetermined fill over topsoil and peat in some locations, and the uncertainty of the structure of the fill. Hydrogeological concerns involve the presence and reconfiguration of springs and watercourses on the property. The covenant outlines investigation and plans required to address these concerns prior to development.

Between 2008 and 2012, nine bylaw investigations occurred on the property, including stop work orders for unauthorized earth moving, and unsightly premises. A Settlement and Release Agreement was signed by the owner and the City of Kelowna (the City) in 2010, whereby the City of Kelowna agreed not to enforce the bylaw dispute with the understanding that the owner would undertake actions, including riparian and watercourse protection and restoration, and adherence to City bylaws.

The current application was initiated in the spring of 2013. This proposal was for rezoning to RU-1 Large Lot Residential included a road access that came through Young and Turner Roads. A Public Information Meeting was held on April 18, 2013, with most of the residents opposed to this access. Based on this public feedback, an alternate plan was developed, with the primary access at Dehart Road. This is the road layout for the current configuration. A Traffic Impact Analysis was prepared for this revised road network, which was received by the City in February, 2014.

Staff supports the application subject to the following conditions being fulfilled prior to 4th Reading, including requirements of the:

- Development Engineering Branch (see attached memo),
- Parks Services, including a blanket Statutory Right of Way over the property, to facilitate a trail network to be established at the time of subdivision;
- Subdivision, Agriculture & Environment Services, including the requirements of the Covenant #KM084923 (see summary below and attached covenant);
- Farm Protection Development Permit;

- Natural Environment Development Permit, including a mitigation/restoration plan (signed off by an QEP/RPBio) for the site, as required by the Settlement and Release Agreement¹ (attached); and a
- The registration of a Build/No Disturb Covenant of 20 m along the northeast property line and a 15 m No Build / No Disturb Covenant along the west property line.

4.0 Proposal

4.1 Project Description

The proposal is to amend the OCP and rezone the property to accommodate a future 55 unit RU1 - Large Lot Residential subdivision on the southern and central portion of 984 Dehart Road. The Official Community Plan (OCP) Amendment is required to facilitate an extension of the Single / Two Unit Residential future land use designation slightly to the north, replacing 9,610 m² of Multi-Unit Residential, Low Density (MRL), and changing 6,550 m² of Single / Two Unit Residential to Multi-Unit Residential, Low Density (MRL). This represents a net increase of 3,060 m² of Single / Two Unit Residential area than is currently designated in the OCP.

The proposal includes two watercourses and associated riparian management areas (RMA) in the land use plan, including a 20 m wide RMA along the northeast boundary and a 15 m RMA along the west boundary. The northeast RMA will also function as a farm protection buffer area. Adjacent water licenses with respect to flow and location must be accommodated within the plan. A memorandum of understanding with their neighbour regarding Ahern and Cowan Spring has been agreed upon.

4.2 Background

As discussed, much of the site has been modified over the last 20 years through grading and infilling. The filling and watercourse modifications, as well as bylaw infractions of moving soil and watercourses without permits, have resulted in a Section 219 Restrictive Covenant being registered on the title of the property and the City of Kelowna Settlement and Release Agreement - 2010. Details of these are included below.

Section 219 Restrictive Covenant #KM084923

The owner entered into a Section 219 Restrictive Covenant with the City of Kelowna in 1998² that stipulated that no further development occur until certain conditions were provided, including:

- Sanitary sewer and a community water provided with fire protection capability;
- A complete geotechnical report, to address the fill material and remedial actions required to make the property suitable for development, as well as address the surface and groundwater characteristics and a drainage plan, including any items that should be included in a restrictive covenant which would safeguard building sites and utility installations;
- Site grading and drainage plan completed to the satisfaction of the Development Engineer;
- A suitable landscape buffer complete with fencing is established along the border of the ALR and non-ALR lands to the southeast and to the north and to the satisfaction of the Approving Officer;

¹ Rezansoff, A.A. & the City of Kelowna, 2010. Settlement and Release Agreement.

² BC Agent – Kamloops Registry Service Ltd, Aug. 28, 1998. Land Title Act Form C No 1044 – KM084923 – Covenant.

- A prepared geotechnical report prepared by Golder & Associates³; and
- A complete inventory of springs, drainage channels and water licenses be identified, complete with a hydrogeotechnical study proposing how water will be handled through culverts and drainage channels.

City of Kelowna Settlement and Release Agreement - 2010

In 2010, a Settlement and Release Agreement (attached) was signed by the owner and the City of Kelowna, stipulating that the owner undertake the following:

- Ensure that there is no net loss of riparian habitat and function for the Lands;
- To remove all of the drain piping located on the Lands as identified on the photograph of the Lands attached hereto as Schedule "A";
- Prepare a mitigation/restoration plan (signed off by an QEP/RPBio) for the site;
- Ensure that all future works on the site will adhere to applicable provincial acts, including but not limited to: the *BC Water Act* and the *BC Fish Protection Act*; and
- Ensure that all future works on the site will adhere to applicable City of Kelowna policies, plans and policies including but not limited: Kelowna 2020 Official Community Plan (7600), Zoning Bylaw (8000) and Subdivision and Servicing Bylaw (7900).

4.3 Site Context

The 9.6 ha subject property is located within the Okanagan Mission Sector of the City (Map 1, below) and is within the Permanent Growth Boundary. The property is currently zoned A1 but is not in the Agricultural Land Reserve. Properties within the Agricultural Land Reserve lie to the north and east (Map 2, below).

The property has a number of water licenses in favour of downstream neighbours. The historical infilling resulted in changes in water flow. This resulted in issues with respect to water licenses. As part of this application package, a Memorandum of Understanding has been prepared to address water license and water flow issues. The neighbours have agreed to cooperate in the construction of a proposed conveyance route in order to ensure the water license rights are sustained in the future.

The natural gas line Right of Way (ROW) runs diagonally through the site from the west of the site to the north.

A number of watercourses are present on the site. The project includes riparian areas and details on cross sections of each of these watercourses in the design⁴ and riparian planting is proposed as part of the environmental report, but a riparian restoration plan has not yet been provided⁵.

4.4 Neighbourhood Context

A single / two unit residential subdivision with RU1 zoning lies to the west of the property, and agricultural properties lie to the north and east. Dehart Road borders the property to the south, and single unit and rural residential properties are south of Dehart Road.

Specifically, adjacent land uses are as follows:

³ Golder & Associates, 1996. Preliminary Geotechnical Investigation Proposed Subdivision, Lot 2, Plan 35773, Sec. 31, TP.29, ODYD Dehart Road, Kelowna, British Columbia

⁴ Aplin & Martin, 2013. Civil Engineering Servicing Brief – Proposed Dehart Road Subdivision – 984 Dehart Rd, Kelowna, BC

⁵ Makonis, 2013. Dehart Road Environmental Assessment – Submitted to New Town Planning Services Inc.

Orientation	Zoning	ALR	Land Use
North	A1 - Agriculture 1	Yes	Agriculture
East	A1 - Agriculture 1	Yes	Agriculture
South	RU1 - Large Lot Housing RR2 - Rural Residential 2	No	Single Family Residential Rural Residential
West	RU1 - Large Lot Housing	No	Single Family Residential

The subject property is located within a Development Permit Area for Farm Protection given its proximity to ALR land. It is within a Natural Development Permit Area for high groundwater and surface water conditions.

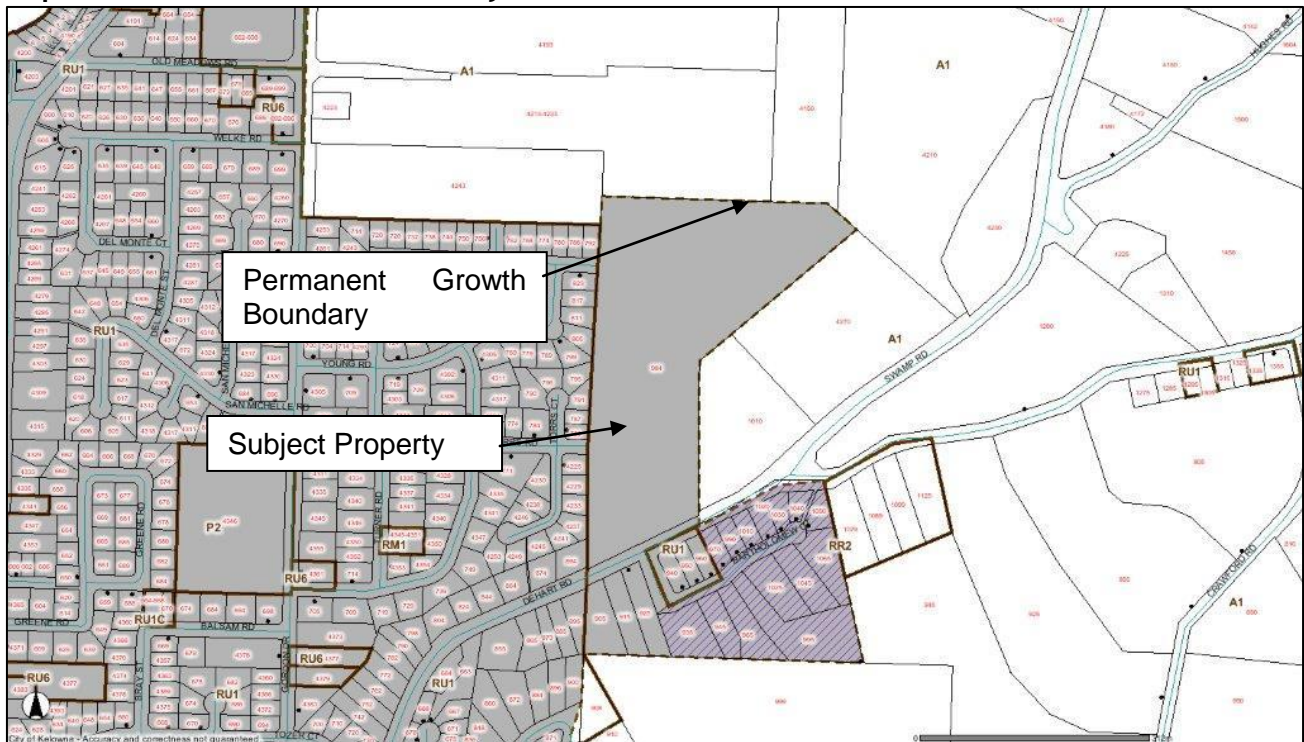
Map 1: Subject Property



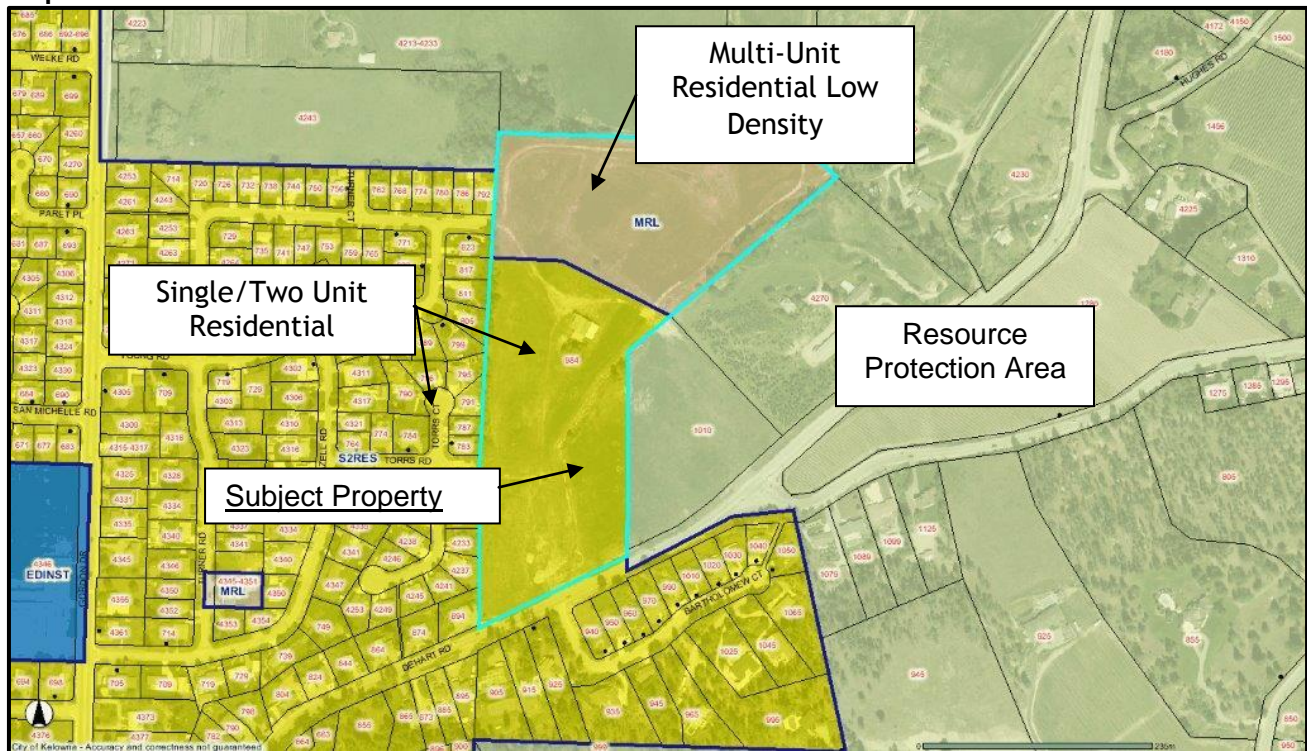
Map 2: Agricultural Land Reserve



Map 3: Permanent Growth Boundary



Map 4: Current Future Land Use



4.5 Traffic Impact Analysis

Two traffic impact analysis (TIA) reports have been completed for the project. The first Traffic Analysis Report⁶ was completed with the main access via Young and Turner Roads. Torrs Road was available for emergency vehicles only, and there was no vehicular access from Dehart Road. This road network option was presented at a public information session held by the applicant in April 18, 2013. It was met with significant concern from the residents of the Young / Turner Road neighbourhood. In response, another road network configuration was developed using an intersection at Dehart Road and Bartholomew Court as the primary access. A second TIA was completed in based on this new configuration.

A second TIA, the Traffic Impact Assessment for the Proposed 984 Dehart Road Development⁷, and was completed in January, 2014. This TIA is based on the current road and lot configuration, with the primary vehicular access on Dehart Road, and a secondary access from Turner Road.

The TIA assessed two phases of the project. The first phase is based on 55 single family units, based on the rezoning under application currently, and is estimated for completion in 2018. The second phase includes a townhouse development of approximately 200 units at the north portion of the property. This is not currently under application, but the use is accommodated in the current OCP and has been estimated for completion in 2030.

The current road layout includes the primary access through an intersection on Dehart Road and Bartholomew Court, to the south of the project. The results of the TIA indicate that the 55 unit

⁶ CTQ, March 2013. Traffic Analysis Report for 984 Dehart Road.

⁷ EYH Consultants Ltd., Jan. 31, 2014. Traffic Impact Assessment for the Proposed 984 Dehart Road Development, Kelowna, BC. A report prepared for New Town Architecture and Urban Planning.

single family development could be accommodated with this intersection. Road A on the site could accommodate three lanes with STOP control, two outbound and one inbound. A westbound right turn lane into the development from Dehart could be installed and should be included in the detailed design stage for the subdivision.⁸ The TIA indicates that painted left turn lanes at Dehart Road for access to the site should also be provided. Along the Young / Turner Roads, the TIA estimates that the 55 unit subdivision will generate an additional 4 vehicles in the morning and 6 in the afternoon peak hours.

The TIA found that, at the time of the addition of the 200 unit townhouse development, a traffic signal would be required at Dehart Road. These results are based on the assumption that the townhouse traffic will use the Road A / Dehart intersection for primary access. Under this assumption, the results estimate an additional 8 trips through the Young / Turner Road route in peak morning hours, and 10 trips along this route at peak afternoon hours.

The OCP indicates that a sidewalk is required along Dehart between the project and Gordon Drive. The City of Kelowna is willing to take a limited responsibility for the costs, for 50% up to a maximum of \$25,000⁹. Frontage upgrades along Dehart Road would be required in accordance with Bylaw # 7900. The TIA indicates that pedestrian access could be provided along the utility easements.

4.6 Utilities

The sanitary sewer services are within the jurisdiction of the City of Kelowna. Currently water is provided by the South-East Kelowna Irrigation District (SEKID). According to the consulting engineer report¹⁰, an agreement in principle has been reached between the City of Kelowna and SEKID to accommodate a boundary adjustment, such that the City of Kelowna will become the water purveyor for the subdivision prior to 4th reading of the rezoning application, subject to the owner covering the administration costs incurred by SEKID for the boundary adjustment (memo attached).

4.7 Public Consultation

A Public Information Session was held for the project on April 18, 2013. There were 51 attendees recorded. An exit questionnaire was prepared with 32 responses received. The road and lot configuration presented included the primary access from Young / Turner Roads. Of the 32 responses, 27 strongly disagreed that the traffic could be accommodated on this route. The results of the April 18, 2013 Public Information Session are attached. The road configuration was changed in response to these results, and that the main access is now from Dehart Road.

4.8 Zoning Requirements

The zoning requirements for RU1 developments are included in the table below. Note that the lot current lot illustrated in the application documents illustrates a proposed configuration, but because a subdivision is not currently being applied for, the exact alignment of lots may change. The road network will not change. However, the current configuration demonstrates that 55 RU1

⁸ EYH Consultants Ltd., Jan. 31, 2014. Traffic Impact Assessment for the Proposed 984 Dehart Road Development, Kelowna, BC. A report prepared for New Town Architecture and Urban Planning.

⁹ Muenz, S. January 21, 2014. Email to K. Funk, Dehart IIA & Sidewalk.

¹⁰ Aplin & Martin, 2013. Civil Engineering Servicing Brief – Proposed Dehart Road Subdivision – 984 Dehart Rd, Kelowna, BC.

- Large Lot Housing is possible for the site. No variances are necessary with the current configuration.

Zoning Analysis Table		
CRITERIA	ZONE REQUIREMENTS	PROPOSAL
Existing Lot/Subdivision Regulations		
Lot Area	550 m ²	Minimum 550 m ²
Lot Width	16.5 m	Minimum 16.5 m
Lot Depth	30 m	Minimum 30 m

No variances are being requested for this rezoning. Staff notes that subdivision plan is not being applied for at this time. The lot current lot configuration has no variances required.

4.9 Transit

4.10 Currently there is a transit stop at the southeast corner of the subject property. This existing bus stop will require upgrading to current standard (UBS2), as per the Development Engineering Department requirements (see attached memo).

4.11 Pedestrian Linkages

The internal roads will be built to a local road standard according to the Subdivision, Development and Servicing Bylaw No. 7900. The current subdivision layout includes a pedestrian connection to Torrs Road. As a subdivision is not currently being applied for and may change, a blanket statutory right-of-way is being requested as a condition of rezoning, with trail connections to be determined during the Preliminary Layout Review of subdivision.

4.12 Geotechnical

As discussed, much of the site has been modified over the last 20 years through grading and infilling. The fill was placed over peat in different locations of the property. A geotechnical report in 1998 concluded that buildings constructed over the existing conditions would result in unacceptable total and differential settlements¹¹. The report provided a number of potential treatments for the property prior to development, including but not limited to:

- Complete subexcavation of the fills, surficial topsoil layer and peat deposits and replacement with granular fills;
- Pile supports;
- Subexcavation of fill layer, combined with preload fill to compact peat deposits;
- Complete subexcavation of fills, topsoil and peats under proposed roads and replacement with granular fill and/or preloaded;
- Dewatering during excavation due to high groundwater table; and
- The provision of adequate subsurface and surface drainage measures.

4.13 Surface Water

¹¹ Golder & Associates, 1996. Preliminary Geotechnical Investigation Proposed Subdivision, Lot 2, Plan 35773, Sec. 31, TP.29, ODYD Dehart Road, Kelowna, British Columbia

Aplin and Martin Consultants Ltd. have provided plans accommodating overland creek flows at the west and northeast portions of the property (see attached plans). The plans include a 15 m creek and RMA along the west property line, and a 20 m wide channel and RMA / farm protection buffer along the northeast property line. The West Creek cross section indicates that the channel will be lined with riprap, and includes culverts under proposed roadways. The Creek Restoration Plan on the northeast property line will have side slopes of 6:1, transitioning into 3:1 slopes to meet existing grade, and will not have riprap.

4.14 Groundwater

The Section 219 Restrictive Covenant #KM084923 stipulates that a geotechnical report that addresses surface and groundwater characteristics must be and outlines recommendations for an overall drainage plan that safeguards building sites and utility installations must be prepared. The Dehart Road Environmental Assessment indicates that 6 springs are known in close proximity to the subject property¹².

A Creek Restoration Plan for the northeast drainage channel and a plan for the West Creek Upgrade have been prepared by Aplin & Martin Consultants Ltd. and are attached.

The property is within a Natural Environment Development Permit Area, and the applicant has a current Natural Environment Development Permit application for this property with the City. The approximate area of high groundwater is shown in Map 5, below.

4.15 Environmental

According to the Dehart Road Environmental Assessment, a number of rare and endangered ecosystems occur on the site. One of these, the Fd- Water Birch - Douglas Maple ecosystem occurs along the northeast property line, where the creek / riparian reserve is proposed. Another ecosystem, the ActFd - Common Snowberry - Red-osier Dogwood Riparian, is present along the west property line along the open drainage in this location. While the ecosystem is not ranked by the Conservation Data Centre of BC, it is associated with riparian and wetland habitat characteristics and should be protected, according to the assessment. Within the ditches, there are several small isolated occurrences of cattail marsh. These are ranked as 'blue-listed' or vulnerable, by the Conservation Data Centre of BC. Avoidance of these areas is recommended in the assessment¹³. The rest of the ecosystems found on site are anthropogenic, or highly modified, according to the report.

The assessment noted evidence of deer and coyote, and likely provide habitat for mice, voles and shrews, although these species were not specifically inventoried. Bighorn sheep were noted on the property during one site visit.

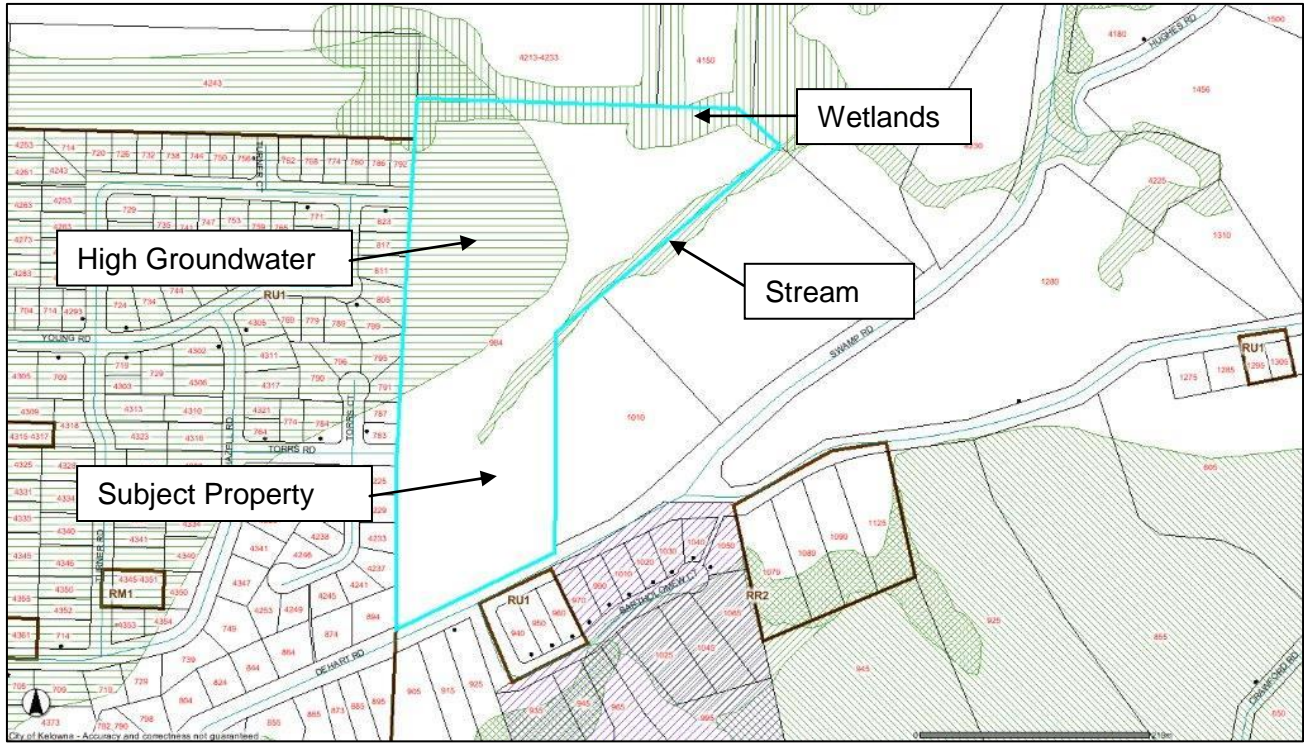
The Dehart Road Environmental Assessment found no rare plants on the property. However the timing and sampling intensity may indicate non-detection rather than absence¹⁴.

¹² Makonis, 2013. Dehart Road Environmental Assessment – Submitted to New Town Planning Services Inc.

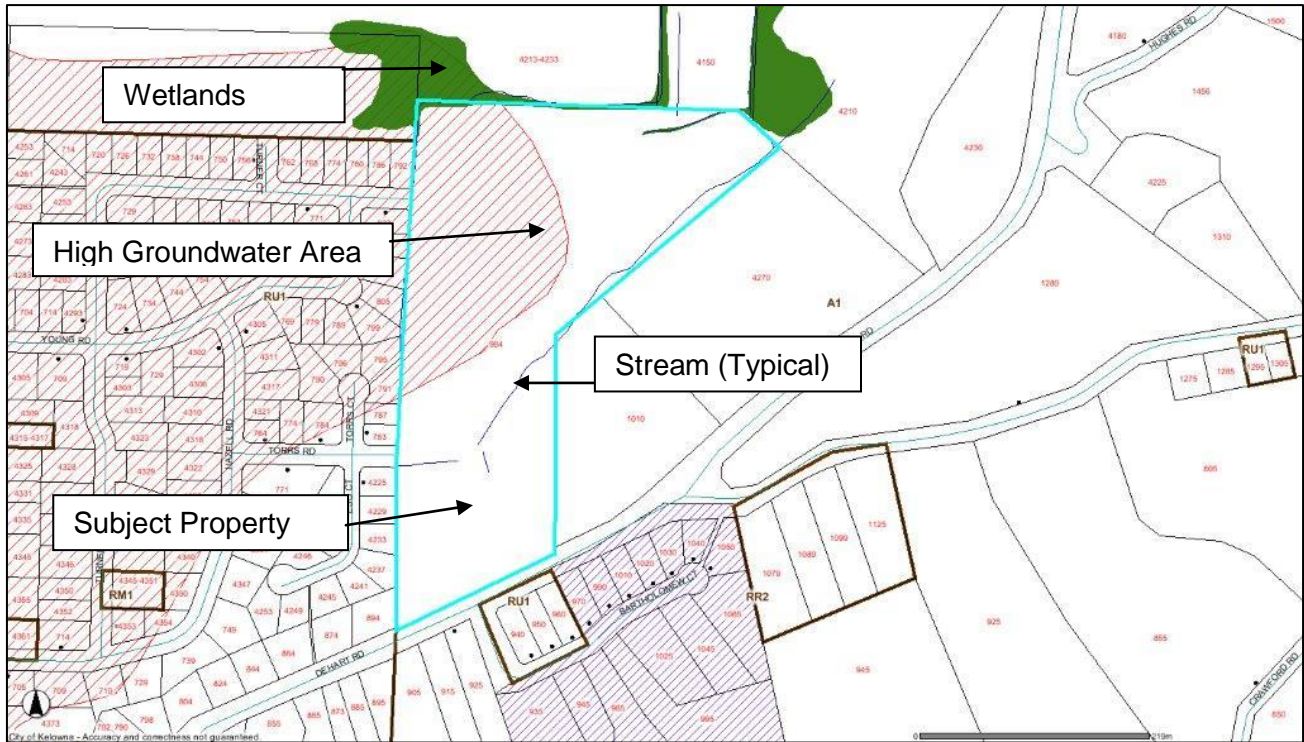
¹³ Makonis, 2013. Dehart Road Environmental Assessment – Submitted to New Town Planning Services Inc.

¹⁴ Makonis, 2013. Dehart Road Environmental Assessment – Submitted to New Town Planning Services Inc.

Map 5: Natural Environment Development Permit Areas



Map 6: Neighbourhood Wetlands, Creeks and Easements



5.0 Current Development Policies

5.1 Kelowna Official Community Plan (OCP)

Focus development to designated growth areas¹⁵.

Permanent Growth Boundary. Establish a Permanent Growth Boundary as identified in the OCP. Support development of property outside the Permanent Growth Boundary for more intensive uses only to the extent permitted as per the OCP Future Land Use designations in place as of initial adoption of OCP Bylaw 10500, except as per Council’s specific amendment of this policy.

Compact Urban Form. Develop a compact urban form that maximizes the use of existing infrastructure and contributes to energy efficient settlement patterns. This will be done by increasing densities (approximately 75 - 100 people and/or jobs per ha located within a 400 metre walking distance of transit stops is required to support the level of transit service) through development, conversion, and re-development within Urban Centres in particular and existing areas as per the provisions of the Generalized Future Land Use.

Farm Protection DP Guidelines¹⁶

Objectives

- Protect farm land and farm operations;
- Minimize the impact of urban encroachment and land use conflicts on agricultural land;
- Minimize conflicts created by activities designated as farm use by ALC regulation and non-farm uses within agricultural areas.

Guidelines

- On properties located adjacent to agricultural lands, design buildings to reduce impact from activities associated with farm operations. Design considerations include, but are not limited to maximizing the setback between agricultural land and buildings and structures, and reducing the number of doors, windows, and outdoor patios facing agricultural land.
- On agricultural and non-agricultural lands, establish and maintain a landscape buffer along the agricultural and/or property boundary, except where development is for a permitted farm use that will not encourage public attendance and does not concern additional residences (including secondary suites), in accordance with guidelines provided by Ministry of Agriculture “Guide to Edge Planning” and the ALC report “Landscape Buffer Specifications” or its replacement.
- Design any subdivision or urban development of land to reduce densities and the intensity of uses gradually towards the boundary of agricultural lands.

Ensure environmentally sustainable development¹⁷.

Environmentally Sensitive Area Linkages. Ensure that development activity does not compromise the ecological function of environmentally sensitive areas and maintains the integrity of plant and wildlife corridors.

¹⁵ City of Kelowna 2030 Official Community Plan (2011) - Farm Protection Development Permit Chapter; p. 15.2 - 15.4.

¹⁶ City of Kelowna 2030 Official Community Plan (2011) - Farm Protection Development Permit Chapter; p. 15.2 - 15.4.

¹⁷ City of Kelowna 2030 Official Community Plan (2011) - Development Process Chapter 5; p. 5.17

Protection Measures. Protect and preserve environmentally sensitive areas (ESAs) using one or more of the following measures, depending on which measures are appropriate to a given situation:

- Dedication;
- Return to Crown Land or covenant for conservation purposes with the City or other government body; and
- Ensure setbacks on adjacent developments on adjacent developments are adequate to maintain the integrity of the ESA.

5.2 City of Kelowna Agriculture Plan

New Growth Areas¹⁸. Discourage the establishment of new growth areas within or beyond agricultural areas that create additional traffic pressure on the local rural road network.

Farmland Preservation¹⁹. Direct urban land uses to areas within the permanent growth boundary, in order to reduce development and speculative pressure. This is to encourage the preservation of agricultural lands and discourage further extension of existing urban areas into agricultural lands.

Urban Buffers. Require new development, adjacent to agricultural areas, to establish setbacks, fencing and landscape buffers on the urban side of the defined urban - rural/agricultural boundary.

6.0 Technical Comments

6.1 Building & Permitting Department

A Geotechnical report will be required to define building requirements / limitations.

6.2 Development Engineering Department

A full list of Development Engineering Services requirements is included in the attached memo. A summary includes:

- Prior to final adoption of the zoning bylaw, a pre-design report must identify all the offsite servicing needs in accordance with the Subdivision, Development and Servicing Bylaw No. 7900.
- The high water table may impact site and building design, and should be determined by an engineer and detailed on a Lot Grading Plan.
- A report by a Geotechnical Engineer in the field of hydro-geotechnical engineering to identify ground water characteristics and limitations, as well as soil conditions and potential requirements or restrictions regarding slope stability, soil suitability and drainage requirements.
- Provide an adequate domestic and fire water system with adequate water pressure, in accordance with Bylaw No. 7900.
- Provide an adequately sized sanitary sewer system complete with individual lot connections.

¹⁸ City of Kelowna Agriculture Plan (1998); p. 99.

¹⁹ City of Kelowna Agriculture Plan (1998); p. 131 & 132.

- Provide an adequately sized drainage system and a detailed Lot Grading Plan, complete with dedications, rights of way, setbacks and non-disturbance areas, a Stormwater Management Plan, and an Erosion and Sediment Control Plan.
- Upgrades to Dehart Road to an urban standard, including all fronting improvements.
- Provide an additional highway allowance widening on Dehart Road.
- Recommendations from the Traffic Impact Analysis (TIA) on a roadway access onto Dehart will be requirements of zoning.
- Provide underground service connections and street lights.
- Design and construction must be prepared and inspected by a Consulting Civil Engineer in accordance with Bylaw No. 7900.
- Provide a Servicing Agreement in accordance with Bylaw No. 7900, including the preparation of adequate drawings and estimates prior to the preparation of the Servicing Agreement.
- Provide all necessary Statutory Rights-of-Way for any utility corridors required, including those on proposed or existing City Lands.

6.3 Bylaw Services

The property was subject to nine bylaw complaints between 2008 and 2011. The complaints varied, but included noise, unsightly premises, nuisance trees and shrubs and altering the land without a development permit. All bylaw incidents were concluded.

6.4 Parks and Public Spaces Department

The Parks and Public Spaces Department have concerns with the ground and surface water conditions of the site, and potential impacts to Thomson Marsh and Mission Recreation Park. They recommend that any creek restoration be done by the developer and that a three year maintenance requirement by the developer.

Infrastructure Planning, in consultation with Parks Services, have determined that there is no need for park dedication within this subdivision, based on the Linear Park Master Plan, the OCP, and that the number of units is not high enough to justify the provision of a park at this time.

However, Parks Services has identified a need for a pedestrian walkway through the development, and across Dehart Road from Bartholomew Court. This would provide access for high school students going to Okanagan Mission High School, and a pedestrian access to Dehart Road at the mid-point of the property. These should be constructed to the Bylaw No. 7900 standard.

Parks Services recommends that any environmental areas are protected with a No Build / No Disturb Covenant on private lands.

6.5 Policy and Planning

The RU1 zoning requested is permitted in the S2RES portion of the property, and therefore compliant with the OCP. Although the portion designated for Multi-Unit Residential - Low Density (MRL) is not consistent with the OCP, it is consistent with the surrounding uses and is also

compliant with other policies within the OCP, including environmental policies of Environmentally Sensitive Linkages, Protection Measures, Voluntary Protection, and Habitat Protection. The proposal indicates there will be pedestrian connections for easier access to the transit stop on Dehart Road.

6.6 Subdivision Approving Officer

The Subdivision Approving Officer notes that fill has been brought to the site for years without geotechnical investigation, and that Development Cost Charges (DCCs) will be payable at final subdivision stage.

6.7 Fire Department

Fire Department access, fire flows, and hydrants must be in accordance with the BC Building Code and the City of Kelowna Subdivision Bylaw #7900. This bylaw requires a minimum of 60 ltr/sec fire flows. Additional comments will be required at the time of the building permit application.

6.8 Fortis BC Inc. - Electric

The subject property is currently serviced by a primary electrical distribution extension from Dehart Road. This line crosses both a neighbour's property and a waterway without a statutory right of way or permitting. Until such a time as this line is protected by land rights, any servicing to the proposed subdivision will require a new extension from the primary distribution facilities along Dehart Rd.

The applicant is responsible for costs associated with servicing the proposed lots as well as the provision of appropriate land rights where required. It should be noted that additional land rights issues may arise from the design process but can be dealt with at that time, prior to construction.

6.9 Fortis Gas

Fortis BC Gas has a pipeline running at a diagonal through the northern portion of the property. They have provided these comments, as well as general development comments.

- Fortis pipeline may require an engineering assessment and possible upgrade to current standards at the expense of the owner.
- The existing soil conditions, final site grades and building elevations, as well as preloading, must be considered as it relates to the existing pipeline and Right of Way (ROW).
- Fortis BC encourages the use of its ROW for linear parks.
- Fortis BC requests that the proposed property lines align with, and do not cross, the Fortis BC ROW boundaries.

7.0 Application Chronology

Application Received: March 26, 2013

Public Information Session April 18, 2013

The proposal was presented to the public. Most of the feedback was regarding the road configuration through Young and Turner Roads. The applicant took this feedback into consideration and prepared an alternate road and layout configuration using Dehart Road as the primary access.

Application put on hold: May 20, 2013

Application was put on hold to investigate the possibility of applying for a land inclusion / exclusion through the Agricultural Land Commission with the adjacent parcel.

Traffic Impact Analysis January 31, 2014

EYH Consultants Ltd. issued a revised Traffic Impact Assessment for the Proposed Dehart Road Development.

Revised Plans Received April 22, 2014

Report prepared by:

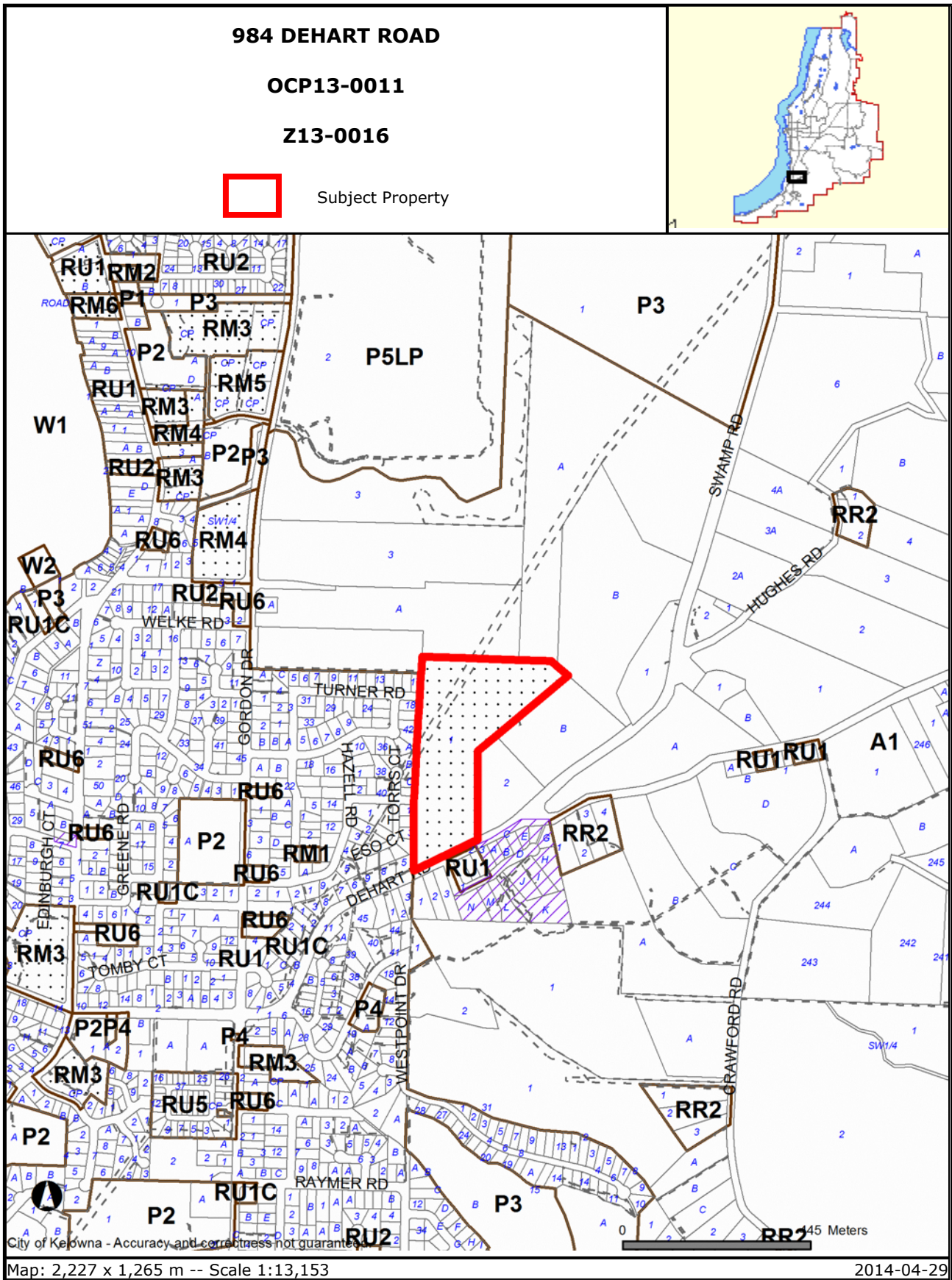
Melanie Steppuhn, Land Use Planner

Reviewed by: Todd Cashin, Manager, Environment & Land Use

Approved for Inclusion: Shelley Gambacort, Director, Land Use Management

Attachments:

Subject Property Map - 1 page
Site Photos - 1 page
OCP Amendment Plan - 1 page
Riparian Layout Plan - 1 page
Lot Easement and Covenant Plan - 1 page
Pedestrian Network Plan - 1 page
Aplin & Martin Consultants Ltd. - Civil Package - 9 pages
Key Plan - West Creek Upgrade - 2 pages
Creek Restoration Plan - 1 page
Development Engineering Manager - Memo
South East Kelowna Irrigation District - Memo
Settlement and Release Agreement - 2010
Section 219 Restrictive Covenant #KM084923 - 20 pages



*Certain layers such as lots, zoning and dp areas are updated bi-weekly. This map is for general information only.
 The City of Kelowna does not guarantee its accuracy. All information should be verified.*

984 Dehart Road – Site Photos



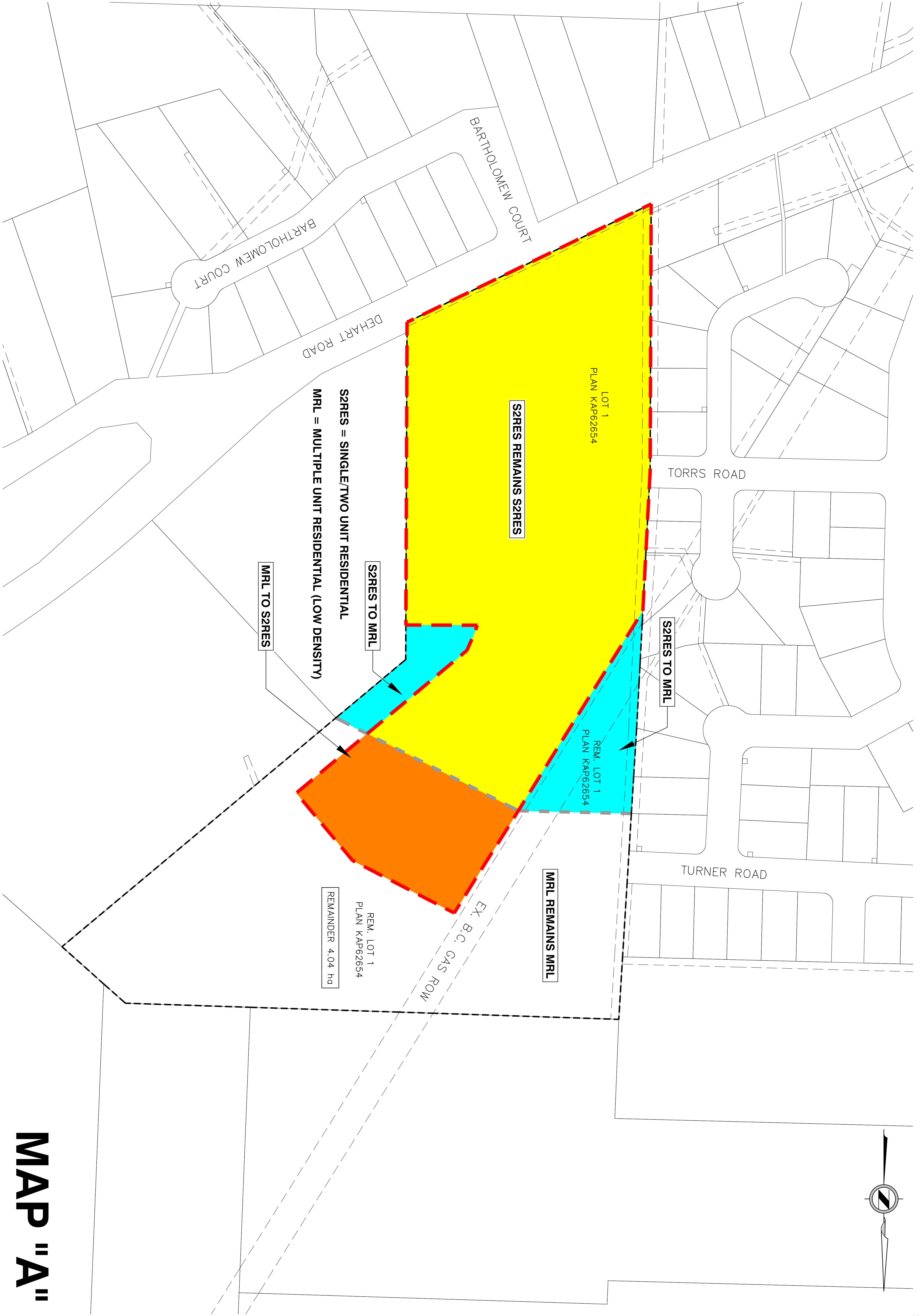
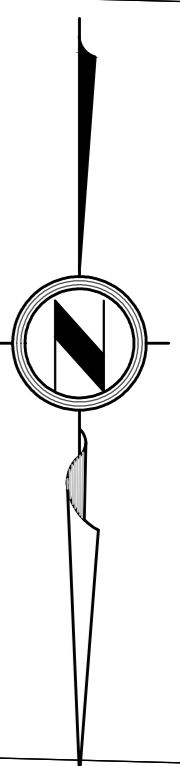
Panorama Photo from the North East Corner of the Subject Site



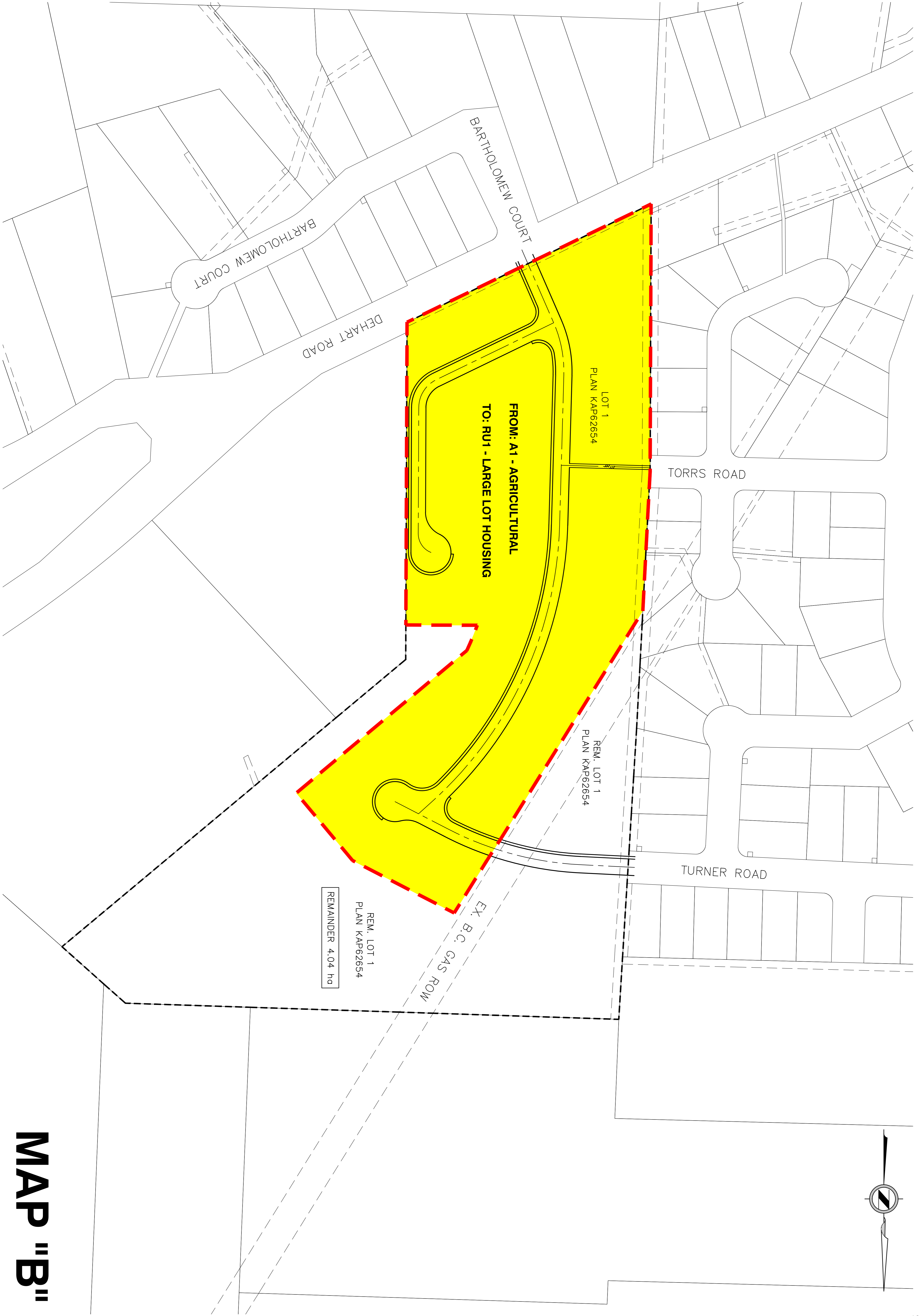
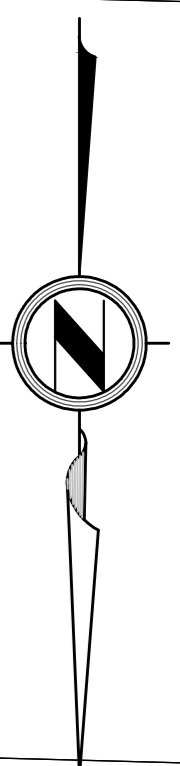
Panorama Photo from the South East Corner of the Subject Site



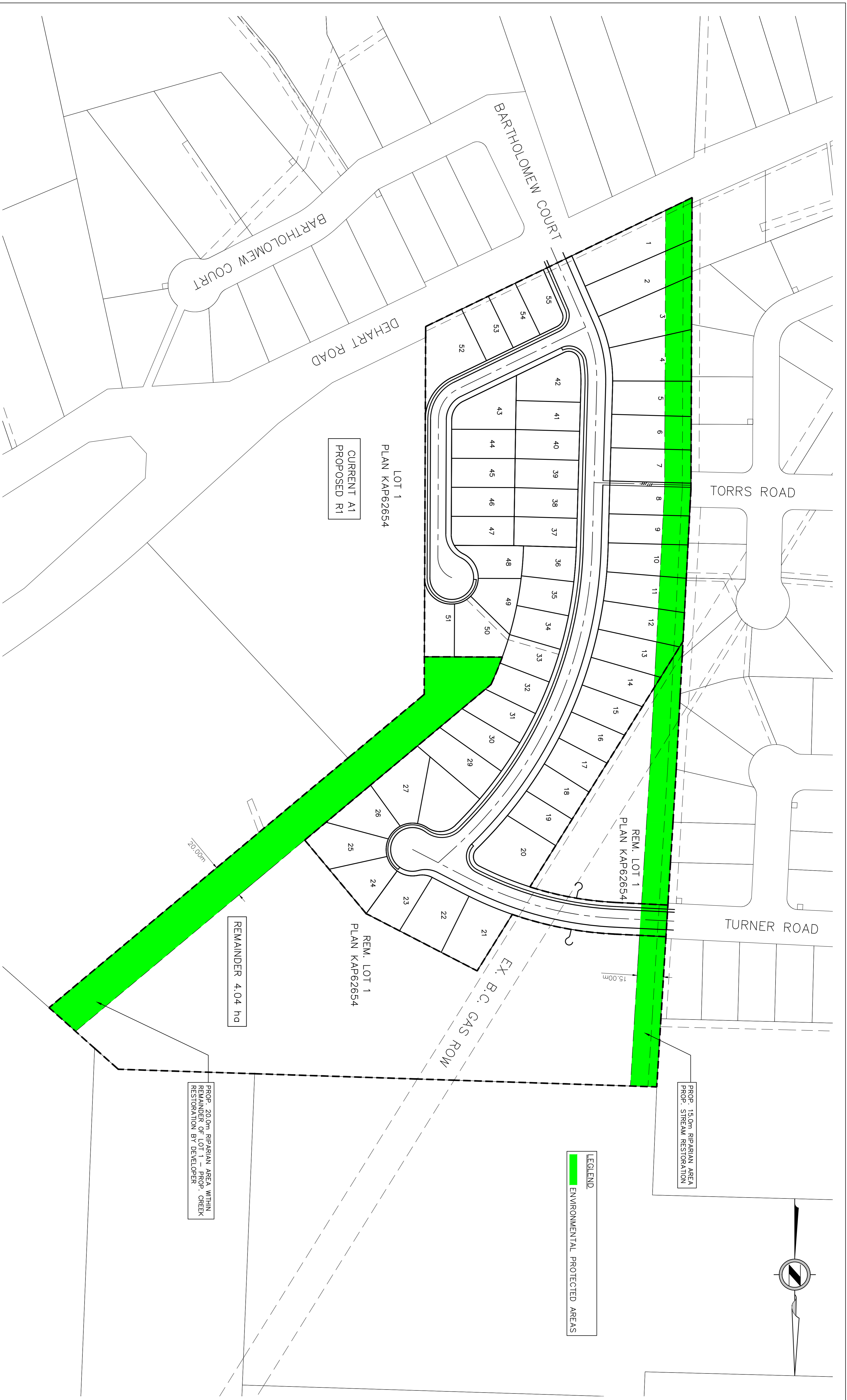
Panorama Photo from south Central Boundary of the Subject Site



MAP "A"



MAP "B"



LEGAL DESCRIPTION: LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.Y.D.

BM.	MONUMENT NO.	OCM73H/715	ELEVATION:	552.764
	LOCATED AT	HAZELL ROAD, 70m SOUTH OF THE TORRS ROAD INTERSECTION		
REV. NO.	DESCRIPTION	DR.	CH.	DATE
0	ISSUED FOR REZONING	TIG	MJT	14/04/11
				CAB

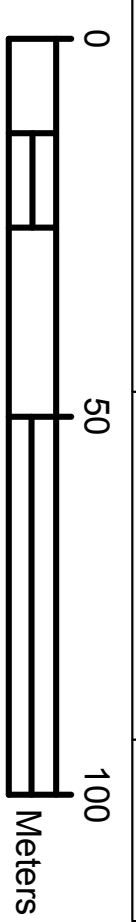
APLIN MARTIN CONSULTANTS LTD
 PROJECT MANAGEMENT • ENGINEERING • PLANNING • SURVEYING
 454 Leon Avenue, Kelowna, B.C., Canada V1Y 6J3
 Tel: (250) 448-0157, Fax: (250) 458-2312, E-mail: general@aplinmartin.com

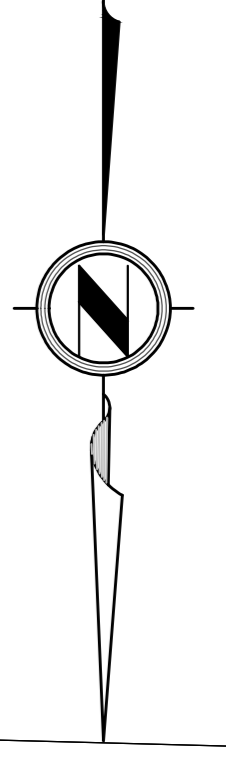
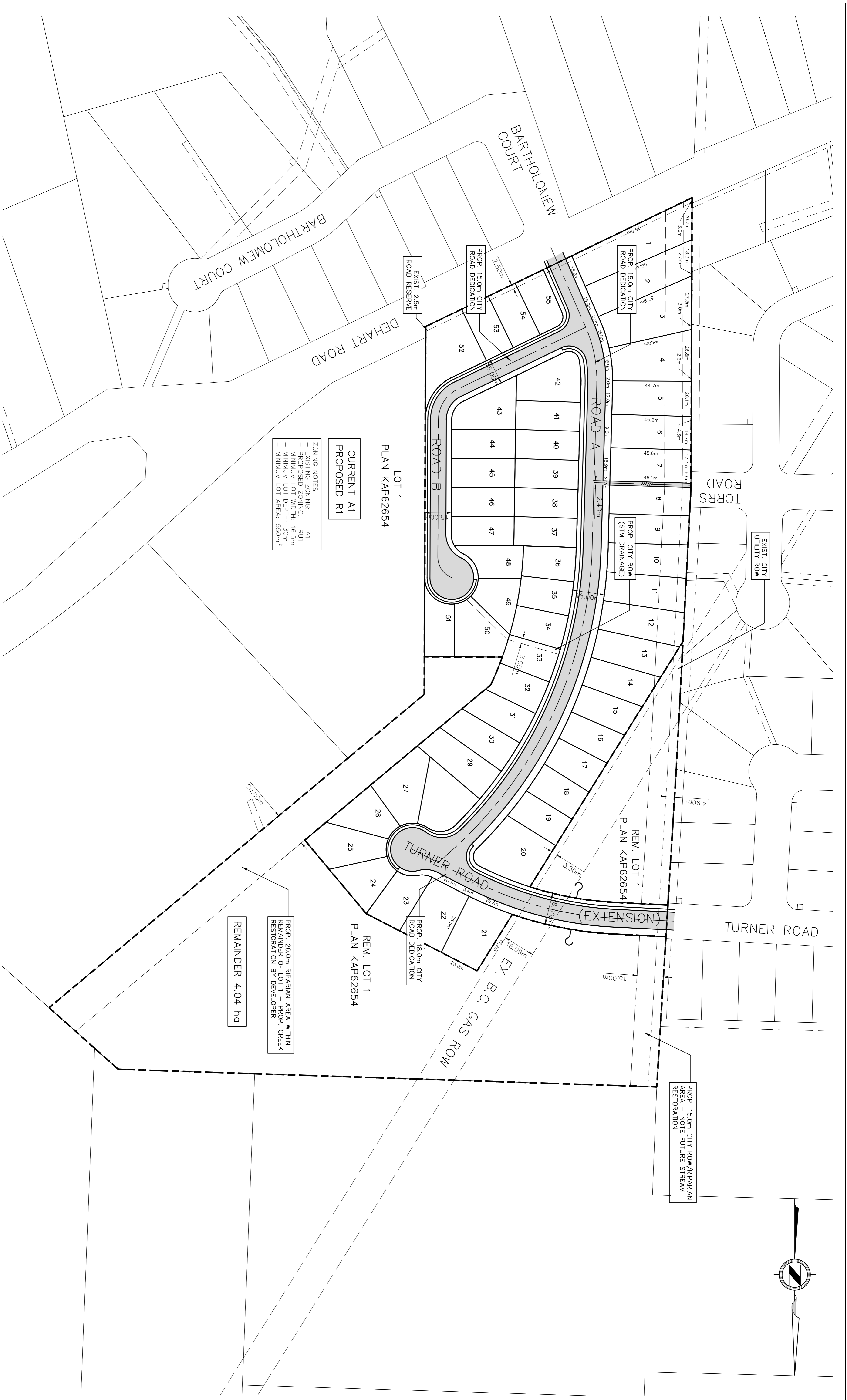
CLIENT: **NEW TOWN PLANNING SERVICES LTD.**
 1450 PANDOSY ST., KELOWNA BC, V1Y 1P3
 PH: (250) 860-9185

PROJECT: **RESIDENTIAL SUBDIVISION**
 984 DEHART RD., KELOWNA BC
 LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.Y.D.

The location of existing underground utilities are shown here for information only and are not to be independently verified by the owner or its representative. The location of all existing utilities exist before commencing work, and for any and all damages which might be incurred by the owner or its representative, the developer shall be liable to satisfy local and provincial laws and regulations and preserve any and all underground utilities.

TITLE:	RIPARIAN LAYOUT PLAN		
PROJECT NO.	SCALE:	HORIZ: 1:1000	VERT: N/A
DRAWING NO.	A & M DRAWING NO.	12-651 - NT02	
DESIGN: MJT	CHECK: MJT	DRAWN: TIG	APPR: CAB
A & M FILE:		12-651	
DRAWING DATE:		APRIL 2014	
SHEET NO.		1 OF 1	
REV:		0	





ZONING NOTES:
 - DIVISION ZONING: A1
 - PROPOSED ZONING: R1
 - MINIMUM LOT WIDTH: 16.5m
 - MINIMUM LOT DEPTH: 30m
 - MINIMUM LOT AREA: 500m²

LOT 1
 PLAN KAP62654
 CURRENT A1
 PROPOSED R1

PROP. 20.0m RIPARIAN AREA WITHIN
 REMAINDER OF LOT 1 - PROP. CREEK
 RESTORATION BY DEVELOPER

PROP. 15.0m CITY ROW/RIPARIAN
 AREA - NOTE FUTURE STREAM
 RESTORATION

PROP. 18.0m CITY
 ROAD DEDICATION

PROP. 15.0m CITY
 ROAD DEDICATION

PROP. 18.0m CITY
 ROAD DEDICATION

PROP. CITY ROW
 (STM DRAINAGE)

EXIST. 2.5m
 ROAD RESERVE

REMAINDER 4.04 ha

LEGAL DESCRIPTION: LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.Y.D.

BM.	MONUMENT NO.	OCM73H715	ELEVATION:	552.764
	LOCATED AT	HAZELL ROAD, 70m SOUTH OF THE TORRS ROAD INTERSECTION		
REV. NO.	DESCRIPTION	DR. CH.	DATE	APP.
0	ISSUED FOR REZONING	TIG MJT	14/04/11	CAB

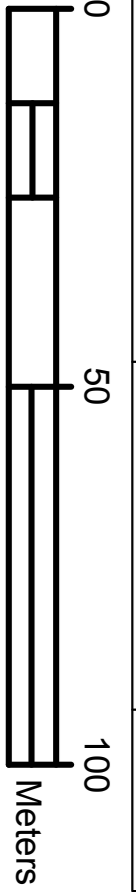
APLIN MARTIN CONSULTANTS LTD
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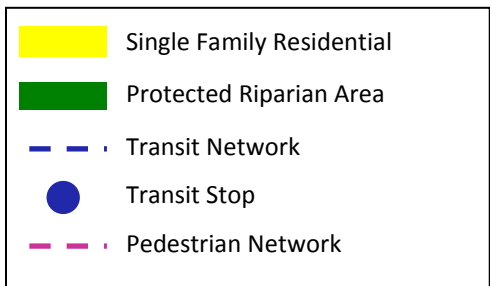
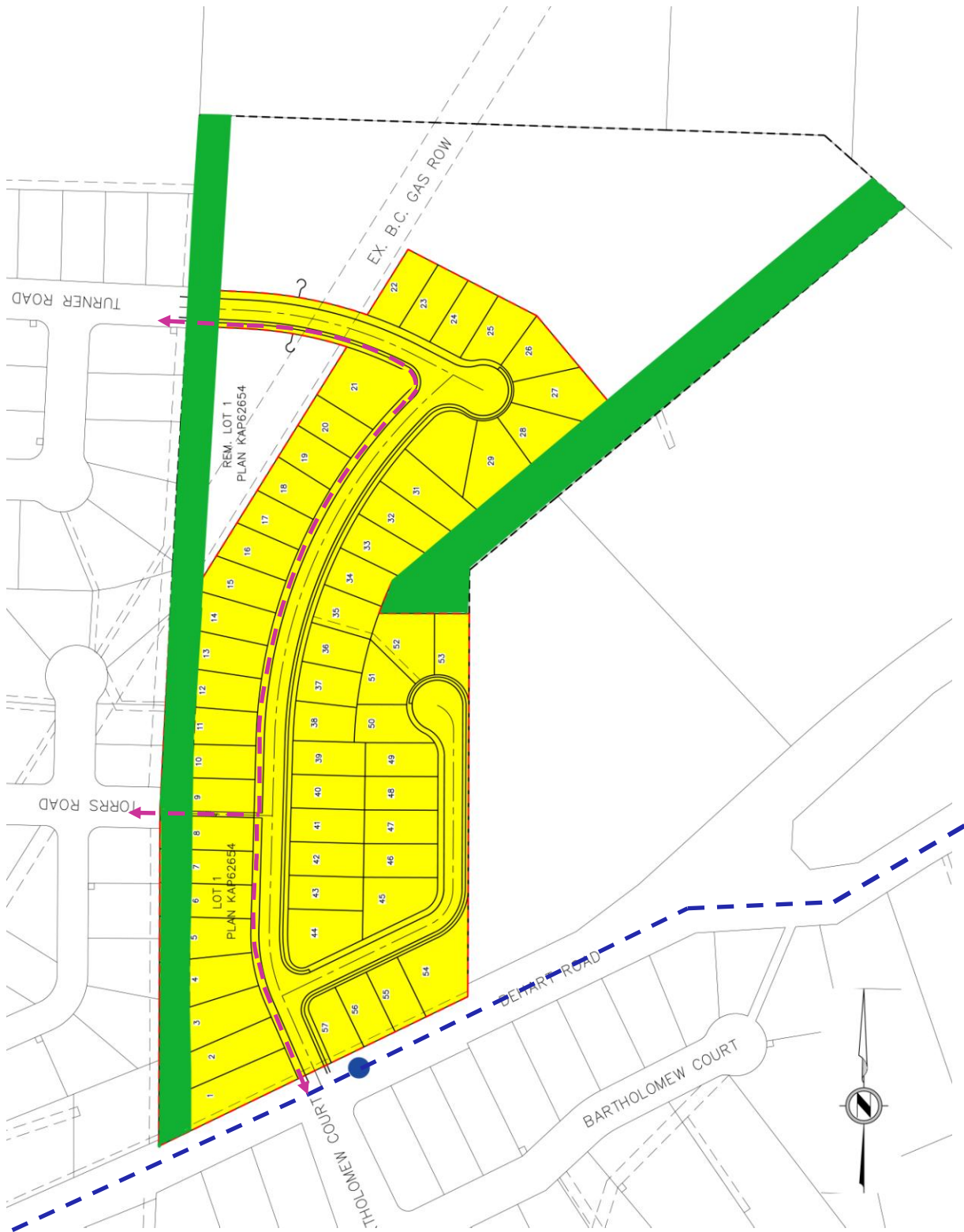
CLIENT: **NEW TOWN PLANNING SERVICES LTD.**
 1450 PANDOSY ST., KELOWNA BC, V1Y 1P3
 PH. (250) 860-9185

PROJECT: **RESIDENTIAL SUBDIVISION**
 984 DEHART RD., KELOWNA BC
 LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.Y.D.

The location of existing underground utilities are shown here for information only and are not to be independently verified by the owner or its representative. The location of all existing utilities exist before commencing work, and for any and all damages which might be incurred by the owner or its representative, the owner shall be responsible for any and all underground utilities.

TITLE:	LOT EASEMENT AND COVENANT PLAN		
PROJECT NO.	SCALE:	DESIGN: MJT	CHECK: MJT
DRAWING NO.	HORIZ: 1:1000	DRAWN: TIG	APPR: CAB
	VERT: N/A	A & M DRAWING NO.	FILE: 12-651
			DRAWING DATE: APRIL 2014
			SHEET NO. 1 OF 1
			REV. 0





Pedestrian and Transit Network Plan

984 Dehart Road

April 15, 2014

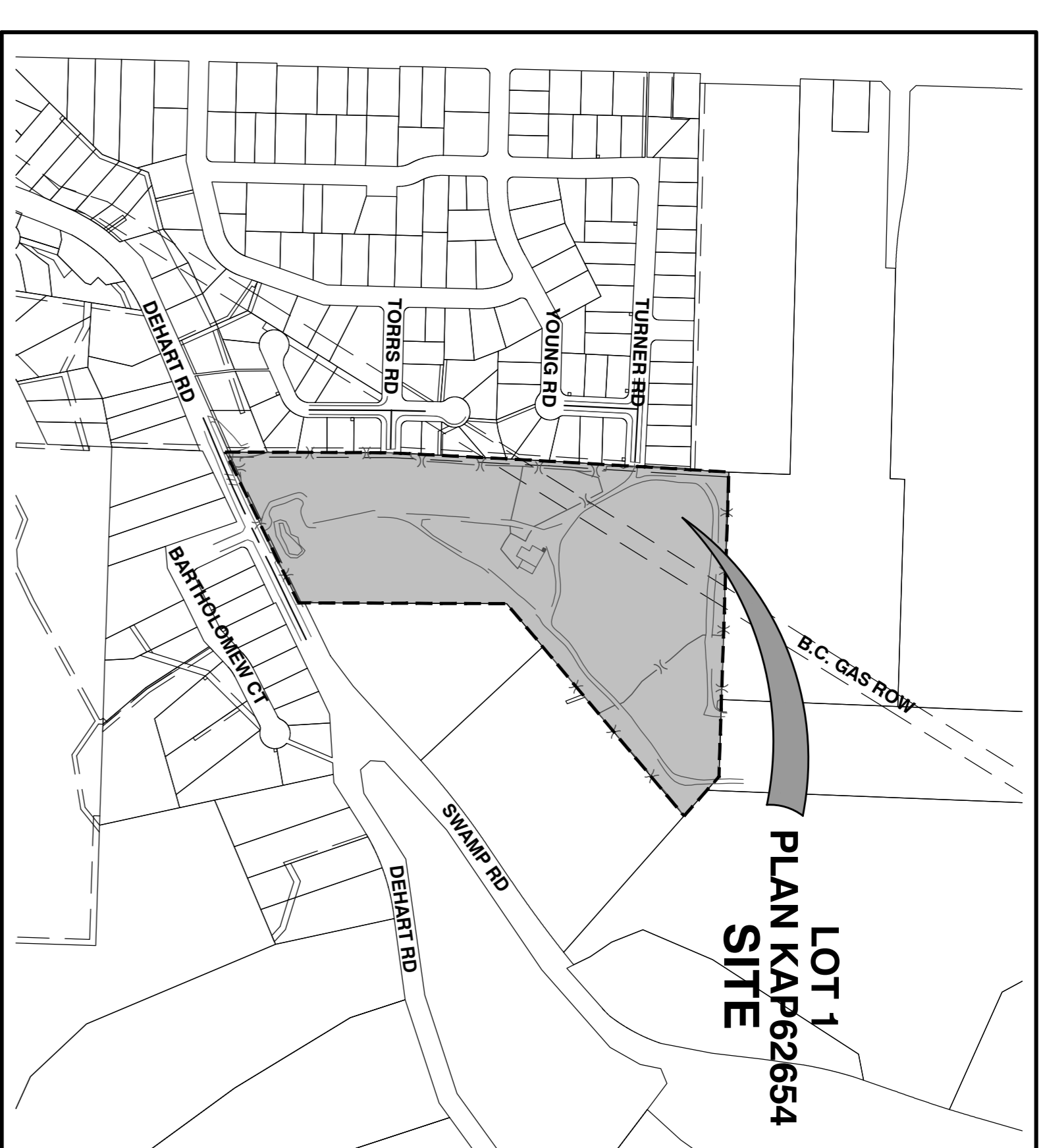
ENGINEERING SERVICES PROVIDED BY
APLIN & MARTIN
 CONSULTANTS LTD
 PROJECT MANAGEMENT ■ ENGINEERING ■ PLANNING ■ SURVEYING
 454 Leona Avenue, Kelowna, B.C. Canada V1Y 6J3
 Tel: (250) 448-0157, Fax: (778) 436-2312, E-mail: general@aplilmartin.com

DEVELOPER:

NEW TOWN PLANNING SERVICES LTD.
 164 ST. PAUL STREET
 KELOWNA, BC
 V1W 2C7

PROJECT:

RESIDENTIAL SUBDIVISION
 984 DEHART ROAD
 LOT 1, PLAN KAP62654, SEC 31, TP 29, O.D.Y.D.



DRAWING INDEX	
SHEET NUMBER	SHEET TITLE
01	COVER PAGE
02	GENERAL NOTES AND LEGEND
03	LOT SUBDIVISION PLAN
04	ROADWORKS - TURNER ROAD (EXTENSION)
05	ROADWORKS - ROAD A - STA. 0+1000 - 0+200
06	ROADWORKS - ROAD A - STA. 0+200 - 0+330
07	ROADWORKS - ROAD B
07	CREEK RESTORATION PLAN
09	STANDARD DETAILS

APLIN & MARTIN PROJECT NO. 12-651

A&M DRAWING NO.: 12-651-01

ENGINEERING DRAWING NOTES

A. CITY GENERAL NOTES

1. FOR THE PURPOSE OF CONSTRUCTION AND ACCOUNTABILITY, THE DEVELOPER MUST ADVISE THE DISTRICT INSPECTOR, IN WRITING, WHICH OF THE DISTRICT NOTES IS GOING TO BE THE RESPONSIBILITY OF THE DEVELOPER'S CONTRACTOR.
2. CONSTRUCTION IS TO BE IN ACCORDANCE WITH THE CITY OF KELOWNA SUBDIVISION, DEVELOPMENT, AND SERVICING BYLAW NO. 7900 AND THE APPLICABLE MUNICIPAL MASTER SPECIFICATIONS AND STANDARD DETAIL DRAWINGS.
3. TRAFFIC CONTROL IS THE RESPONSIBILITY OF THE DEVELOPER AND THE DEVELOPER SHALL COMPLY WITH SECTION 52 OF THE INDUSTRIAL HEALTH AND SAFETY REGULATIONS OF THE WORKERS' COMPENSATION BOARD OF B.C. AND THE INSTRUCTIONS OUTLINED ON THE CITY ROAD AND RIGHT-OF-WAY PERMIT AND TRAFFIC OBSTRUCTION PERMIT ISSUED BY THE CITY. THE DEVELOPER IS TO HAVE, ON SITE, A COPY OF THE CURRENT "B.C. TRAFFIC CONTROL MANUAL FOR WORK ON ROADWAYS" AS PUBLISHED BY THE MINISTRY OF TRANSPORTATION AND HIGHWAYS.
4. THE DEVELOPER SHALL BE RESPONSIBLE FOR OBTAINING ALL PERMITS FOR WORK WITHIN THE DISTRICT ROAD ALLOWANCE.
5. WHERE UTILITY OR SERVICE CROSSINGS ARE REQUIRED ACROSS EXISTING PAVEMENTS, AN UNDERGROUND METHOD OF INSTALLATION IS REQUIRED UNLESS SPECIAL APPROVAL IS GIVEN FROM THE CITY FOR AN OPEN CUT OPERATION. ALL EXISTING PAVEMENTS, BOULEVARDS, DRIVEWAYS, ETC., ARE TO BE REINSTATED TO ORIGINAL OR BETTER CONDITION AND IN ACCORDANCE WITH DISTRICT SPECIFICATIONS AND THE PAVEMENT CUT POLICY.
6. CITY ISA MONUMENT(S) ARE TO BE PROTECTED AND SHOULD THEY REQUIRE RAISING OR RELOCATING, THE DEVELOPER WILL NOTIFY THE CITY OF KELOWNA SURVEY DEPARTMENT AT LEAST THREE WORKING DAYS IN ADVANCE OF SCHEDULING WORK AFFECTING THEM. ANY ISA MONUMENT DISTURBED BY THE DEVELOPER SHALL BE REPLACED BY THE CITY OR A DISTRICT APPROVED BCIS ALL AT THE DEVELOPER'S EXPENSE. THE CITY WILL INVOICE THE DEVELOPER A FEE FOR EACH ISA MONUMENT DISTURBED AS FOLLOWS:
 - (1) LOWERS THE GRADE OF THE ROAD AT THE LOCATION OF AN ISA MONUMENT(S)
 - (2) RAISES THE GRADE OF THE ROAD AT THE LOCATION OF AN ISA MONUMENT(S), OR
 - (3) INSTALLS ANY UNDERGROUND UTILITIES (INCLUDING BC GAS, BC HYDRO, BC TELEPHONE, GVRD WATER/SANITARY SEWER/OR DRAINAGE ETC.) WITHIN 1.500 METER RADIUS OF THE ISA MONUMENT(S).
7. ALL STREET, TRAFFIC, AND ADVISORY SIGNS, PAVEMENT MARKINGS AND NO-POST GUARDRAILS REQUIRED BUT NOT NECESSARILY SHOWN ON THE DRAWINGS, SHALL BE INSTALLED BY THE CITY AT THE DEVELOPER'S COST.
8. WHERE INFILL OF EXISTING DITCHES IS REQUIRED OR WHERE SERVICES ARE CONSTRUCTED IN A FILL SECTION, FILL MATERIAL ARE TO BE IN ACCORDANCE WITH CITY SPECIFICATIONS AND ARE TO BE COMPACTED TO 95% OF MODIFIED PROCTOR DENSITY.
9. DRIVEWAY BOULEVARD CROSSINGS TO EACH OF THE PROPOSED LOTS ARE TO BE INSTALLED IN ACCORDANCE WITH THE CITY STANDARD DRAWINGS.
10. RESIDENTS DIRECTLY AFFECTED BY CONSTRUCTION OF THIS PROJECT MUST BE GIVEN 48 HOURS WRITTEN NOTICE OF THE PROPOSED START OF CONSTRUCTION. THE DEVELOPER WILL REQUIRE WRITTEN AUTHORIZATION FROM A PRIVATE PROPERTY OWNER WITH A COPY TO THE CITY, PRIOR TO ANY ENTRY ONTO PRIVATE PROPERTY AND A WRITTEN RELEASE, FROM THE PROPERTY OWNER, WHEN COMPLETED.
11. WHEN NATIVE SITE GRANULAR BACKFILL IS PROPOSED FOR USE IN TRENCHES, THE DEVELOPER SHALL EMPLOY A PROFESSIONAL ENGINEER WITH EXPERIENCE IN GEOTECHNICAL ENGINEERING FOR PERFORMANCE OF IN PLACE DENSITY AND SIEVE TESTING. SELECTION OF THE PROFESSIONAL ENGINEER AND USE OF THE SITE MATERIAL IS TO BE APPROVED BY THE CITY. THE SITE MATERIAL MUST FALL WITHIN ONE OF THE GRANULAR BACKFILL MATERIAL SPECIFICATIONS. RIVER SAND IS NOT ACCEPTABLE AS TRENCH BACKFILL MATERIAL.
12. THE DEVELOPER SHALL FACILITATE AND SUPPLY ALL NECESSARY SAFETY EQUIPMENT REQUIRED UNDER THE WORK REGULATIONS FOR THE DISTRICT OR IT'S REPRESENTATIVES OR THE ENGINEER OF RECORD TO INSPECT THE SANITARY SEWER AND STORM SEWER SYSTEMS. THE EQUIPMENT SHALL BE SUPPLIED UNTIL SUCH TIME AS A CERTIFICATE OF COMPLETION IS ISSUED BY THE CITY OF KELOWNA.
13. DEVELOPER IS TO VERIFY THE LOCATION AND ELEVATION OF ALL PIPES, OR OTHER UTILITY CROSSINGS, PRIOR TO CONSTRUCTION AND SHALL NOTIFY THE ENGINEER OF RECORD OF ANY CONFLICTS.
14. THE DEVELOPER SHALL EMPLOY A PROFESSIONAL ENGINEER TO DESIGN, INSTALL AND MAINTAIN A SEDIMENT AND EROSION CONTROL SYSTEM IN THE DEVELOPMENT IN ORDER TO PREVENT SILT DISCHARGES TO THE STORM DRAINAGE SYSTEM AND WATERCOURSES.

B. CITY ROADWORK NOTES

1. THE DEVELOPER SHALL EMPLOY A PROFESSIONAL ENGINEER WITH EXPERIENCE IN GEOTECHNICAL ENGINEERING FOR PERFORMANCE OF IN PLACE TESTING DURING THE PREPARATION OF THE SUBGRADE AND CONSTRUCTION OF THE ROAD STRUCTURE TO VERIFY THE ADEQUACY OF THE PROPOSED AND EXISTING ROAD STRUCTURE AND SUBGRADE. SELECTION OF THE PROFESSIONAL ENGINEER IS TO BE APPROVED BY THE CITY.
2. EXISTING VALVE BOXES, MANHOLES, ETC., WITHIN THE ROAD ALLOWANCE MUST BE ADJUSTED TO SUIT THE PROPOSED FINISHED GRADE.
3. ALL LOOSE, ORGANIC, OTHERWISE DELETERIOUS MATERIALS OR SOFT SPOT(S) ARE TO BE EXCAVATED AND REMOVED FROM THE ROADWAY AND UTILITY TRENCHES IN THE ROADWAY AS PER THE GEOTECHNICAL CONSULTANT'S REPORT OR AS DIRECTED BY THE DISTRICT.

C. CITY SANITARY SEWER AND STORM SEWER NOTES

1. UNLESS PRIOR APPROVAL IS GIVEN TO THE DEVELOPER BY THE DISTRICT, TIE-INS AND CONNECTIONS TO EXISTING SANITARY SEWERS ARE TO BE PERFORMED BY THE CITY AT THE DEVELOPER'S COST.
 2. ALL SERVICE CONNECTIONS SHALL BE MADE TO THE MAIN WHEREVER POSSIBLE. SHOULD A CONNECTION HAVE TO BE MADE TO A MANHOLE, THE CONNECTION INVERT SHALL BE AT THE SAME ELEVATION AS THE CROWN OF THE HIGHEST SEWER MAIN.
 3. ALL MANHOLES ARE TO BE A MINIMUM OF 1050mm DIAMETER UNLESS OTHERWISE NOTED.
 4. ALL SANITARY SEWER AND STORM SEWER SERVICE CONNECTIONS ARE TO BE A MINIMUM 100mm DIAMETER.
 5. ALL GRANULAR PIPE BEDDING SHALL BE EITHER TYPE 1 OR TYPE 2 ONLY AS PER THE CITY SPECIFICATIONS.
- D. CITY WATER WORKS NOTES**
1. THE DEVELOPER SHALL SUPPLY ALL MATERIALS AND FITTINGS REQUIRED FOR THE TIE-IN OF THE NEW WATER MAINS BY THE DISTRICT.
 2. ALL NEW WATER MAINS, AT TIE-IN POINTS, ARE TO BE CAPPED 1.5m FROM THE EXISTING WATER MAIN. THE PROPOSED WATER MAIN IS TO BE SET AT THE LINE AND GRADE TO MEET THE EXISTING WATER MAIN.
 3. TIE-INS TO EXISTING WATER MAINS AND FINAL TESTING AND CHLORINATION OF NEW MAINS IS TO BE PERFORMED BY THE CITY AT THE DEVELOPER'S COST.
 4. ALL DOMESTIC SERVICE CONNECTIONS WILL BE A MINIMUM OF 19mm DIAMETER UNLESS OTHERWISE SPECIFIED.
 5. WHERE 100mm DIAMETER PIPE IS USED IT WILL BE DUCTILE IRON (D.I.) AND SHALL CONFORM TO THE CITY SPECIFICATIONS.
 6. NO MAJORITY FITTINGS OR VALVES, ETC., ARE TO BE USED.

APLIN & MARTIN CONSULTANTS LTD.

A. GENERAL NOTES:

1. ALL TOPSOIL AND SEEDING TO BE IN ACCORDANCE WITH MASTER MUNICIPAL SPECIFICATIONS AND STANDARD DETAIL DRAWINGS AND CITY OF KELOWNA SUBDIVISION, DEVELOPMENT, AND SERVICING BYLAW NO. 7900.
2. BUILDING PERMIT APPLICATIONS WILL NOT BE RECEIVED OR PROCESSED UNTIL THE SANITARY SEWER, STORM SEWER, SILTATION PONDS, AND WATER MAINS HAVE BEEN TESTED, TIED-IN AND ARE OPERATIONAL. THE ASPHALT BASE COURSE HAS BEEN PLACED AND THE ROUGH LOT GRADING HAS BEEN CERTIFIED.

B. ROADWORKS NOTES:

1. CHANGES OF GRADE SHALL BE FORMED BY SMOOTH CURVES.
2. ALL SUBGRADE AND GRANULAR BASE MATERIALS TO BE COMPACTED TO 95% MODIFIED PROCTOR.
3. THE CONDITIONS FOR PLACING ASPHALT PAVEMENT AND P.C. CONCRETE SHALL BE IN ACCORDANCE WITH MASTER MUNICIPAL SPECIFICATIONS AND THE CITY OF KELOWNA SUBDIVISION, DEVELOPMENT, AND SERVICING BYLAW NO. 7900. SHOULD DEVIANCE BE ALLOWED FROM THESE SPECIFICATIONS, THE CONTRACTOR IS TO ASSUME ALL RESPONSIBILITY FOR THESE PRODUCTS.
4. ROAD CATCH BASINS TO BE SET TO ASPHALT BASE COURSE ELEVATION.
5. FINAL 35mm ASPHALT OVERLAY TO BE PLACED PRIOR TO END OF ONE YEAR MAINTENANCE PERIOD.

C. STORM SEWER NOTES:

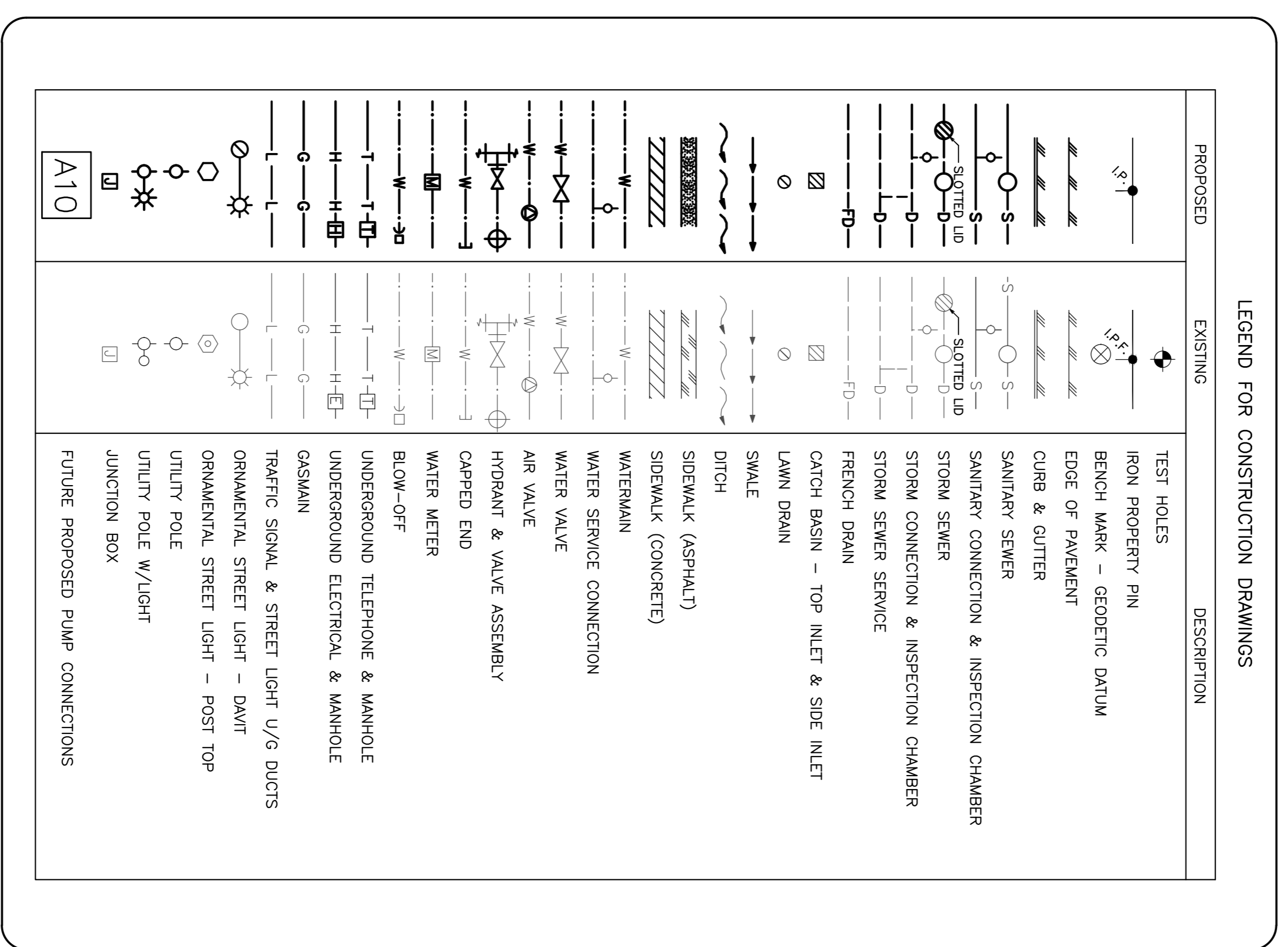
1. CONCRETE PIPE SHALL BE NON-REINFORCED A.S.T.M C-14 CLASS 3 OR REINFORCED CONFORMING TO A.S.T.M. C-76 CLASS III FOR SIZES UP TO AND INCLUDING 675mm DIA. FOR SIZES LARGER THAN 675mm DIA. CONCRETE PIPE SHALL BE REINFORCED A.S.T.M. C-76 CLASS III.
2. ALL JOINTS SHALL BE CLOSED JOINTS.

D. SANITARY SEWER NOTES:

1. SANITARY SEWER PIPE TO CONFORM WITH MASTER MUNICIPAL SPECIFICATIONS AND STANDARD DETAIL DRAWINGS AND THE CITY OF KELOWNA SUBDIVISION, DEVELOPMENT, AND SERVICING BYLAW NO. 7900.
2. ALL SANITARY SEWER CONNECTIONS TO BE PRE-PLUGGED, BY MANUFACTURER.

E. WATERWORKS NOTES:

1. DURING CONSTRUCTION AND AT ANY TIME PRIOR TO ACCEPTANCE AND PRESSURIZING OF WATER MAINS BY THE CITY, THE CONTRACTOR, ON BEHALF OF THE DEVELOPER, SHALL INSTALL A 300mm SQUARE x 18mm SHEET OF PLYWOOD OVER THE PUMPER NOZZLE OF EACH HYDRANT TO INDICATE THAT HYDRANT NOT IN USE.
2. MINIMUM GRADE OF WATER MAIN TO BE 0.10%.
3. WATER MAIN AND SERVICE CONNECTION PIPE TO CONFORM WITH MASTER MUNICIPAL SPECIFICATIONS AND STANDARD DETAIL DRAWINGS AND THE CITY OF KELOWNA SUBDIVISION, DEVELOPMENT, AND SERVICING BYLAW NO. 7900 THAT ARE APPLICABLE AT TIME OF CONSTRUCTION.
4. ALL FITTINGS AND VALVES SHALL BE DUCTILE IRON (D.I.) TYPON JOINT WITH CLOSED LUGS.
5. CURB STOPS TO BE FITTED WITH MUELLER TELESCOPING SERVICE BOXES OR APPROVED EQUAL.



CLIENT: NEW TOWN PLANNING SERVICES LTD.
1460 PANDOSY ST., KELOWNA BC V1Y 1P3
PH. (250) 860-9185

PROJECT: RESIDENTIAL SUBDIVISION
964 DEHART RD., KELOWNA BC
LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.V.D.

LEGAL DESCRIPTION: LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.V.D.

BM. LOCATED AT HAZELL ROAD, 20m SOUTH OF THE TORRS ROAD INTERSECTION	ELEVATION: 552.764				
REV. NO.	DESCRIPTION	DR. CH.	DATE	APP.	
0	ISSUED FOR REVIEW	TIG	MJT	13/01/02	JBK
1	ISSUED FOR DISCUSSION	TIG	MJT	13/03/03	JBK
2	ISSUED FOR REZONING	TIG	MJT	13/03/15	JBK

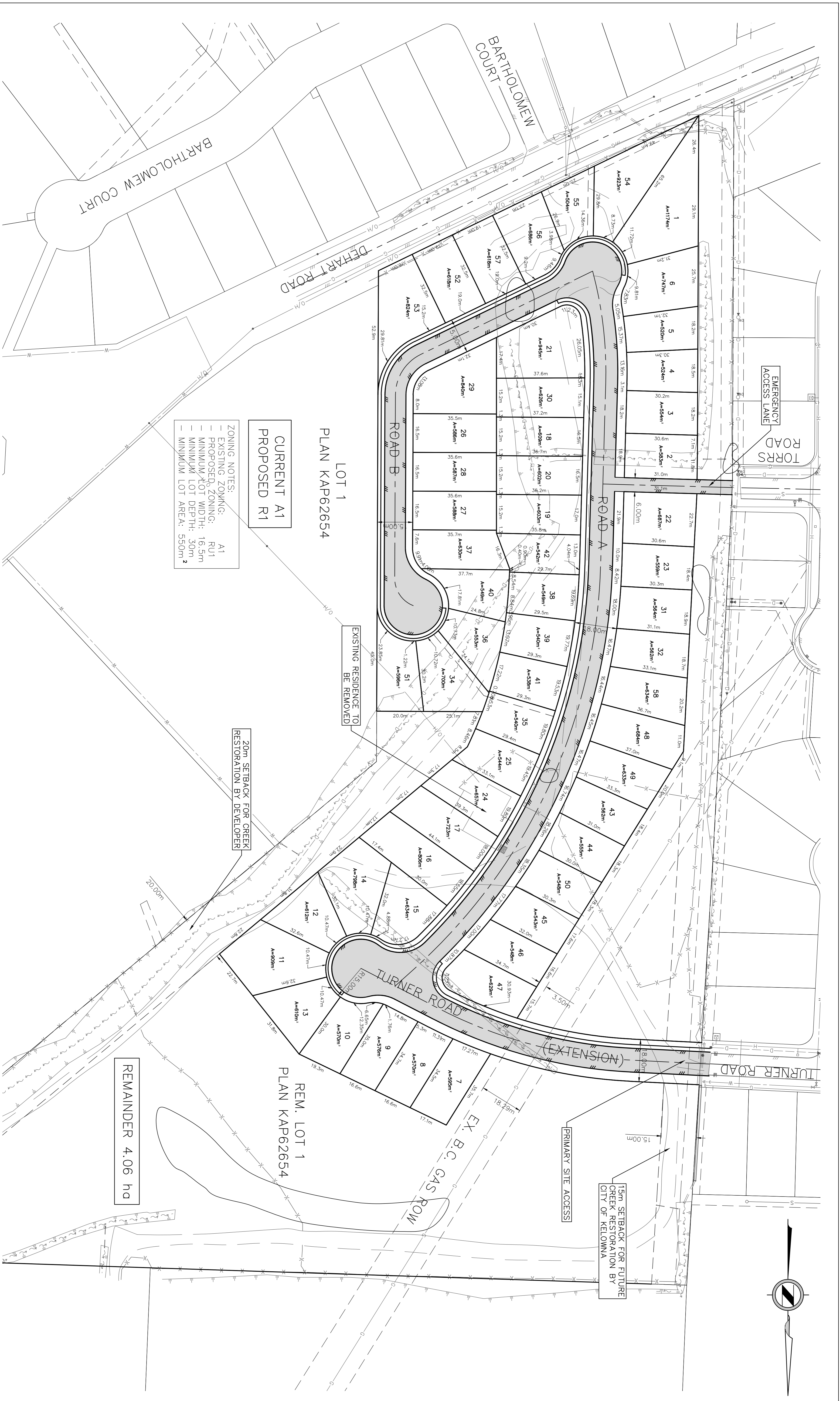
APLIN & MARTIN CONSULTANTS LTD.
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464 Leon Avenue, Kelowna, B.C. Canada V1Y 6A3
Tel: (250) 448-0157, Fax: (250) 458-2312, E-mail: general@aplinmartin.com

TITLE: GENERAL NOTES AND LEGEND

PROJECT NO.: 12-651
DRAWING NO.: A & M DRAWING NO. 12-651 - 02

DESIGN: MJT
DRAWN: TIG

CHECK: MJT
APPR: JBK
DRAWING DATE: MARCH 2013
SHEET NO.: 2 OF 9
REV: 2



LOT 1
PLAN KAP62654

CURRENT A1
PROPOSED R1

ZONING NOTES:
 - EXISTING ZONING: A1
 - PROPOSED ZONING: R1
 - MINIMUM LOT WIDTH: 16.5m
 - MINIMUM LOT DEPTH: 30m
 - MINIMUM LOT AREA: 550m²

LEGAL DESCRIPTION: LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.Y.D.

REV. NO.	DESCRIPTION	DR. CH.	DATE	APP.
0	ISSUED FOR REVIEW	TIG	13/01/02	JBK
1	ISSUED FOR DISCUSSION	TIG	13/03/05	JBK
2	ISSUED FOR REZONING	TIG	13/03/15	JBK

APLIN MARTIN CONSULTANTS LTD.
 PROJECT MANAGEMENT • ENGINEERING • PLANNING • SURVEYING
 454 Leon Avenue, Kelowna, B.C., Canada V1Y 6J3
 Tel: (250) 488-0157, Fax: (250) 458-2312, Email: general@aplmartin.com

CLIENT: **NEW TOWN PLANNING SERVICES LTD.**
 1450 PANDOSY ST., KELOWNA BC V1Y 1P3
 PH. (250) 860-9185

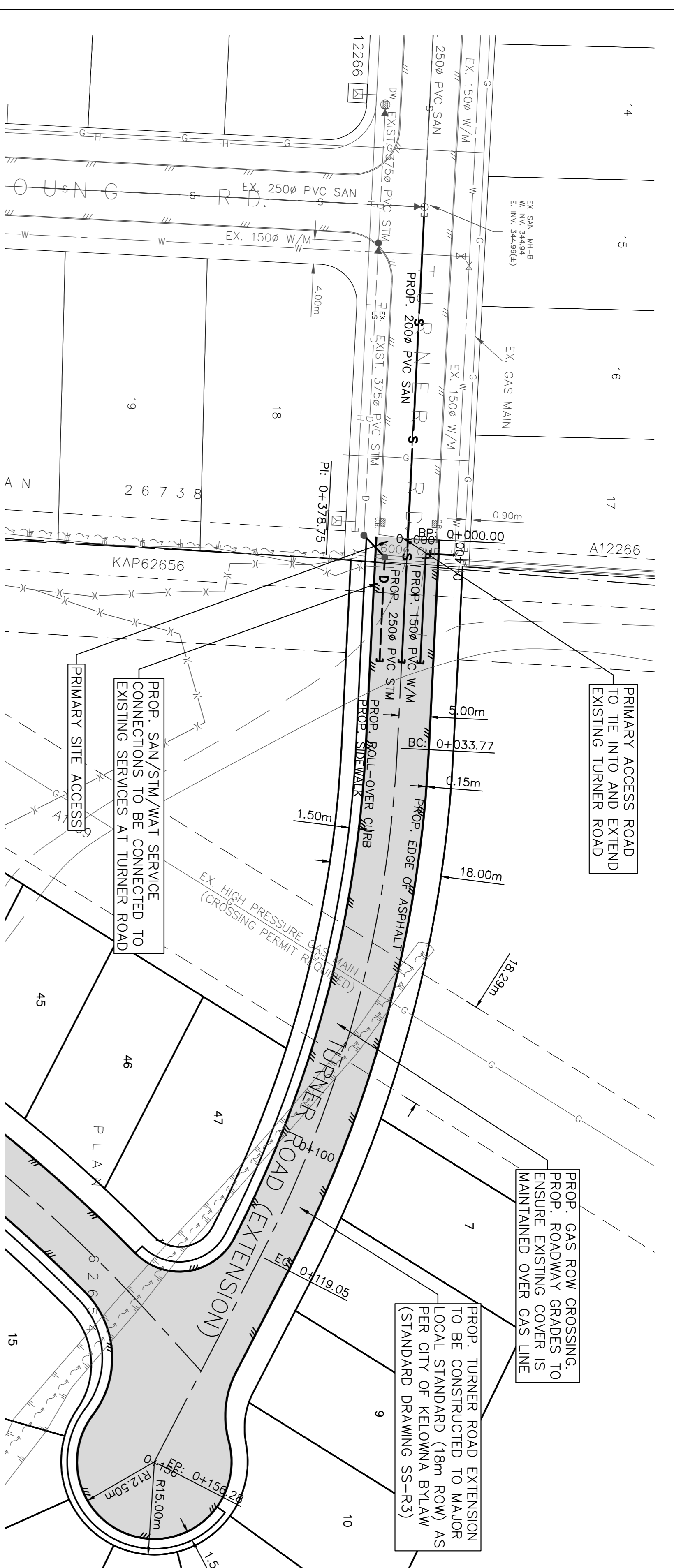
PROJECT: **RESIDENTIAL SUBDIVISION**
 984 DEHART RD., KELOWNA BC
 LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.Y.D.

TITLE: **LOT SUBDIVISION PLAN**

PROJECT NO. _____

SCALE: HORIZ: 1:750
 VERT: N/A
 A & M DRAWING NO. **12-651 - 03**

DESIGN: MJT	CHECK: MJT
DRAWN: TIG	APPR: JBK
A & M FILE: 12-651	
DRAWING DATE: MARCH 2013	
SHEET NO. 3 OF 9	REV. 2



REM. LOT 1
PLAN KAP626

	ORIGINAL CONTOUR
	EXISTING ELEVATION
	EXISTING ELEVATION TO BE MATCHED
	PROPOSED ELEVATION
	EXISTING CATCH BASIN
	PROPOSED CATCH BASIN
	EXISTING MANHOLE
	EXISTING WATER VALVE
	PROPOSED WATER VALVE
	EXISTING FIRE HYDRANT
	PROPOSED FIRE HYDRANT
	PROPOSED PIPE CAP
	NEW/REPLACEMENT ASPHALT

ENGINEERING DRAWING NOTES

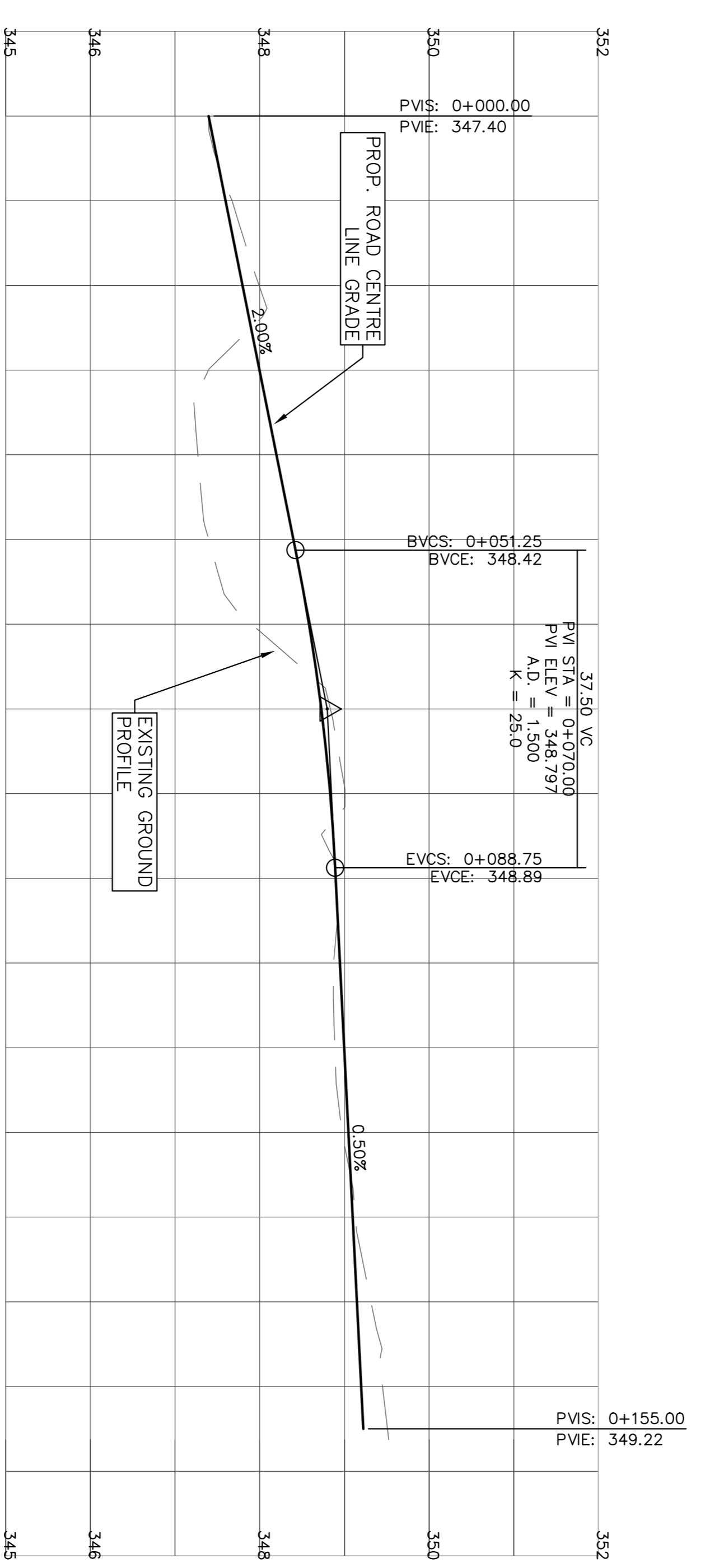
CITY ROADWORK NOTES

1. THE DEVELOPER SHALL EMPLOY A PROFESSIONAL ENGINEER WITH EXPERIENCE IN GEOTECHNICAL ENGINEERING FOR PERFORMANCE OF IN PLACE TESTING DURING THE PREPARATION OF THE SUBGRADE AND CONSTRUCTION OF THE ROAD STRUCTURE TO VERIFY THE ADEQUACY OF THE PROPOSED AND EXISTING ROAD STRUCTURE AND SUBGRADE. SELECTION OF THE PROFESSIONAL ENGINEER IS TO BE APPROVED BY THE CITY.
2. EXISTING VALVE BOXES, MANHOLES, ETC., WITHIN THE ROAD ALLOWANCE MUST BE ADJUSTED TO SUIT THE PROPOSED FINISHED GRADE.
3. ALL LOOSE, ORGANIC, OTHERWISE DELETERIOUS MATERIALS OR SOFT SPOTS ARE TO BE EXCAVATED AND REMOVED FROM THE ROADWAY AND UTILITY TRENCHES IN THE ROADWAY AS PER THE GEOTECHNICAL CONSULTANT'S REPORT OR AS DIRECTED BY THE DISTRICT.

APLUN & MARTIN CONSULTANTS LTD.

ROADWORKS NOTES:

1. CHANGES OF GRADE SHALL BE FORMED BY SMOOTH CURVES.
2. ALL SUBGRADE AND GRANULAR BASE MATERIALS TO BE COMPACTED TO 95% MODIFIED PROCTOR.
3. THE CONDITIONS FOR PLACING ASPHALT PAVEMENT AND P.C. CONCRETE SHALL BE IN ACCORDANCE WITH MASTER MUNICIPAL SPECIFICATIONS AND THE CITY OF KELOWNA SUBDIVISION, DEVELOPMENT, AND SERVICING BYLAW NO. 7900. SHOULD DEVANCE BE ALLOWED FROM THESE SPECIFICATIONS, THE CONTRACTOR IS TO ASSUME ALL RESPONSIBILITY FOR THESE PRODUCTS.
4. ROAD CATCH BASINS TO BE SET TO ASPHALT BASE COURSE ELEVATION.
5. FINAL 35mm ASPHALT OVERLAY TO BE PLACED PRIOR TO END OF ONE YEAR MAINTENANCE PERIOD.



CHAINAGE	CENTRELINE ROAD ELEVATIONS	EXISTING	PROPOSED
0+010			
0+000	347.40	347.40	
0+020	347.99	347.80	
0+040	347.27	348.20	
0+060	347.91	348.58	
0+080	349.01	348.83	
0+100	348.87	348.95	
0+120	348.97	349.05	
0+140	349.31	349.15	
0+160			
0+170			

LEGAL DESCRIPTION: LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.Y.D.

REV. NO.	DESCRIPTION	DR. CH.	DATE	APP.
0	ISSUED FOR REVIEW	TIG	MJT 13/01/02	JBK
1	ISSUED FOR DISCUSSION	TIG	MJT 13/03/05	JBK
2	ISSUED FOR REZONING	TIG	MJT 13/03/15	JBK

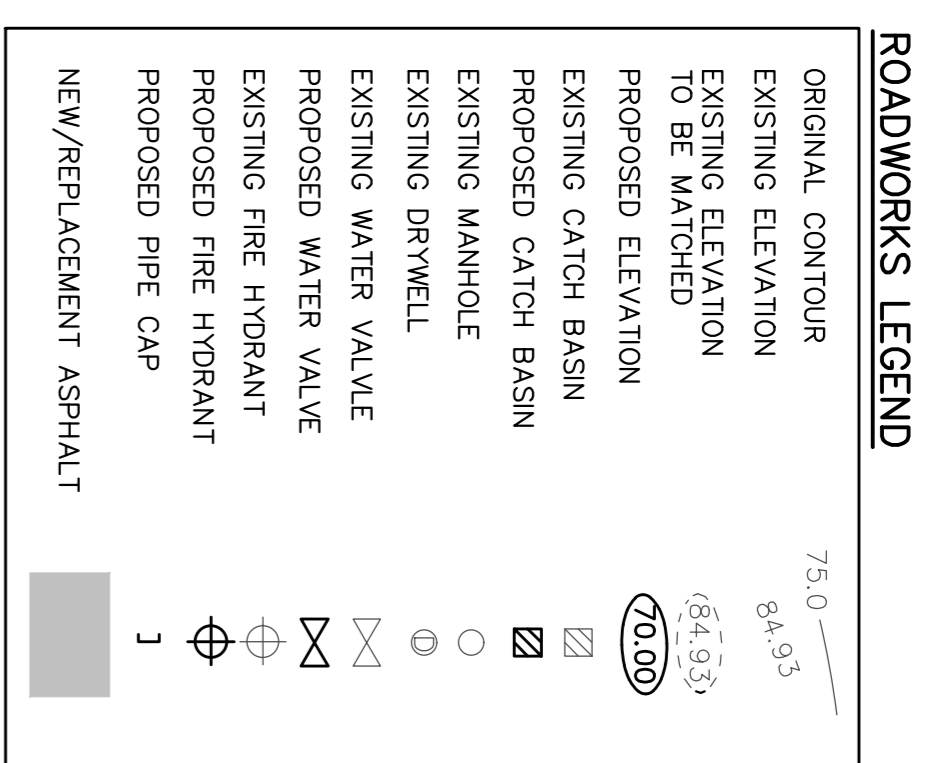
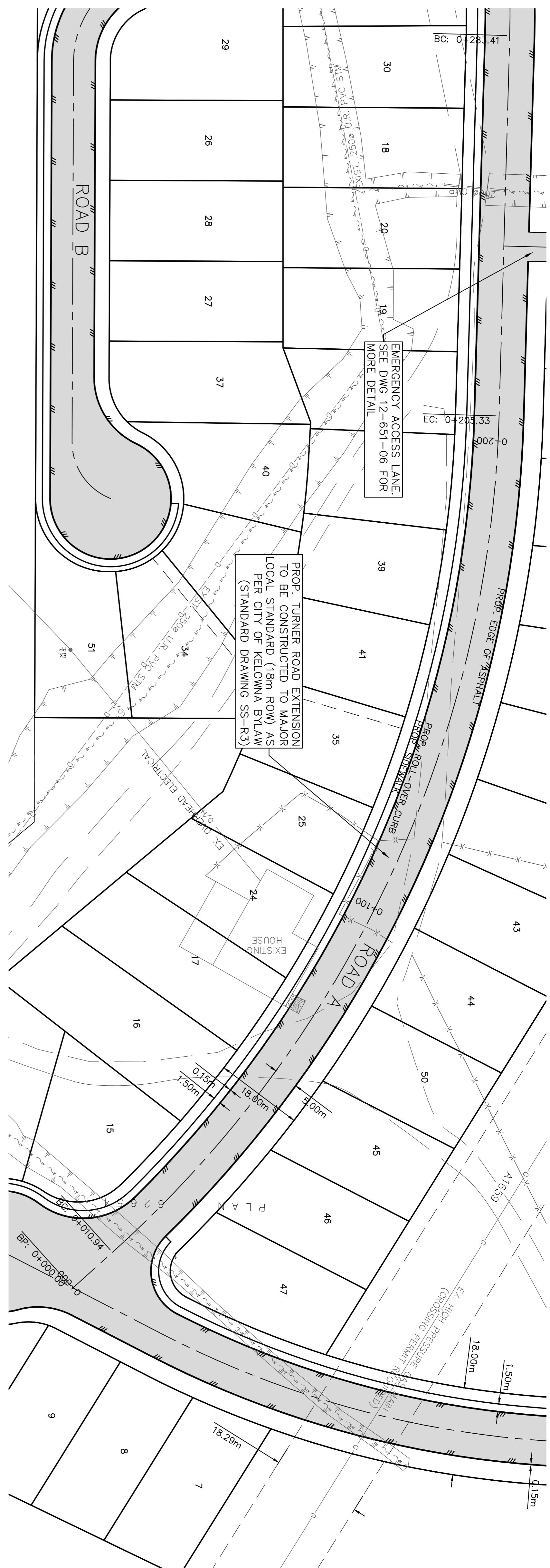
APLUN & MARTIN CONSULTANTS LTD.
PROJECT MANAGEMENT • ENGINEERING • PLANNING • SURVEYING
454 Leon Avenue, Kelowna, B.C. Canada V1Y 6J3
Tel: (250) 448-0157, Fax: (250) 458-2312, E-mail: general@aplunmartin.com

CLIENT: NEW TOWN PLANNING SERVICES LTD.
1450 PANDOSY ST., KELOWNA BC V1Y 1P3
PH: (250) 860-8185

PROJECT: RESIDENTIAL SUBDIVISION
984 DEHART RD., KELOWNA BC
LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.Y.D.

The location of existing underground utilities are shown here not being independently verified by the owner or its representative. The location of all existing utilities exist before commencing work, and for any and all damages which might be incurred by the contractor or its representative to existing underground utilities, the contractor shall be responsible.

TITLE:	ROADWORKS - TURNER ROAD (EXTENSION)	
PROJECT NO.	12-651	SCALE: HORIZ: 1:500 VERT: 1:50
DRAWING NO.	A & M DRAWING NO. 12-651 - 04	DRAWING DATE: MARCH 2013
		SHEET NO. 4 OF 9
		REV. 2



ENGINEERING DRAWING NOTES

CITY ROADWORK NOTES

1. THE DEVELOPER SHALL EMPLOY A PROFESSIONAL ENGINEER WITH EXPERIENCE IN GEOTECHNICAL ENGINEERING FOR PERFORMANCE OF IN PLACE TESTING DURING THE PREPARATION OF THE SUBGRADE AND CONSTRUCTION OF THE ROAD STRUCTURE TO VERIFY THE ADEQUACY OF THE PROPOSED AND EXISTING ROAD STRUCTURE AND SUBGRADE. SELECTION OF THE PROFESSIONAL ENGINEER IS TO BE APPROVED BY THE CITY.
2. EXISTING VALVE BOXES, MANHOLES, ETC., WITHIN THE ROAD ALLOWANCE MUST BE ADJUSTED TO SUIT THE PROPOSED FINISHED GRADE.
3. ALL LOOSE, ORGANIC, OTHERWISE DELETERIOUS MATERIALS OR SOFT SPOT(S) ARE TO BE EXCAVATED AND REMOVED FROM THE ROADWAY AND UTILITY TRENCHES IN THE ROADWAY AS PER THE GEOTECHNICAL CONSULTANT'S REPORT OR AS DIRECTED BY THE DISTRICT.

APLIN & MARTIN CONSULTANTS LTD.

ROADWORKS NOTES:

1. CHANGES OF GRADE SHALL BE FORMED BY SMOOTH CURVES.
2. ALL SUBGRADE AND GRANULAR BASE MATERIALS TO BE COMPACTED TO 95% MODIFIED PROCTOR.
3. THE CONDITIONS FOR PLACING ASPHALT PAVEMENT AND P.C. CONCRETE SHALL BE IN ACCORDANCE WITH MASTER MUNICIPAL SPECIFICATIONS AND THE CITY OF KELOWNA SUBDIVISION, DEVELOPMENT, AND SERVING BYLAW NO. 7900. SHOULD DEVIANCE BE ALLOWED FROM THESE SPECIFICATIONS, THE CONTRACTOR IS TO ASSUME ALL RESPONSIBILITY FOR THESE PRODUCTS.
4. ROAD CATCH BASINS TO BE SET TO ASPHALT BASE COURSE ELEVATION.
5. FINAL 35mm ASPHALT OVERLAY TO BE PLACED PRIOR TO END OF ONE YEAR MAINTENANCE PERIOD.

STATION	EXISTING ELEVATION	PROPOSED ELEVATION	CHAINAGE
0+260	356.80	356.99	
0+240	354.91	356.26	
0+220	355.22	355.70	
0+200	354.47	354.91	
0+180	353.67	354.06	
0+160	352.74	353.21	
0+140	351.78	352.46	
0+120	351.44	351.86	
0+100	351.23	351.36	
0+080	351.13	350.86	
0+060	350.98	350.36	
0+040	350.61	349.86	
0+020	350.41	349.36	
0+000	349.31		
0+000			CHAINAGE
0+000			CENTRELINE ROAD ELEVATIONS
0+000			EXISTING PROPOSED

LEGAL DESCRIPTION: LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.Y.D.

REV. NO.	DESCRIPTION	DR. CH.	DATE	APP.
0	ISSUED FOR REVIEW	TIG	MJT 13/01/02	JBK
1	ISSUED FOR DISCUSSION	TIG	MJT 13/03/05	JBK
2	ISSUED FOR REZONING	TIG	MJT 13/03/15	JBK

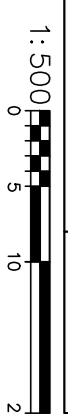
APLIN & MARTIN CONSULTANTS LTD.
 PROJECT MANAGEMENT • ENGINEERING • PLANNING • SURVEYING
 454 Leon Avenue, Kelowna, B.C. Canada V1Y 6A3
 Tel: (250) 448-0157, Fax: (250) 458-2312, E-mail: general@aplinmartin.com

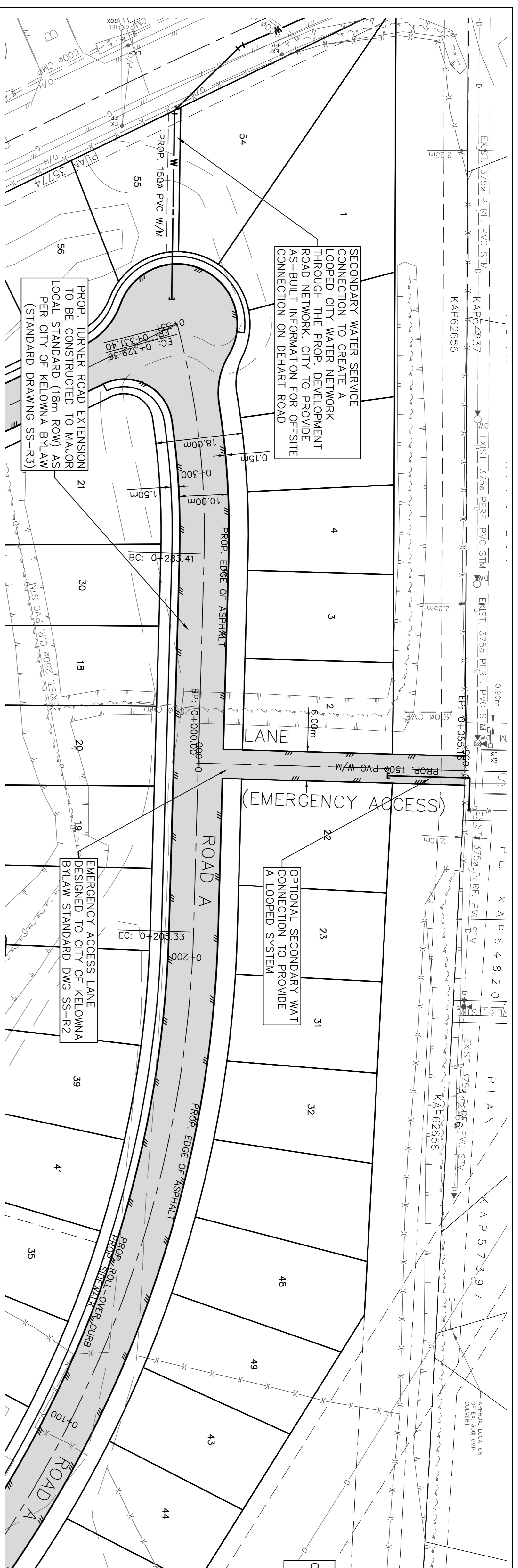
CLIENT: **NEW TOWN PLANNING SERVICES LTD.**
 1480 PANDOSY ST., KELOWNA BC V1Y 1P3
 PH: (250) 860-8185

PROJECT: **RESIDENTIAL SUBDIVISION**
 984 DEHART RD., KELOWNA BC
 LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.Y.D.

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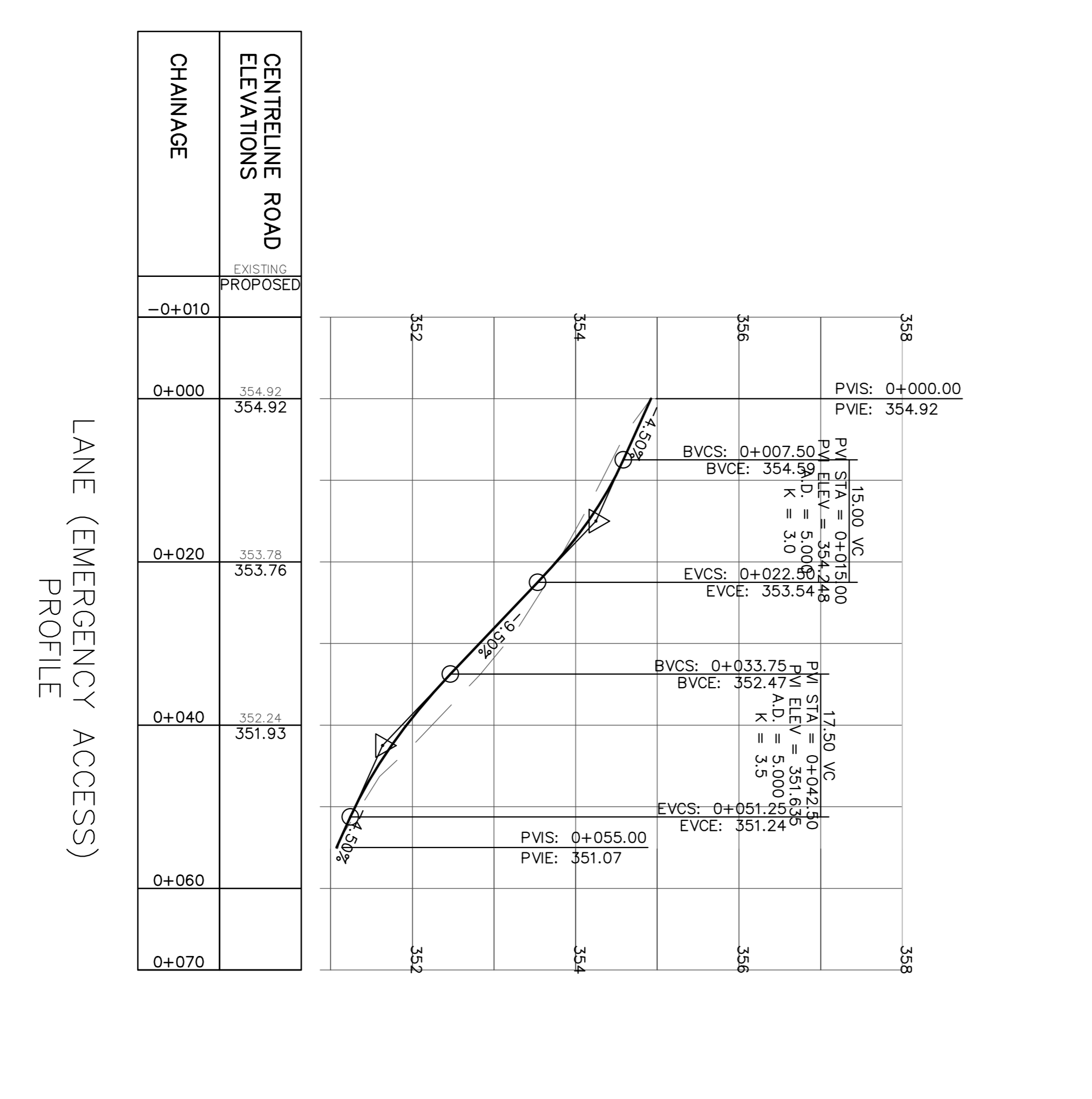
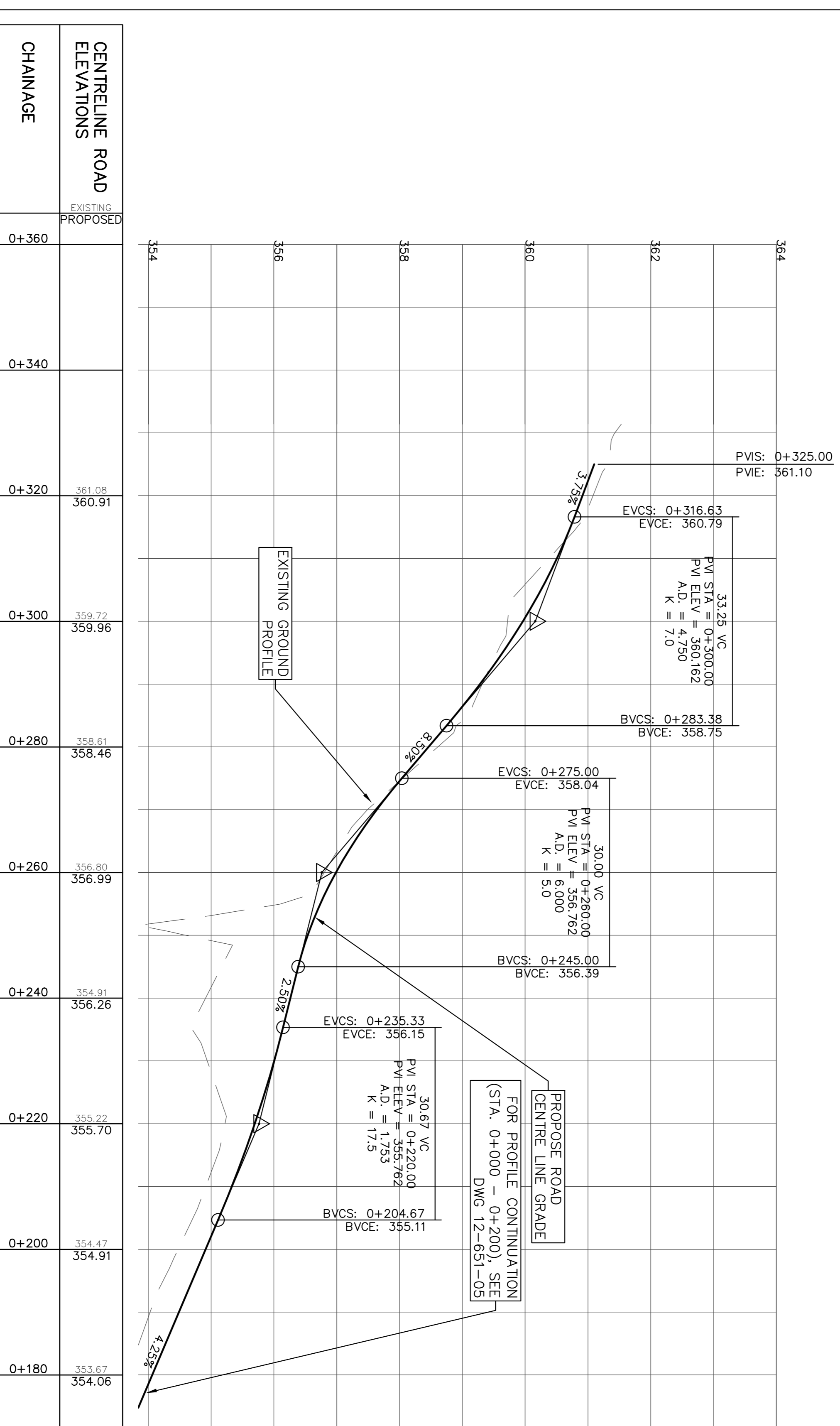
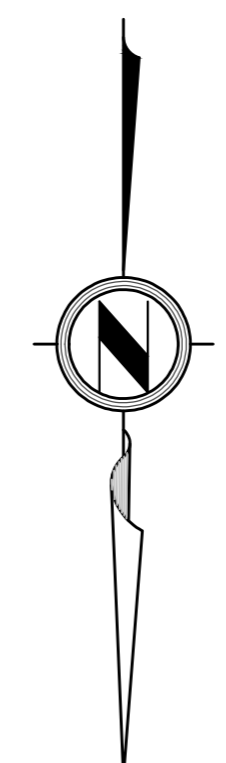
TITLE:	ROADWORKS - ROAD A	DESIGN: MJT	CHECK: MJT
PROJECT NO.:	STA 0+000 - 0+200	DRAWN: TIG	APPR: JBK
SCALE:	HORIZ: 1:500 VERT: 1:50	A & M FILE:	12-651
DRAWING NO.:	12-651 - 05	DRAWING DATE:	MARCH 2013
SHEET NO.:	5 OF 9	SHEET NO.:	REV. 2





ROADWORKS LEGEND

- 75.0 - ORIGINAL CONTOUR
- 84.95 - EXISTING ELEVATION
- (70.00) - EXISTING ELEVATION TO BE MATCHED
- EXISTING CATCH BASIN
- EXISTING DRYWELL
- EXISTING WATER VALVE
- PROPOSED WATER VALVE
- EXISTING FIRE HYDRANT
- PROPOSED FIRE HYDRANT
- PROPOSED PIPE CAP
- NEW/REPLACEMENT ASPHALT



CHAINAGE	EXISTING ELEVATIONS	PROPOSED ELEVATIONS
0+010		
0+000	354.92	354.92
0+020	353.78	353.76
0+040	352.24	351.93
0+060		
0+070		

LANE (EMERGENCY ACCESS) PROFILE

LEGAL DESCRIPTION: LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.Y.D.

BM. MONUMENT NO. OCM7341715 ELEVATION: 552.764
 LOCATED AT HAZELL ROAD, 70m SOUTH OF THE TORRS ROAD INTERSECTION

REV. NO.	DESCRIPTION	DR. CH.	DATE	APP.
0	ISSUED FOR REVIEW	TIG MJT	13/01/02	JBK
1	ISSUED FOR DISCUSSION	TIG MJT	13/03/02	JBK
2	ISSUED FOR REZONING	TIG MJT	13/03/15	JBK

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 Tel: (250) 480-0157, Fax: (250) 458-2312, E-mail: general@aplmartin.com

CLIENT: **NEW TOWN PLANNING SERVICES LTD.**
 1480 PANDOSY ST., KELOWNA BC V1Y 1P3
 PH: (250) 860-9188

PROJECT: **RESIDENTIAL SUBDIVISION**
 984 DEHART RD., KELOWNA BC
 LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.Y.D.

TITLE: **ROADWORKS - ROAD A**
STA 0+200 - 0+330

PROJECT NO. **12-651**

SCALE: HORIZ: 1:500
 VERT: 1:50

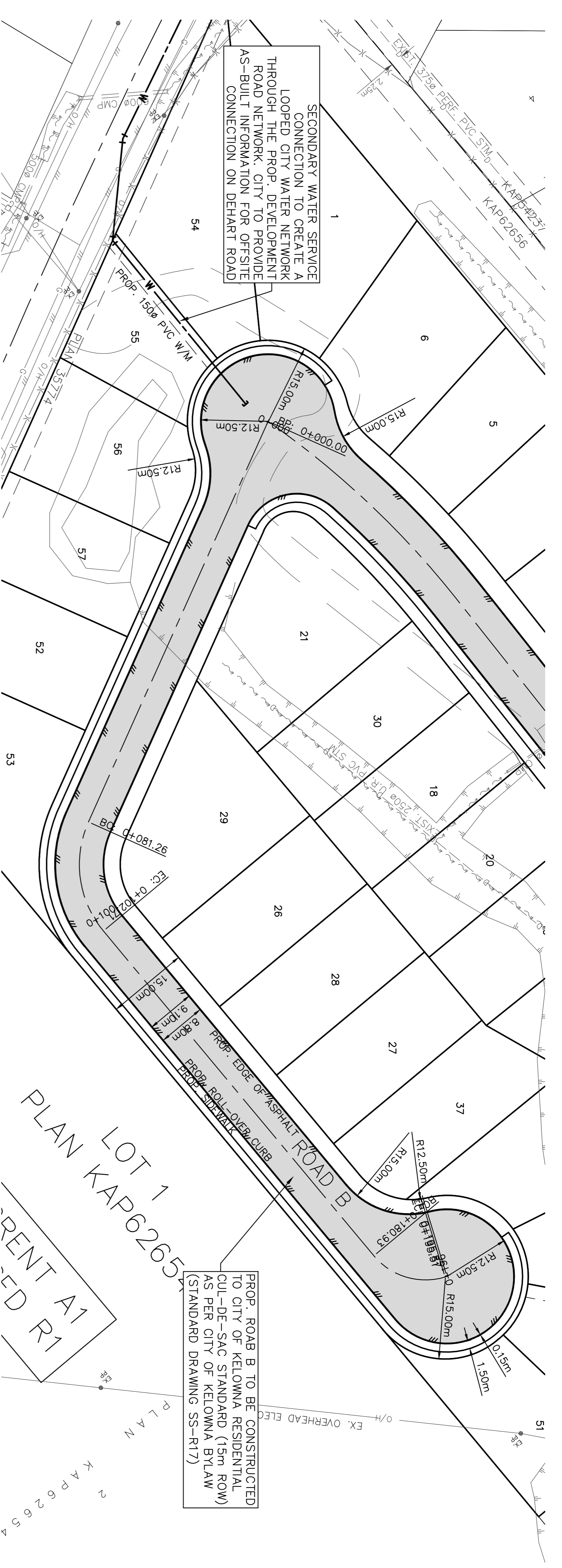
DRAWING NO. **12-651 - 06**

SHEET NO. **6 OF 9**

DRAWING DATE: **MARCH 2013**

DESIGN: MJT CHECK: MJT
 DRAWN: TIG APPR: JBK

The location of existing underground utilities are shown here for information only and are not to be independently verified by the owner or its representative. The location of all existing utilities shall be confirmed by the owner before commencing work, and for any and all damages which might be incurred by the owner, the contractor shall be held liable to notify the owner of any and all underground utilities.



ROADWORKS LEGEND

75.0	ORIGINAL CONTOUR
84.93	EXISTING ELEVATION
84.93	EXISTING ELEVATION TO BE MATCHED
70.00	PROPOSED ELEVATION
⊗	EXISTING CATCH BASIN
⊗	PROPOSED CATCH BASIN
⊗	EXISTING MANHOLE
⊗	EXISTING DRYWELL
⊗	PROPOSED WATER VALVE
⊗	EXISTING FIRE HYDRANT
⊗	PROPOSED FIRE HYDRANT
⊗	PROPOSED PIPE CAP
1	NEW/REPLACEMENT ASPHALT

ENGINEERING DRAWING NOTES

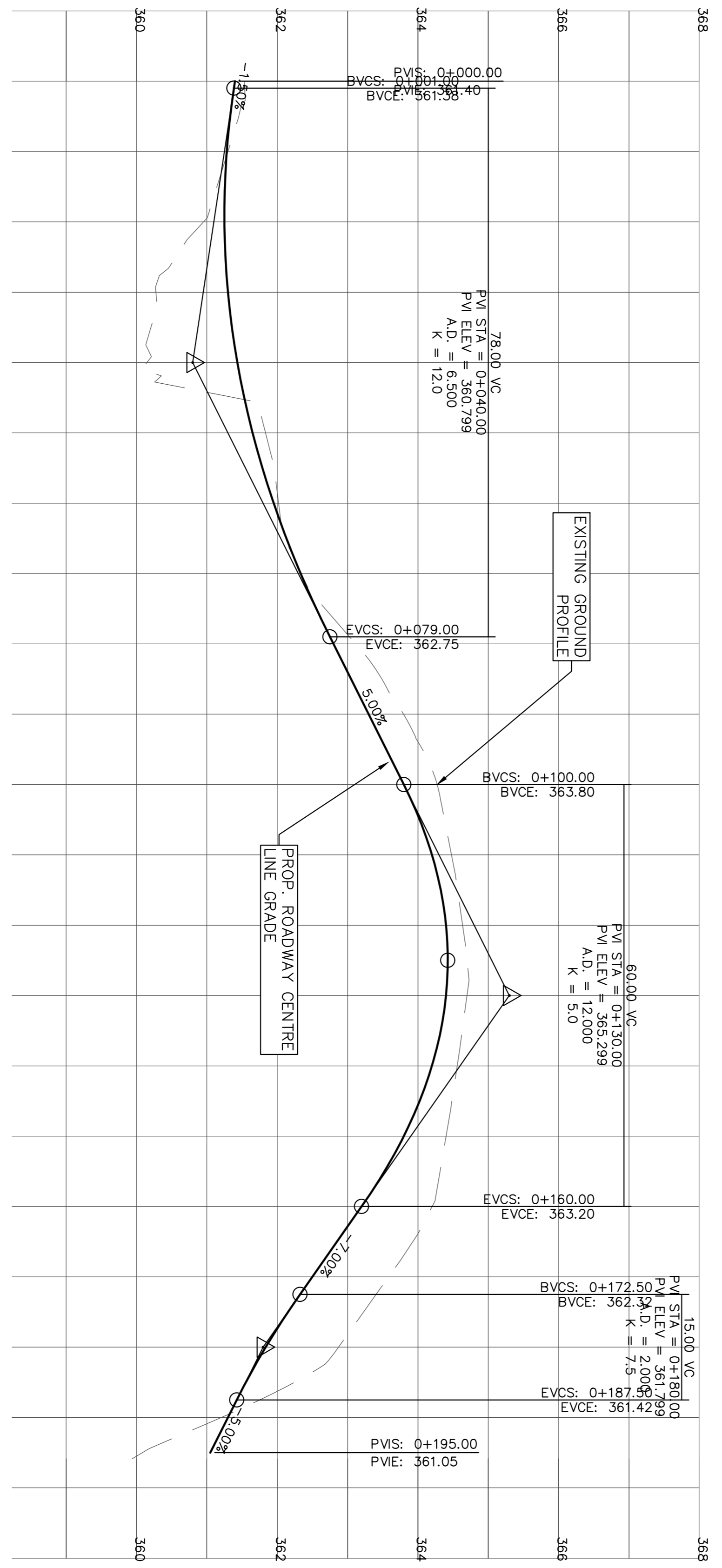
- CITY ROADWORK NOTES**
1. THE DEVELOPER SHALL EMPLOY A PROFESSIONAL ENGINEER WITH EXPERIENCE IN GEOTECHNICAL ENGINEERING FOR PERFORMANCE OF IN PLACE TESTING DURING THE PREPARATION OF THE SUBGRADE AND CONSTRUCTION OF THE ROAD STRUCTURE TO VERIFY THE ADEQUACY OF THE PROPOSED AND EXISTING ROAD STRUCTURE AND SUBGRADE. SELECTION OF THE PROFESSIONAL ENGINEER IS TO BE APPROVED BY THE CITY.
 2. EXISTING VALVE BOXES, MANHOLES, ETC., WITHIN THE ROAD ALLOWANCE MUST BE ADJUSTED TO SUIT THE PROPOSED FINISHED GRADE.
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APLUN & MARTIN CONSULTANTS LTD.

ROADWORKS NOTES:

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4. ROAD CATCH BASINS TO BE SET TO ASPHALT BASE COURSE ELEVATION.
5. FINAL 35mm ASPHALT OVERLAY TO BE PLACED PRIOR TO END OF ONE YEAR MAINTENANCE PERIOD.

CENTRELINE ROAD ELEVATIONS	EXISTING	PROPOSED
CHAINAGE		
0+000	361.53	361.40
0+020	360.95	361.25
0+040	360.15	361.43
0+060	362.02	361.95
0+080	363.11	362.80
0+100	364.27	363.80
0+120	364.63	364.40
0+140	364.56	364.20
0+160	364.20	363.20
0+180	362.87	361.84
0+200		
0+210		



LEGAL DESCRIPTION: LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.Y.D.

REV. NO.	DESCRIPTION	DR. CH.	DATE	APP.
0	ISSUED FOR REVIEW	TIG MJT	13/01/02	JBK
1	ISSUED FOR DISCUSSION	TIG MJT	13/03/05	JBK
2	ISSUED FOR REZONING	TIG MJT	13/03/15	JBK

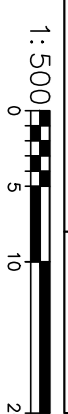
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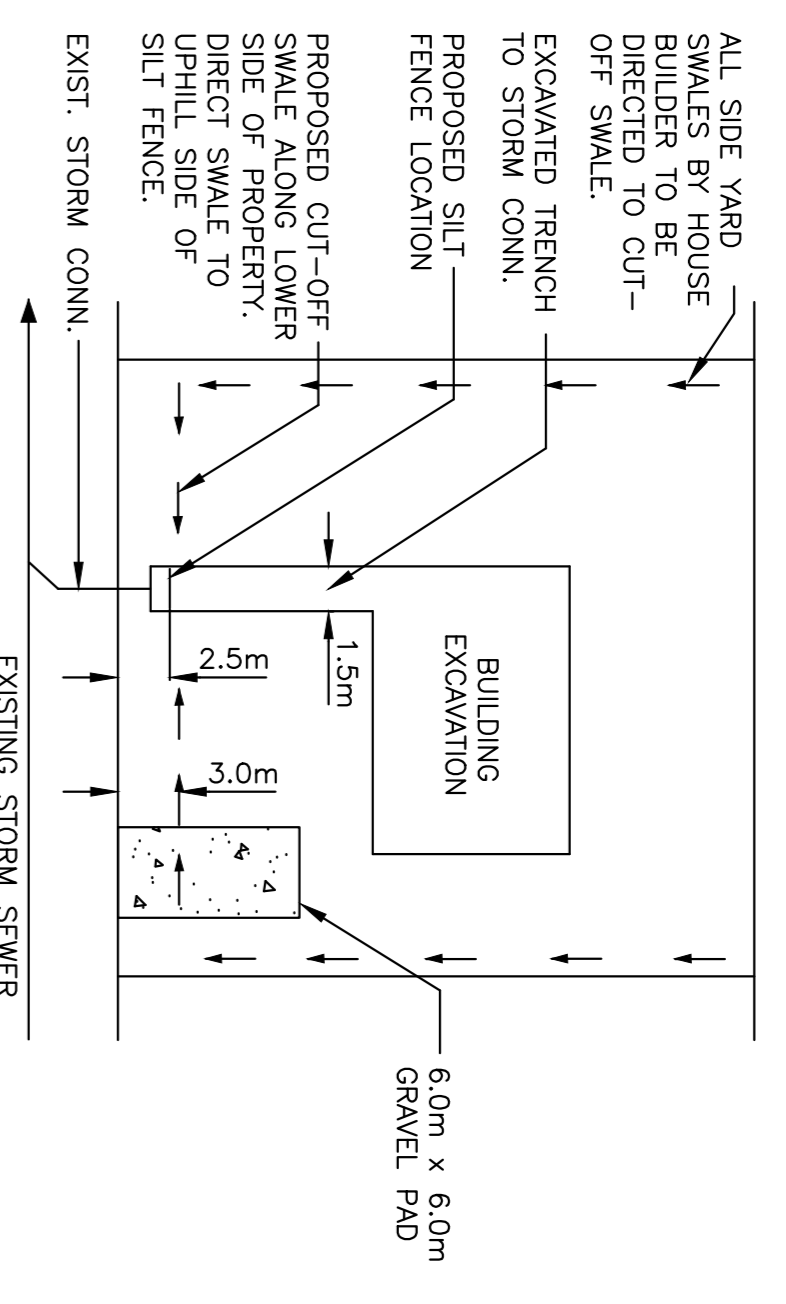
CLIENT: **NEW TOWN PLANNING SERVICES LTD.**
 1450 PANDOSY ST., KELOWNA BC V1Y 1P3
 PH. (250) 860-8185

PROJECT: **RESIDENTIAL SUBDIVISION**
 984 DEHART RD., KELOWNA BC
 LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.Y.D.

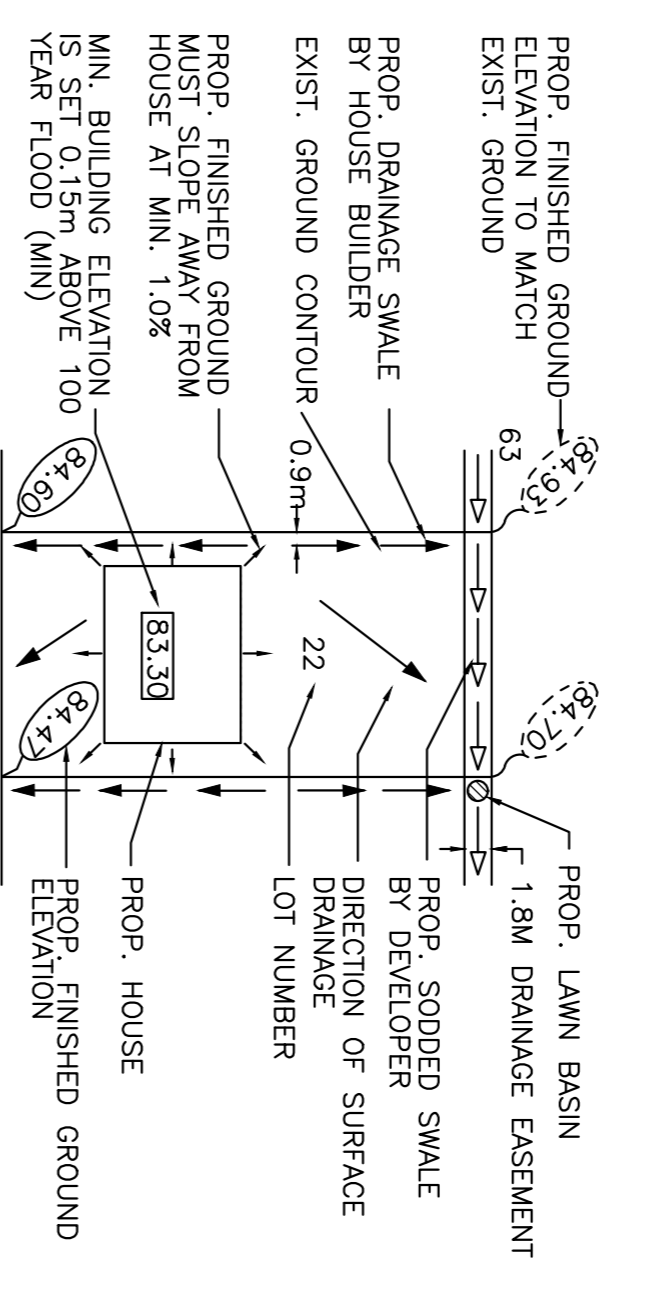
The location of existing underground utilities are shown here for information only. They are not to be independently verified by the owner or its representative. The location of all existing utilities shown on this drawing is for information only and does not constitute any and all damages which might be incurred by the owner or its representative. The location of all existing utilities shown on this drawing is for information only and does not constitute any and all damages which might be incurred by the owner or its representative.

TITLE:	ROADWORKS - ROAD B	
PROJECT NO.	12-651	
DRAWING NO.	12-651 - 07	
SCALE:	HORIZ: 1:500 VERT: 1:50	
DRAWING DATE:	MARCH 2013	
SHEET NO.	7 OF 9	
REV:	2	
DESIGN: MJT	CHECK: MJT	
DRAWN: TIG	APPR: JBK	

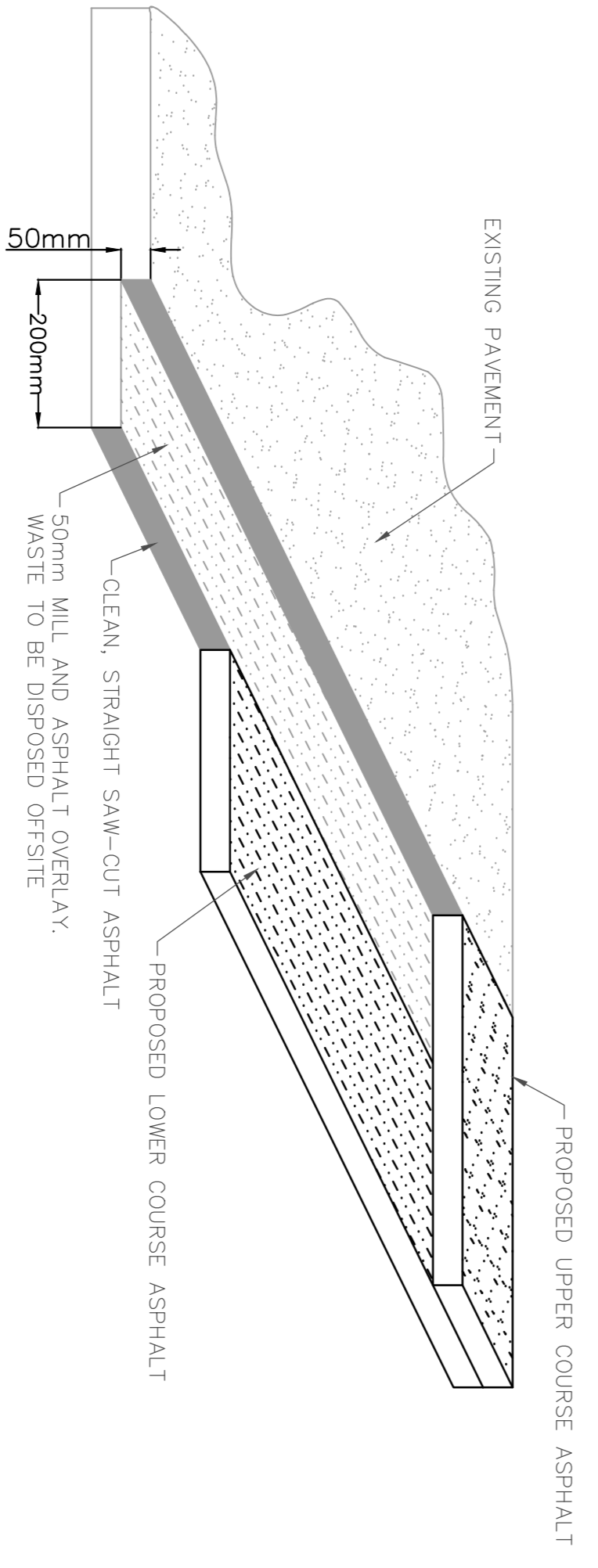




INDIVIDUAL LOT SILT CONTROL (BY HOUSE BUILDER)
N.T.S.



TYPICAL LOT DETAIL
N.T.S.

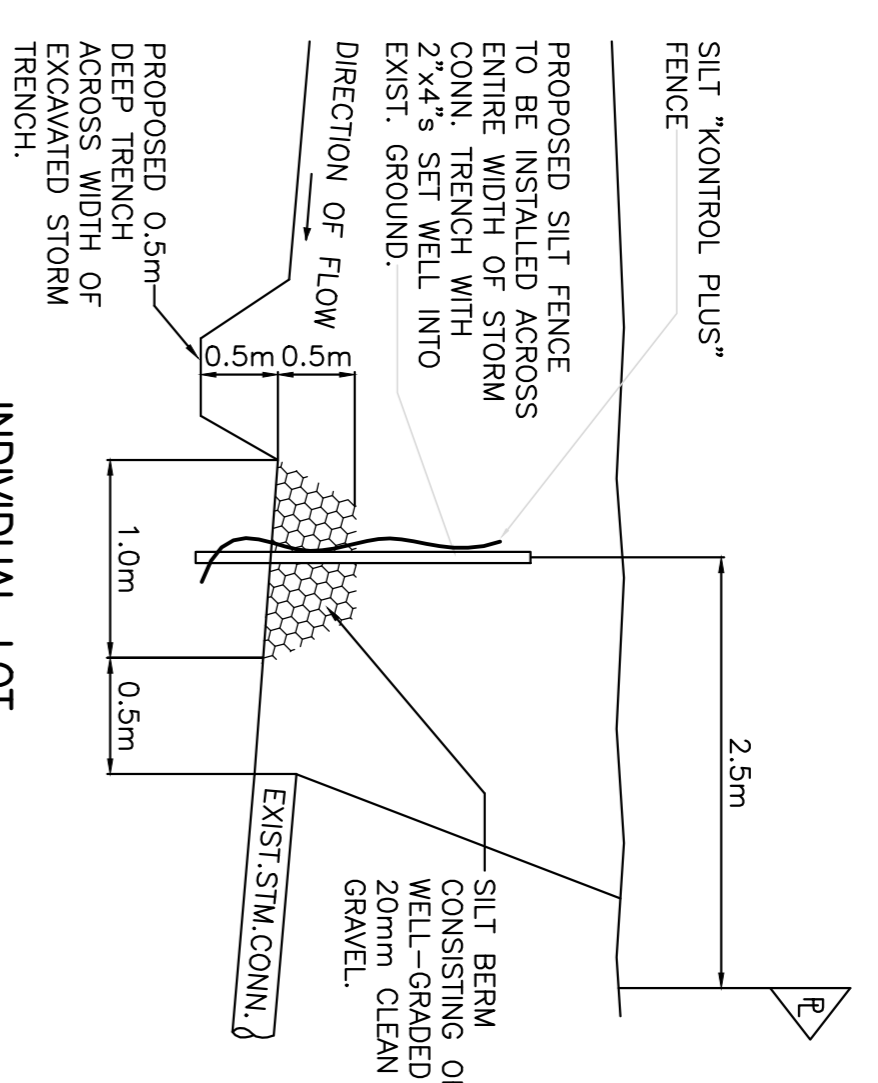


TIE-IN OF NEW PAVEMENT TO EXISTING PAVEMENT
N.T.S.

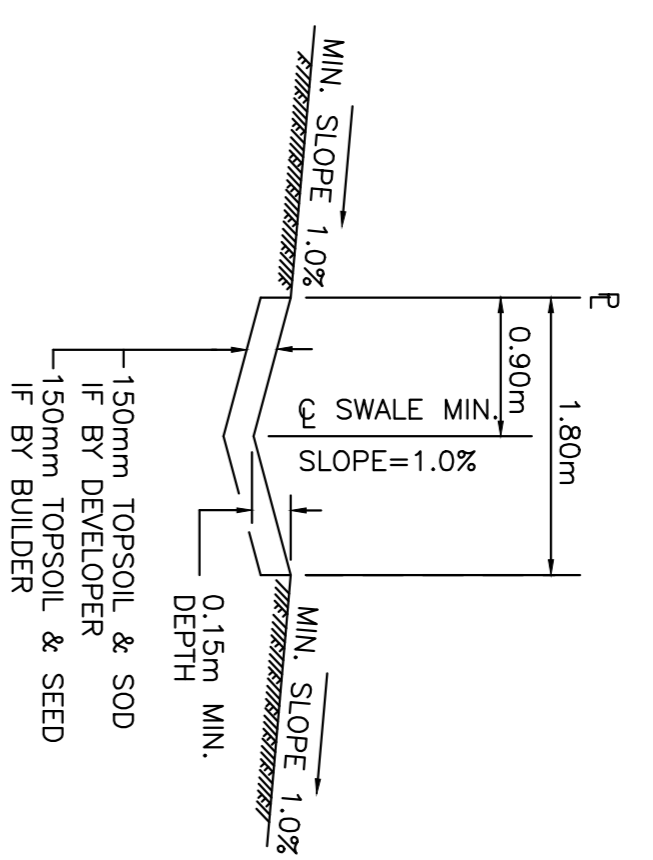
INDIVIDUAL LOT SILT CONTROL NOTES:

1. IT IS THE COMPLETE RESPONSIBILITY OF THE DEVELOPER TO ENSURE THAT HOUSE BUILDERS DIRECT ALL LOT DRAINAGE TO A TEMPORARY SILT CONTROL FENCE/BERM.
2. SILT CONTROL AND CUT-OFF SWALE SHALL REMAIN IN PLACE UNTIL FINISHED LOT GRADING. ALSO, FOUNDATION TILE SHALL NOT BE CONNECTED TO STM. CONNECTION UNTIL ALL ON-SITE TILE IS PLACED AND BACKFILLED. IN THE INTERIM, TILE TRENCH TO STM CONNECTION SHALL BE TEMPORARILY BLOCKED BY A SILT FENCE/BERM.
3. FILTER FABRIC FOR CONSTRUCTION OF SILT FENCE TO BE NICOLON "CONTROL PLUS" OR EQUIVALENT.
4. EACH HOUSE BUILDER SHALL INSTALL THE SILT FENCE/BERM AND CUT-OFF SWALE PRIOR TO UNCAPPING THE STORM CONNECTION AND EXCAVATING THE BUILDING FOOTPRINTS.
5. HOUSE BUILDER SHALL ENSURE THAT NO SILT-LADEN WATER SHALL BE PUMPED OUT OF OR DISCHARGED INTO THE STORM SEWER SYSTEM. EACH HOUSE BUILDER MAY BE REQUIRED TO INSTALL ADDITIONAL FACILITIES, AS NECESSARY, TO CONTROL THE DISCHARGE OF SILT-LADEN WATERS.
6. HOUSE BUILDER TO CONSTRUCT 6.0m x 6.0m GRAVEL SITE ACCESS PAD. PAD SHALL BE CONSTRUCTED OF 40mm CRUSHED GRAVEL AND BE 200mm.
7. ROAD MAINTENANCE THICK GRAVEL PAD TO BE USED FOR ALL DELIVERIES TO SITE.

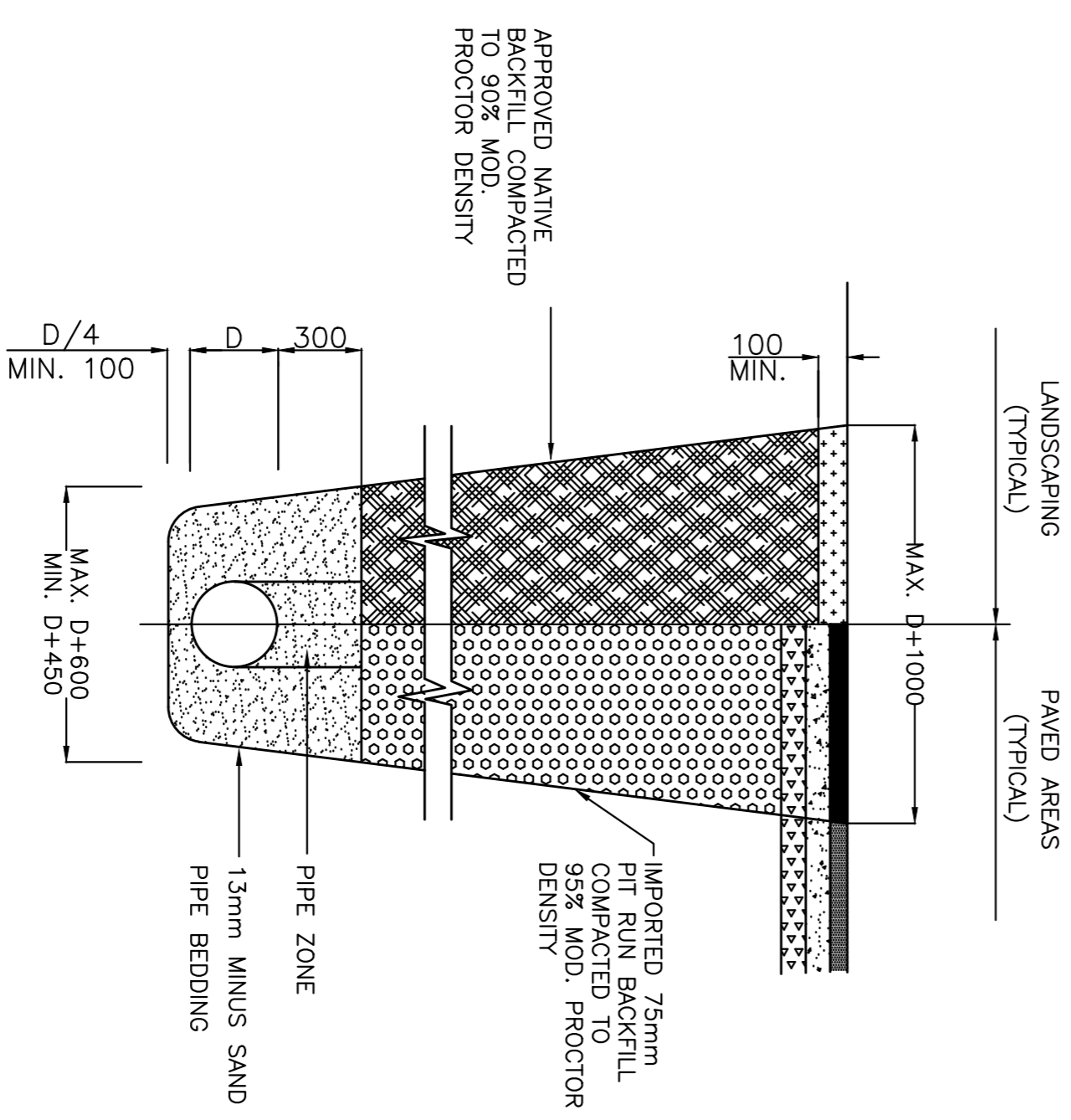
NO SOIL, SAND OR OTHER MATERIAL WITH A HIGH SEDIMENT CONTENT SHALL BE DEPOSITED OR PILED OUTSIDE THE PROPERTY BOUNDARIES, PARTICULARLY ON THE PAVED ROAD SURFACE. NO MATERIAL OF ANY TYPE IS TO BE DEPOSITED IN SUCH A WAY THAT IT INTERFERES WITH THE FLOW OF WATER ALONG THE CURBS OR INTO CATCH BASINS.



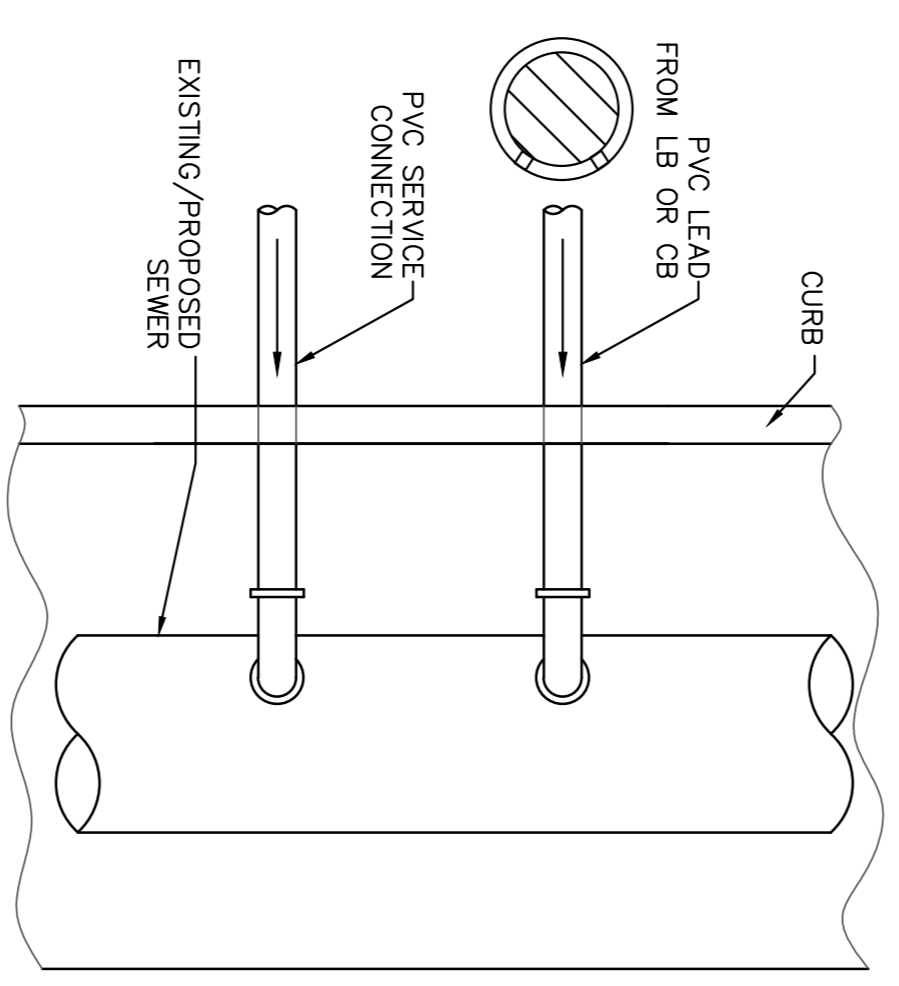
INDIVIDUAL LOT SILT FENCE/BERM DETAIL
N.T.S.



TYPICAL SECTION OF SWALE
N.T.S.



UTILITY TRENCH DETAIL
N.T.S.



SECTION

CONNECTIONS TO SANITARY AND STORM SEWER
N.T.S.

LEGAL DESCRIPTION: LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.Y.D.	
B.M. MONUMENT NO. OCM73H1715	ELEVATION: 552.764
LOCATED AT HAZELL ROAD, 20m SOUTH OF THE TORRS ROAD INTERSECTION	
REV. NO.	DESCRIPTION
0	ISSUED FOR REVIEW
1	ISSUED FOR DISCUSSION
2	ISSUED FOR RENOVING

DR. CH.	DATE	APP.
TIG MJT	13/01/02	JBK
TIG MJT	13/03/05	JBK
TIG MJT	13/03/15	JBK

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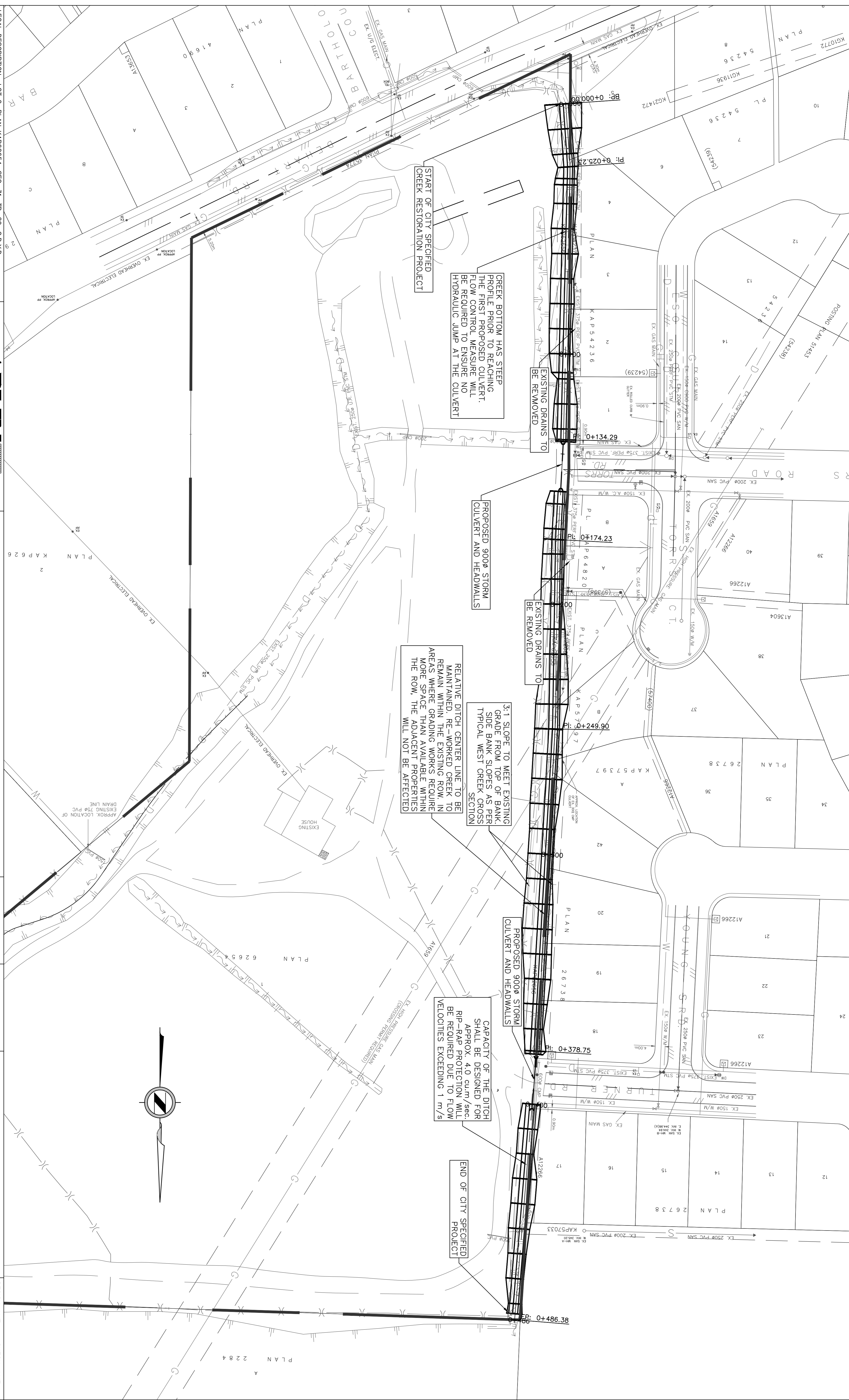
CLIENT: NEW TOWN PLANNING SERVICES LTD.
1460 PANDOSY ST., KELOWNA BC V1Y 1P3
PH. (250) 860-9185

PROJECT: RESIDENTIAL SUBDIVISION
984 DEHART RD., KELOWNA BC
LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.Y.D.

TITLE: STANDARD DETAILS

PROJECT NO. _____
SCALE: HORIZ. _____
VERT. _____
DRAWING NO. _____
A & M DRAWING NO. **12-651 - 09**

DESIGN: MJT
DRAWN: TIG
CHECK: MJT
APP: JBK
DRAWING DATE: **MARCH 2013**
SHEET NO. **9 OF 9**
REV. **2**



LEGAL DESCRIPTION: LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.Y.D.

REV. NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR REVIEW	13/01/02	JBK

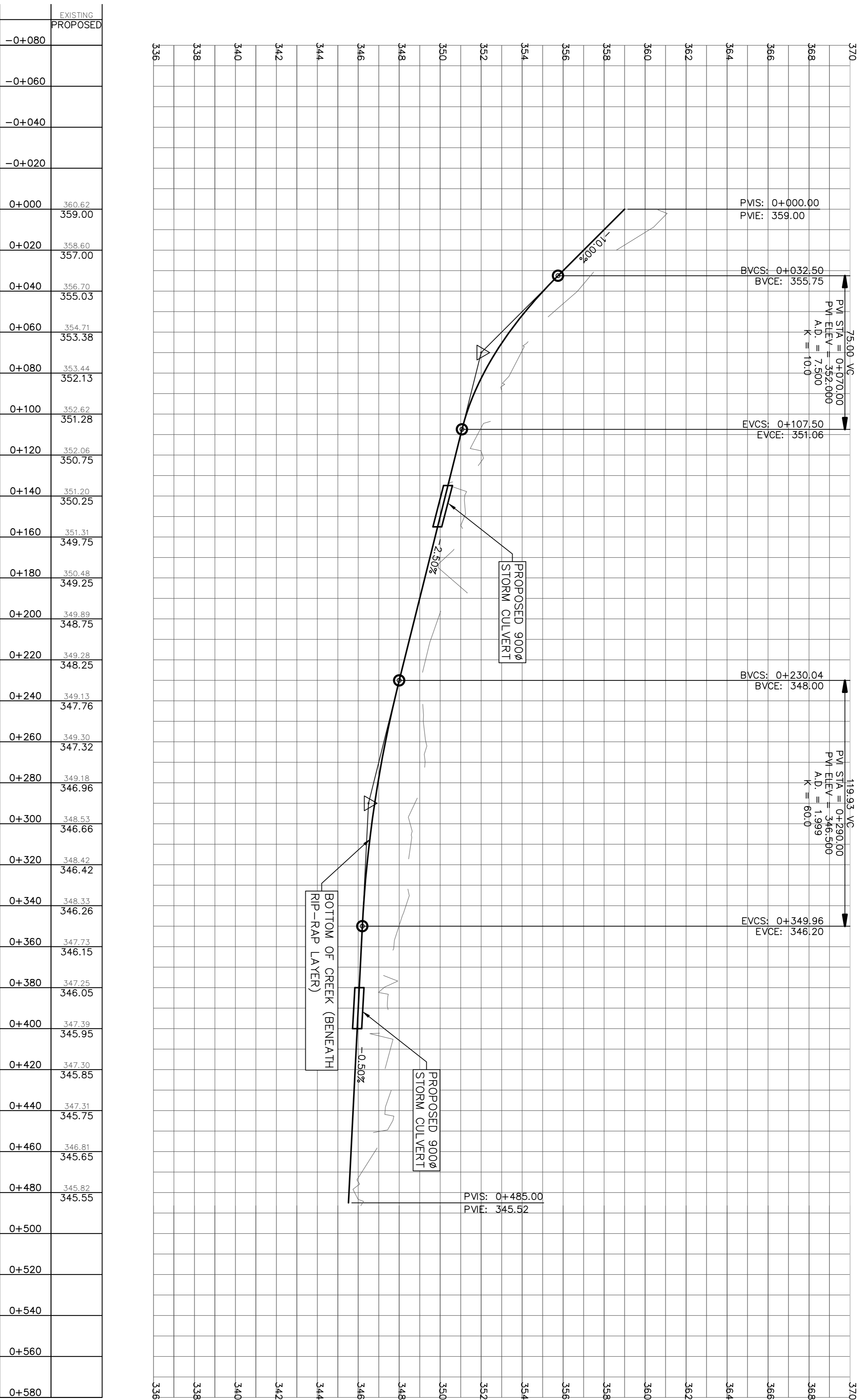
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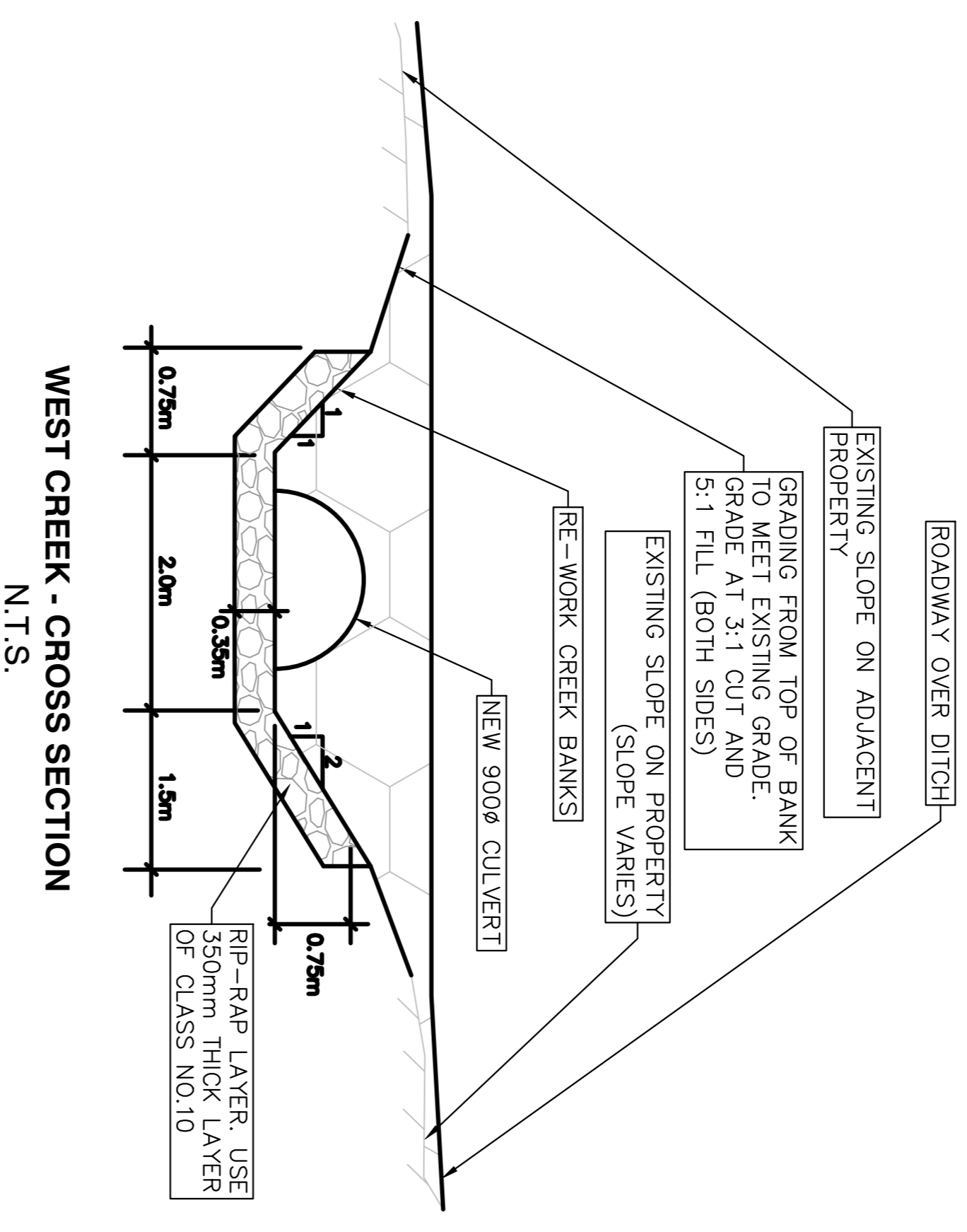
PROJECT: **RESIDENTIAL SUBDIVISION**
 984 DEHART RD., KELOWNA BC
 LOT 2, PLAN KAP62654, SEC. 31, TP. 29, O.D.Y.D.

The location of existing underground utilities are shown for information only. They are not to be independently verified by the owner or its representative. The location of all existing utilities shall be confirmed by the owner before commencing work, and for any and all damages which might be caused by the location of these utilities, the owner shall be liable to satisfy local and provincial laws and regulations.

TITLE:	KEY PLAN - WEST CREEK UPGRADE		
PROJECT NO.	SCALE:	DESIGN: MJT	CHECK: MJT
DRAWING NO.	HORIZ: A & M DRAWING NO.	DRAWN: TIG	APPR: JBK
	VERT: 12-651 - 01		
		DESIGN DATE:	
		DECEMBER 2012	
		SHEET NO.	REV
		01 OF 02	0



WEST CREEK - PROFILE



WEST CREEK - CROSS SECTION
N.T.S.

LEGAL DESCRIPTION: LOT 2, PLAN KAP62854, SEC. 31, TP. 29, O.D.Y.D.

B.M. MONUMENT NO. OCM7341715 ELEVATION: 352.764
LOCATED AT HAZELL ROAD, 70m SOUTH OF THE TORRS ROAD INTERSECTION

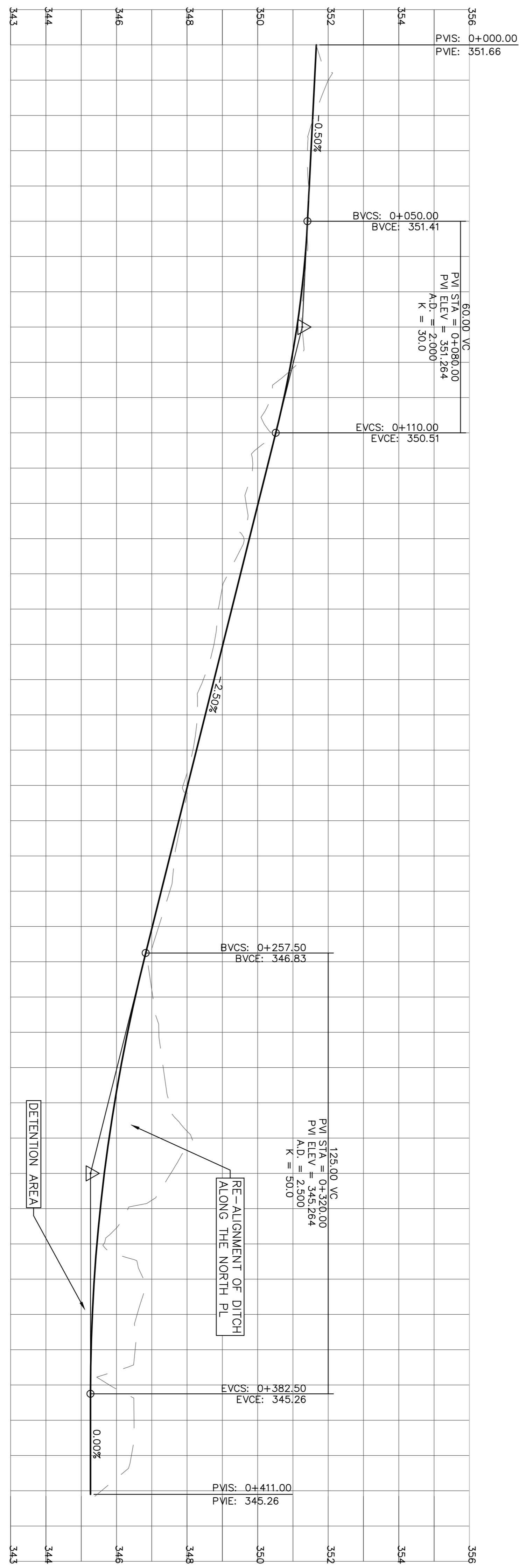
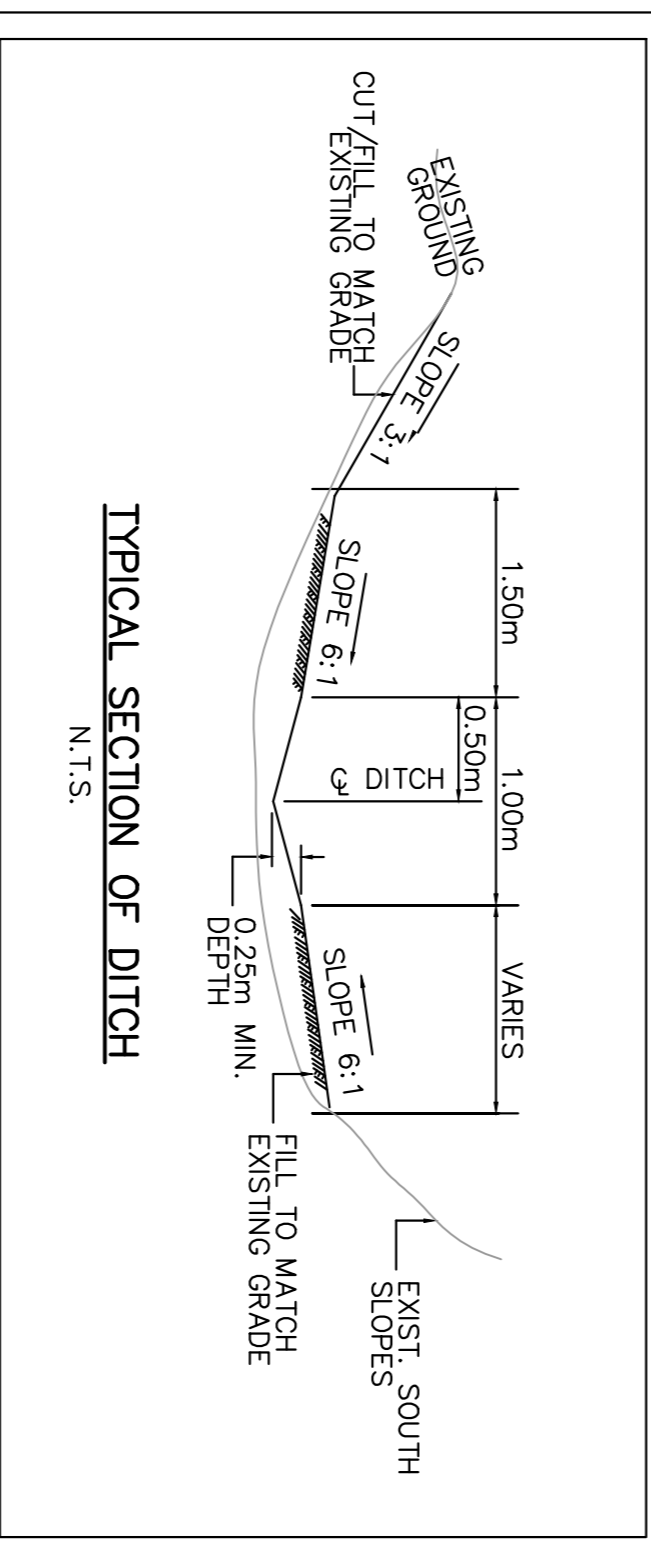
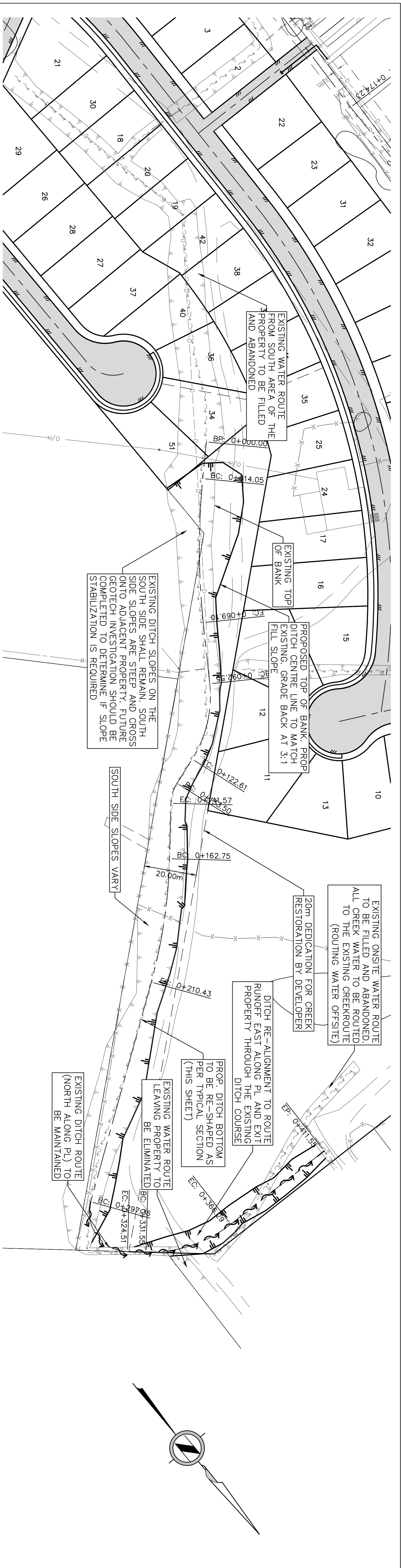
REV. NO.	DESCRIPTION	DR. CH.	DATE	APP.
0	ISSUED FOR REVIEW	TIG	13/01/02	JBK

APLUN MARTIN CONSULTANTS LTD.
PROJECT MANAGEMENT & ENGINEERING & PLANNING & SURVEYING
454 Leon Avenue, Kelowna, B.C., Canada V1Y 6J3
Tel: (250) 448-0157, Fax: (250) 458-2312, E-mail: general@aplunmartin.com

CLIENT:	NEW TOWN PLANNING SERVICES LTD. 1450 PANDOSY ST., KELOWNA BC V1Y 1P3 PH. (250) 860-9185
PROJECT:	RESIDENTIAL SUBDIVISION 984 DEHART RD., KELOWNA BC LOT 2, PLAN KAP62854, SEC. 31, TP. 29, O.D.Y.D.

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TITLE:	PROFILE AND SECTION		
PROJECT NO.	SCALE:	DESIGN: MJT	CHECK: MJT
DRAWING NO.	HORIZ. VERT.	DRAWN: TIG	APPR: JBK
	A & M DRAWING NO.	A & M FILE:	
	12-651 - 02	12-651	
		DRAWING DATE:	
		DECEMBER 2012	
		SHEET NO.	REV.
		02 OF 02	0



CHAINAGE	CENTRELINE ROAD ELEVATIONS	PROPOSED	EXISTING
0+010	351.66	351.66	
0+020	351.61	351.56	
0+040	351.44	351.46	
0+060	351.41	351.35	
0+080	351.26	351.11	
0+100	350.37	350.75	
0+120	349.86	350.26	
0+140	349.61	349.76	
0+160	348.88	349.26	
0+180	348.46	348.76	
0+200	348.17	348.26	
0+220	347.84	347.76	
0+240	347.51	347.26	
0+260	346.89	346.76	
0+280	347.20	346.31	
0+300	347.49	345.94	
0+320	347.54	345.65	
0+340	345.64	345.44	
0+360	346.58	345.31	
0+380	345.80	345.26	
0+400	346.42	345.26	
0+420			
0+430			

LEGAL DESCRIPTION: LOT 2, PLAN KAP62854, SEC. 31, TP. 29, O.D.Y.D.

BM. MONUMENT NO. OCM7341715 ELEVATION: 352.764
 LOCATED AT HAZELL ROAD, 20m SOUTH OF THE TORRS ROAD INTERSECTION

REV. NO.	DESCRIPTION	DR. CH.	DATE	APP.
0	ISSUED FOR REVIEW	TIG MJT	13/01/02	JBK
1	ISSUED FOR DISCUSSION	TIG MJT	13/03/05	JBK
2	ISSUED FOR REZONING	TIG MJT	13/03/15	JBK

APLUN MARTIN CONSULTANTS LTD
 PROJECT MANAGEMENT • ENGINEERING • PLANNING • SURVEYING
 454 Leon Avenue, Kelowna, B.C. Canada V1Y 6J3
 Tel: (250) 448-0157, Fax: (250) 458-2312, E-mail: general@aplunmartin.com

CLIENT: **NEW TOWN PLANNING SERVICES LTD.**
 1480 PANDOSY ST., KELOWNA BC V1Y 1P3
 PH: (250) 860-8188

PROJECT: **RESIDENTIAL SUBDIVISION**
 984 DEHART RD., KELOWNA BC
 LOT 2, PLAN KAP62854, SEC. 31, TP. 29, O.D.Y.D.

TITLE: **CREEK RESTORATION PLAN**

PROJECT NO. _____ SCALE: HORIZ: 1:750
 DRAWING NO. _____ A & M DRAWING NO. **12-651 - 08**

DESIGN: MJT CHECK: MJT
 DRAWN: TIG DATE: **MARCH 2013**
 SHEET NO. **8 OF 9** REV. **2**

The location of existing underground utilities are shown here for information only and are not to be independently verified by the owner or its representative. The location of all existing utilities exist before commencing work, and for any and all damages which might be incurred by the owner or its representative, the contractor shall be liable to notify the owner of any and all underground utilities.

CITY OF KELOWNA
MEMORANDUM

Date: May 15, 2013
File No.: Z13-0016

To: Land Use Management (GS)

From: Development Engineering Manager

Subject: 984 Dehart Road A1 to RU1

The Development Services Department has the following comments and requirements associated with this rezoning for the proposed residential development. The following Works & Services are required for this application:

.1) General

- a) This proposed subdivision may require the installation of centralized mail delivery equipment. Please contact Rick Ould, Delivery Planning Officer, Canada Post Corporation, 530 Gaston Avenue, Kelowna, BC V1Y 2K0 to obtain further information and to determine suitable location(s) within the development.
- b) Provide a Pre-design report to address all offsite Utility and Transportation servicing including costs.
- c) **PRIOR TO FINAL ADOPTION OF THE ZONE AMENDING BYLAW;** a Predesign report specific to this development is required to identify all offsite servicing needs in accordance with the Subdivision, Development and Servicing Bylaw. A Servicing Agreement for all offsite works required by this subdivision must be provided, along with Performance Security, before final adoption of the Zone Amending Bylaw.
- d) Where there is a possibility of a high water table or surcharging of storm drains during major storm events, non-basement homes may be required. This must be determined by the engineer and detailed on the Lot Grading Plan required in the drainage section.
- e) The Drainage Division noted that tributary stream crosses the lands. The stream may affect the layout.

.2) Geotechnical Report

- a) Provide a comprehensive geotechnical report prepared by a Professional Engineer competent in the field of hydro-geotechnical engineering to address the items below: **NOTE: The City is relying on the Geotechnical Engineer's report to prevent any damage to property and/or injury to persons from occurring as a result of problems with soil slippage or soil instability related to this proposed subdivision.**

The Geotechnical reports must be submitted to the Development Services Department prior to submission of engineering drawings.

- (i) Area ground water characteristics, including any springs and overland surface drainage courses traversing the property. Identify any monitoring required.
- (ii) Site suitability for development.
- (iii) Site soil characteristics (i.e. fill areas, sulphate content, unsuitable soils such as organic material, etc.).
- (iv) Any special requirements for construction of roads, utilities and building structures.
- (v) Suitability of on-site disposal of storm water and sanitary waste, including effects upon adjoining lands.
- (vi) Slope stability, rock fall hazard and slippage including the effects of drainage and septic tank effluent on the site.
- (vii) Identify slopes greater than 30%.
- ii) Top of bank assessment and location including recommendations for property line locations, septic field locations, building setbacks, and ground water disposal locations.
- iii) Recommendations for items that should be included in a Restrictive Covenant.
- iv) Any special requirements that the proposed subdivision should undertake so that it will not impact the bank(s). The report must consider erosion and structural requirements.
- v) Any items required in other sections of this document.
- vi) Recommendations for erosion and sedimentation controls for water and wind.
- vii) Recommendations for roof drains and perimeter drains.

.3) Water

- a) The property is located within the South East Kelowna Irrigation District (SEKID) service area.
- b) Provide an adequately sized domestic water and fire protection system complete with individual lot connections. The water system must be capable of supplying domestic and fire flow demands of the project in

accordance with the Subdivision, Development & Servicing Bylaw. Provide water calculations for this subdivision to confirm this. Ensure every building site is located at an elevation that ensures water pressure is within the bylaw pressure limits. Note: Private pumps are not acceptable for addressing marginal pressure.

.4) Sanitary Sewer

- a) Provide an adequately sized sanitary sewer system complete with individual lot connections.
- b) Our records indicate a 200mm main exists at the end of Torrs Road adjacent to the site.

.5) Drainage

- a) Provide an adequately sized drainage system complete with individual lot connections. The Subdivision, Development and Servicing Bylaw require that each lot be provided with an individual connection; however, the City Engineer may permit use of individual ground water disposal systems, where soils are suitable. For on-site disposal of drainage water, a hydrogeotechnical report will be required complete with a design for the disposal method (i.e. trench drain / rock pit). The Lot Grading Plan must show the design and location of these systems for each lot.
- b) Provide the following drawings:
 - i) A detailed Lot Grading Plan (indicate on the Lot Grading Plan any slopes that are steeper than 30% and areas that have greater than 1.0 m of fill);
 - ii) A detailed Stormwater Management Plan for this subdivision; and,
 - iii) An Erosion and Sediment Control Plan.
- c) Show details of dedications, rights-of-way, setbacks and non-disturbance areas on the lot Grading Plan.
- d) Significant banks are located on the subject property. The slopes appear to be sensitive to erosion; temporary and permanent measures must be taken to prevent erosion and sediment transport.
- e) Identify clearly on a contour map, or lot grading plan, the top of bank(s). Provide cross sections along the top of the bank at each property corner and at locations where there are significant changes in slope. Cross sections are to be perpendicular to the contour of the slope. Show the proposed property lines on the cross sections. Not all areas have a clear top of bank; and therefore, field reconnaissance by City staff and the applicant may be needed to verify a suitable location for property lines.

.6) Roads

- (a) Dehart Road is designated an arterial road. Dehart Rd must be upgraded to an urban standard along the full frontage of this proposed development, including curb and gutter, separate sidewalk, drainage system including catch basins, manholes and pavement removal and replacement, street lighting and re-location or adjustment of utility appurtenances if required to accommodate the upgrading construction.

The estimated cost for this construction for bonding purposes is **\$128,000.00**. Not included in the estimate are costs for pole relocations.

- b) Provide an additional highway allowance widening of ~3.20m (west end) tapering to ~6.22m (east end) on Dehart Road. This widening is to be accomplished by:
 - i) A dedication on the subdivision plan.
- c) The existing bus stop on Dehart Road will require upgrading to current standard (UBS2).
- d) Landscaped boulevards, complete with underground irrigation, may be required on Dehart Road.
- e) Driveway access is not permitted onto Dehart Road. A restrictive covenant in favour of the City of Kelowna, registered under Section 219 of the Land Title Act, must be granted to the effect that vehicular access is not permitted from abutting lots. The subdivision plan must be endorsed to the effect that a covenant is to be registered. The covenant must be registered as a priority charge and is to be indicated on the Lot Grading Plan.
- f) Roadway access to Dehart Road must be reviewed via a Traffic Impact Analysis (TIA). The City's Transportation & Mobility group will determine the terms of reference for the study. Recommendations from the Traffic Impact Analysis (TIA) will become requirements of rezoning.
- g) Verify that physical driveway access will satisfy City requirements for all lots. For steeper lots (15% and greater), show driveways on the lot grading plan with grades or profiles. Where lots are serviced by onsite sewage disposal systems, show limits of cut and fill lines.

.7) Power and Telecommunication Services and Street Lights

- a) All proposed distribution and service connections for the subdivision are to be installed underground. Existing distribution and service connections, on that portion of a road immediately adjacent to the site, are to be relocated and installed underground
- b) Street lights must be installed on all roads.
- c) Before making application for approval of your subdivision plan, please make arrangements with Fortis BC for the pre-payment of applicable charges and tender a copy of their receipt with the subdivision application.
- d) Make servicing applications to the respective Power and Telecommunication utility companies. The utility companies are required to obtain the City's approval before commencing construction.

.8) Design and Construction

- a) Design, construction supervision and inspection of all off-site civil works and site servicing must be performed by a Consulting Civil Engineer and all such work is subject to the approval of the City Engineer. Drawings must conform to City standards and requirements.

- b) Engineering drawing submissions are to be in accordance with the City's "Engineering Drawing Submission Requirements" Policy. Please note the number of sets and drawings required for submissions.
- c) Quality Control and Assurance Plans must be provided in accordance with the Subdivision, Development & Servicing Bylaw No. 7900 (refer to Part 5 and Schedule 3).
- d) A "Consulting Engineering Confirmation Letter" (City document 'C') must be completed prior to submission of any designs.
- e) Before any construction related to the requirements of this subdivision application commences, design drawings prepared by a professional engineer must be submitted to the City's Works & Utilities Department. The design drawings must first be "Issued for Construction" by the City Engineer. On examination of design drawings, it may be determined that rights-of-way are required for current or future needs.

.9) Servicing Agreements for Works and Services

- a) A Servicing Agreement is required for all works and services on City lands in accordance with the Subdivision, Development & Servicing Bylaw No. 7900. The applicant's Engineer, prior to preparation of Servicing Agreements, must provide adequate drawings and estimates for the required works. The Servicing Agreement must be in the form as described in Schedule 2 of the bylaw.
- b) Part 3, "Security for Works and Services", of the Bylaw, describes the Bonding and Insurance requirements of the Owner. The liability limit is not to be less than \$5,000,000 and the City is to be named on the insurance policy as an additional insured.

.10) Other Engineering Comments

- a) Provide all necessary Statutory Rights-of-Way for any utility corridors required, including those on proposed or existing City Lands.
- b) If any road dedication affects lands encumbered by a Utility right-of-way (such as Terasen, etc.) please obtain the approval of the utility prior to application for final subdivision approval. Any works required by the utility as a consequence of the road dedication must be incorporated in the construction drawings submitted to the City's Development Manager.

Steve Muenz, P.Eng.
Development Engineering Manager

SS

1555-C

December 18, 2012

South East Kelowna Irrigation District

Attn.: Mr. Toby Pike

3235 Gulley Road

Kelowna, BC

V1W 4E5

Dear Mr. Pike

Subject: SEKID Boundary Adjustment to Exclude Lot 1, Plan 62654

This letter is to confirm that I will cover the administrative costs incurred by SEKID for the advancement of this proposed boundary adjustment to exclude 984 Dehart Road. Kindly advance any invoices to:

Dr. A. Rezansoff
25088 PO Mission
Kelowna BC
V1W 3Y7

Thank you for your assistance with this application for service boundary adjustment.

Sincerely,



Dr. A. Rezansoff

cc. New Town Planning Services Inc.

SETTLEMENT AND RELEASE AGREEMENT

This Agreement dated for reference this ____ day of _____, 2010.

BETWEEN

CITY OF KELOWNA
1435 Water Street
Kelowna, B.C. V1Y 1J4

(the "City")

AND

ALEXANDER REZANSOFF
PO Box 25088 RPO Mission Park
Kelowna, BC V1W 3Y7

("the Owner")

WHEREAS:

- A. The Owner is the registered owner of a 50% interest in title to lands within the boundaries of the City of Kelowna on which it wishes to undertake residential development, with a civic address of 984 Dehart Road and a legal description of Section 31, Plans KAP 62654 , Lot 1 (the "Lands")
- B. The Owner has been farming the Lands since 1987. Between June 22, 2009 and July 9, 2009 the City has alleged that the Owner performed work on the lands including screening of materials, infilling of a tributary to Thompson Creek and other works not permitted to occur without having confirmed that a development permit had been obtained, as required by Section 920 of the *Local Government Act* which allegations have been denied by the Owner (the "Bylaw Dispute")
- C. The parties have agreed to resolve the Bylaw Dispute upon the terms and conditions as set forth in this Agreement.

Therefore, in and for the consideration of the mutual agreements set out below, the parties covenant and agree as follows:

Undertakings of the Owner

- 1) The Owner hereby undertakes to:
 - a) ensure that there is no net loss of riparian habitat and function for the Lands;
 - b) to remove all of the drain piping located on the Lands as identified on the photograph of the Lands attached hereto as Schedule "A";
 - c) prepare a mitigation/restoration plan (signed off by an QEP/RPBio) for the site;
 - d) ensure that all future works on the site will adhere to applicable provincial acts, including but not limited to: the Water Act; and the Fish Protection Act.
 - e) ensure that all future works on the site will adhere to applicable City of Kelowna policies, plans and policies including but not limited: Kelowna 2020 Official Community Plan (7600), Zoning Bylaw (8000) and Subdivision and Servicing Bylaw (7900).

City Covenant Not to Enforce

2. In consideration of the undertakings of the Owner , the City covenants, represents and warrants to the owners, its principals, shareholders, partners, subcontractors, successors and assigns that it will not commence an action prosecution, inquiry, suit, arbitration, or other proceeding against the owners in connection with the Bylaw Dispute and shall stay all existing proceedings against the Owner in connection with the By Law Dispute.

Condition Precedent of Council Approval

3. This settlement agreement shall not be binding and effective unless and until the Council of the City has given its approval by resolution.

General

4. The Owner and the City acknowledge and agree that the facts and law in respect of which this settlement agreement is made may prove to be other than, or different from, the facts and law in that connection now known or believed by the City or the Owner to be true, and the City and the Owner specifically accepts and assumes this risk and agree that this settlement agreement is not affected by any such difference and remains effective and binding unaffected by any such difference.
5. This settlement agreement shall be binding upon and for the benefit of the parties and their respective heirs, successors, and assigns.
6. The terms of this settlement agreement are contractual and not merely recitals, and this settlement agreement is executed for the purpose of making a full and final irrevocable settlement.
7. There are no representations, collateral agreements or conditions with respect to this settlement agreement except as contained herein. The Owner acknowledges and agree that he has obtained independent legal counsel or has had the opportunity to obtain legal advice regarding this settlement agreement and that he has executed this agreement upon the advice of his lawyer or expressly waiving any right to consult a lawyer and that it has fully read and understood its contents.

8. This settlement agreement shall be governed and construed in accordance with the laws of the Province of British Columbia.

As evidence of their agreement to be bound by the above terms, as both a contract and a deed executed and delivered under seal, the parties have executed this settlement agreement on the date set out below.

Alexander A. Rezansoff

The Corporate Seal of the CITY OF KELOWNA was hereto affixed in the presence of:)
)
)
)
)
)
)
)
)
)
)

Mayor **SHARON SHEPHERD, MAYOR**

C/S

Clerk: Stephen Fleming,
City Clerk

Date executed: August 16, 2010

SCHEDULE A

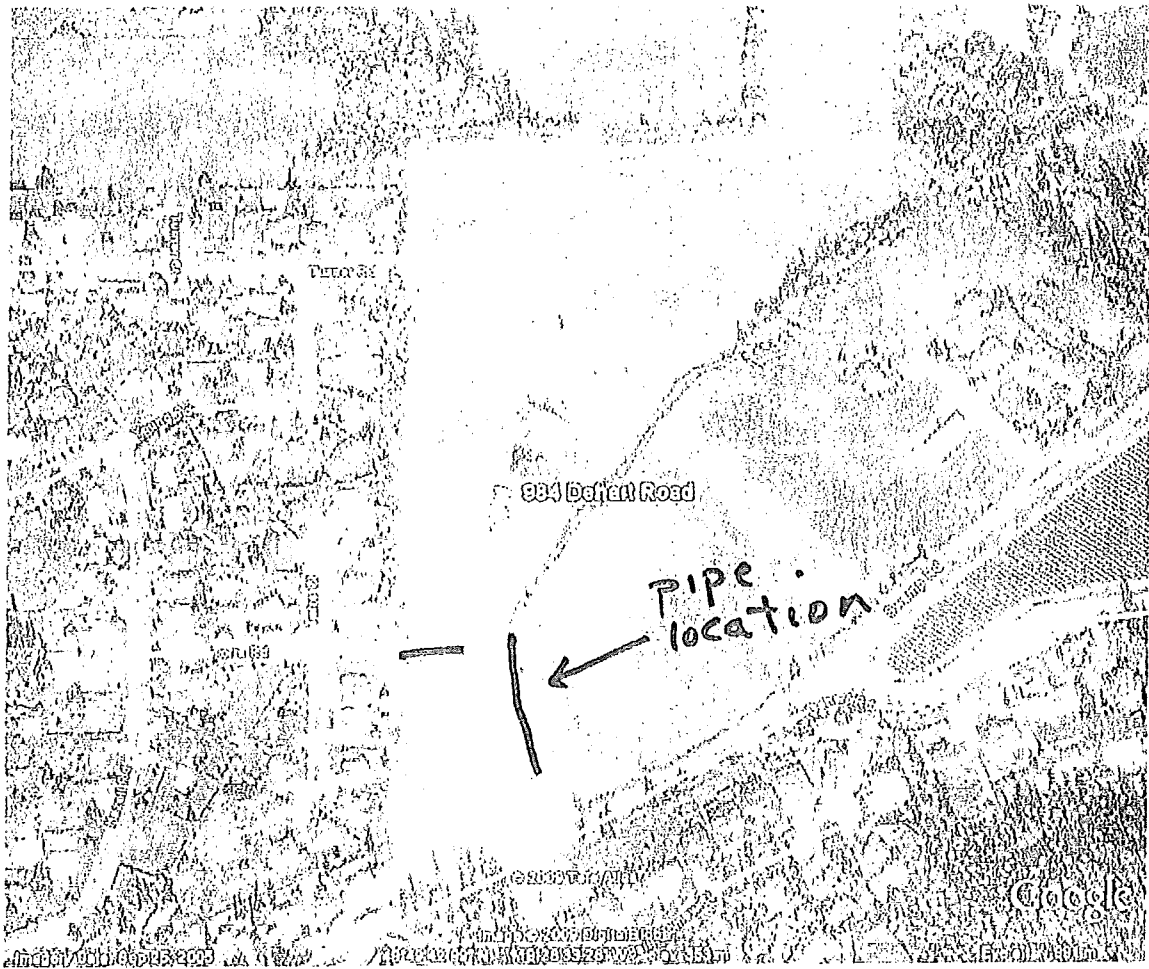


Figure 2 Close up of Site

Site Plans

Photos

2/1

48 AUG 28 10 44

KM084923

(55)

LAND TITLE ACT
FORM C

(Section 219.81)

LAND TITLE OFFICE
KAMLOOPS/NELSON

Province of
British Columbia

AGENT
KAMLOOPS REGISTRY
SERVICE LTD

GENERAL INSTRUMENT - PART 1

(This area for Land Title Office use) PAGE 1 of 2 pages

1. APPLICATION: (Name, address, phone number and signature of applicant, applicant's solicitor or agent) (250) 862-9692
HEATHER J. DUNLOP, Barrister & Solicitor, 1974 McDougall St., Kelowna, B.C. V1Y 1A3

2. PARCEL IDENTIFIER(S) AND LEGAL DESCRIPTION(S) OF LAND: *
(PID) (LEGAL DESCRIPTION)
024 095 141 LOT 1 SECTION 31 TOWNSHIP 29 ODYD PLAN KAP61330 KAP 62654

3. NATURE OF INTEREST: * DESCRIPTION	DOCUMENT REFERENCE (page and paragraph)	PERSON ENTITLED TO INTEREST
Covenant	entire document	TRANSFEREES

4. TERMS: Part 2 of this instrument consists of (select one only)

- | | | | | |
|---------------------------------|--------------------------|---------------------------------------|----------------------------|---------|
| (a) Filed Standard Charge Terms | <input type="checkbox"/> | D.F. No. | 01 98/08/28 10:58:06 01 KL | 209389 |
| (b) Express Charge Terms | <input type="checkbox"/> | Annexed as Part 2 | CHARGE | \$55.00 |
| (c) Release | <input type="checkbox"/> | There is no Part 2 of this instrument | | |

A selection of (a) includes any additional or modified terms referred to in item 7 or in a schedule annexed to this instrument. If (c) is selected, the charge described in item 3 is released or discharged as a charge on the land described in item 2.

5. TRANSFEROR(S): *
ALEXANDER ALLAN REZANSOFF

6. TRANSFEREE(S): (including postal address(es) and postal code(s)) * CITY OF KELOWNA, a municipal corporation having offices at 1435 Water Street, Kelowna, B.C. V1Y 1J4

7. ADDITIONAL OR MODIFIED TERMS: *
n/a

8. EXECUTION(S): ** This instrument creates, assigns, modifies, enlarges, discharges or governs the priority of the interest(s) described in item 3 and the transferor(s) and every other signatory agree to be bound by this instrument, and acknowledge(s) receipt of a true copy of the filed standard charge terms, if any.

Officer Signature(s)

HEATHER J. DUNLOP
Barrister & Solicitor
1974 McDOUGALL ST.
KELOWNA, B.C. V1Y 1A3

Execution Date		
Y	M	D
98	06	03

Party (last signature(s))

ALEXANDER ALLAN REZANSOFF

OFFICER CERTIFICATION:

Your signature constitutes a representation that you are a solicitor, notary public or other person authorized by the Evidence Act, R.S.B.C. 1979, c. 116, to take affidavits for use in British Columbia and certifies the matters set out in Part 5 of the Land Title Act as they pertain to the execution of this instrument.

- * If space insufficient, enter "SEE SCHEDULE" and attach schedule in Form E.
- ** If space insufficient, continue executions on additional page(s) in Form D.

Page 2 of 6 Pages

PART 2

THIS INDENTURE made this 24th day of July, 1998.

BETWEEN:

ALEXANDER ALLAN REZANSOFF, of Box 9, Stn G,
Calgary, Alberta T3A 2G1

(hereinafter referred to as the "Grantor")

OF THE FIRST PART

AND:

CITY OF KELOWNA, a municipal corporation, having
its offices at 1435 Water Street, Kelowna, British
Columbia V1Y 1J2
(hereinafter referred to as the "Grantee")

OF THE SECOND PART

WHEREAS:

A. The Grantor is the registered owner in fee simple of all and singular that certain parcel or tract of land and premises, situate, lying and being in the City of Kelowna, in the Province of British Columbia and being more particularly known and described as:

PARCEL IDENTIFIER: 024-095-141
LOT ~~A~~ SECTION 31 TOWNSHIP 29 OSOYOOS DIVISION YALE DISTRICT
PLAN ~~KAP 61930~~ KAP 62654

(hereinafter referred to as the "Lands");

B. The Grantee is a municipality duly incorporated pursuant to the "Municipal Act", R.S.B.C., Ch. 323 and amendments thereto;

C. The Grantor has applied to the City to develop the Lands;

pg 3

D. The Grantee, pursuant to the provisions of the Land Title Act, required this Indenture be entered into as a condition of its consent to the development of the Lands;

NOW THEREFORE IN CONSIDERATION of the premises and the sum of ONE DOLLAR (\$1.00) paid by the Grantee to the Grantor, the receipt and sufficiency whereof is hereby acknowledged:

1.1 The Grantor, for itself and for its successors and assigns, hereby covenants, promises and agrees that with respect to the area marked "Lot 1" of the Reference Plan KAR 62654 filed herewith:

That no further development may occur until the following requirements are addressed to the satisfaction of the Approving Officer of the City of Kelowna:

- a) Sanitary Sewer and a community water supply with fire protection capability are provided to service proposed Lot 1.
- b) A complete geotechnical report must be prepared by a Professional Engineer, competent in the field of geotechnical study. The report must identify the material used to fill the property and all remedial actions necessary to make the property suitable for development. The report must also address surface and ground water characteristics with recommendations for an overall drainage plan. Terms of Reference for the geotechnical study must be approved by the Development Engineer before a geotechnical consultant is commissioned. The report must also make recommendations for any items that should be included in a restrictive covenant which would safeguard building sites and utility installations.
- c) A site grading plan and drainage plan must be completed to the satisfaction of the Development Engineer. These plans must address the findings of the geotechnical report mentioned above.
- d) A suitable landscaped buffer complete with fencing is established along the border of the ALR and non-ALR lands to the southeast and to the north to the satisfaction of the Approving Officer for the City of Kelowna.
- e) A geotechnical report on the Lands has been prepared by Golder Associates Ltd. on the 30 day of Jan, 1998-1996. A copy of this report is attached hereto as Schedule "A".
- f) A complete inventory of springs, drainage channels and water licenses that affect this property must be identified. A hydrogeotechnical study must be completed to identify how this water is to be handled through the

pg 4

subject property and where culverts and drainage channels are to be established.

2. The Grantor will, forthwith after execution hereof by the Grantor and the Grantee and at its own expense, do or cause to be done all acts or things necessary to ensure that this document is registered against the title to the Lands in the Kamloops Land Title Office.

3. The Grantor will pay all of the costs and expenses of performing the obligations hereby created.

4. The Grantor will upon the reasonable request of the Grantee make, do, execute or cause to be made, done or executed all such further and other lawful acts, deeds, documents and assurances whatsoever for the better or more perfect and absolute performance of the grants, covenants, promises and agreements herein set forth as may be requisite.

5. This Agreement will not be cancelled or modified without the prior written consent of the Approving Officer for the City of Kelowna.

6. The restrictions and covenants herein contained shall covenants running with the Lands and shall be perpetual and shall be registered in the Land Title Office at Kamloops, British Columbia, pursuant to Section 219 of the Land Title Act as covenants in favour of the Grantee.

7. The Grantor will indemnify and save harmless the Grantee and its servants and agents against all losses, damages, costs and expenses, including fees of solicitors and other professionals, advisors, arising out of any breach, violation or non-performance of any term, condition, covenant, or other provision of the Agreement.

8.1 No term, condition, covenant or other provision of this Agreement will be considered to have been waived by the Grantee unless the waiver is expressed in writing by the Grantee.

8.2 Any waiver by the Grantee of any term, condition, covenant or other provision of this Agreement or any waiver by the Grantor of any breach, violation or non-performance of any term, condition, covenant or other provisions of this Agreement does not constitute and will not be construed as a waiver of any further or other term, condition, covenant or other provision of this Agreement or any further or other breach, violation or non-performance of any term, condition, covenant or other provision of this Agreement.

9. The terms, conditions, covenants and other provisions of this Agreement will extend to, be binding upon

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and enure to the benefit of the parties to this Agreement and their respective successors and assigns.

10. In this Agreement unless the context otherwise requires, the singular includes the plural and vice versa.

11. This Agreement will be interpreted according to the laws of the Province of British Columbia.

12. Where there is a reference to an enactment in this Agreement, the reference will include any subsequent enactment of the Province of British Columbia of the effect and all enactments referred to are enactments of the Province of British Columbia.

13. If any part of this Agreement is found to be illegal or unenforceable, that part will be considered separate and severable the remaining parts will not be affected thereby and will be enforceable to the fullest extent permitted by law.

14. Nothing contained or implied in this Agreement will prejudice or affect the rights, powers and remedies of the City in the exercise of the City's functions under any public or private statutes, regulations, bylaws or orders or in equity, all of which may be fully and effectively exercised by the City in relation to the Grantor or the lands as if this Agreement had not been made.

15. This Agreement will not be modified or discharged except in accordance with the provisions of Section 219 of the Land Title Act.

16. Wherever the expression "Grantor" and "Grantee" are used herein the same shall be construed as meaning the plural, feminine or body corporate or politic where the context or the parties so require.

17. This Agreement shall enure to the benefit of and be binding upon the parties hereto, their respective heirs, executors, successors and assigns, but nevertheless, the same shall be binding upon the Grantor as personal covenants only during the period of its respective ownership of any interest in the Land.

IN WITNESS WHEREOF the parties have affixed their hands and

pg 6

seals the day and year first above written.

EXECUTION(S)

Officer Signature(s) Execution Date: Party(ies) Signature(s)
Y M D

[Signature]
as to all signatures

98 06 04 [Signature]

ALEXANDER ALLAN
REZANSOFF

HEATHER J. DUNLOP
Barrister & Solicitor
1974 McDOUGALL ST.
KELOWNA, B.C. V1Y 1A3

THIS IS THE INSTRUMENT creating the condition or covenant entered into under Section 219 of the Land Title Act by the registered owners referred to herein and shown on the print of plan initialled by me and annexed hereto.

[Signature] July 29 1998
Approving Officer for City of Kelowna
Robert G. Shaughnessy

~~End of Document.~~ ^{etc.}

Page 7

Golder Associates Ltd.

SCHEDULE "A"

243-1889 Springfield Road
 Kelowna, BC, Canada V1Y 5V5
 Telephone (604) 860-8424
 Fax (604) 860-9874



Our Ref.: 962-4008

January 30, 1996

Mr. Alex Rezansoff
 3162 Watt Road
 Kelowna, British Columbia
 V1W 3C8

(JP) 860-5811
 Pg: 717-3815-9426
 H: 861-3122

**RE: PRELIMINARY GEOTECHNICAL INVESTIGATION
 PROPOSED SUBDIVISION,
 LOT 2, PLAN 35773, SEC. 31, TP. 29, O.D.Y.D.
 DEHART ROAD, KELOWNA, BRITISH COLUMBIA**

Dear Sir:

As requested, Golder Associates Ltd (Golder) have completed a preliminary geotechnical investigation at the above reference site. The purpose of the investigation was to identify the subsurface soil and groundwater conditions and based on our interpretation of this information, to provide preliminary comments and recommendations pertaining to the geotechnical aspects of the proposed subdivision development.

The field work was carried out on January 17 and 18, 1996 during which time 9 test pits were excavated at the approximate locations shown on Figure 1 using an excavator provided by the owner of the property. The test pits were extended to depths ranging from 2.2 to 5.1 m below the existing ground surface. Standpipe piezometers were installed in selected test pits to be used for monitoring the site groundwater levels.

The test pits were located and logged in the field by a member of our geotechnical staff. Representative samples of the various insitu soil deposits were collected and brought back to our laboratory for examination and testing. Testing consisted of grain size analyses of selected soil samples.

In addition to the foregoing, the soils in Test pits 10 to 16, inclusive excavated by others together that exposed in cut faces were logged.

January 30, 1996

- 2 -

962-4008

1.0 SITE CONDITIONS AND PROPOSED DEVELOPMENT

The property about 11.7 hectares in area is located northwest of the intersection of Dehart and Swamp Roads. As shown on Figure 1, the site is split into two lots designated as Lots A and B. The west boundary of the site is defined by an existing residential development. Except for a Natural Gas right of way that cuts diagonally across the northwest corner of the site and a single building located in the approximate center of the property, the remaining site area is undeveloped.

Observations and available topographic information indicates Lot A consists of a lowlying area while Lot B rises up to the east and south to form a gently sloping terrace or bench. Further observations indicate that Lot A is overlain by a variable thickness of fill materials. Based on discussions, it is understood that the majority of the fill consists of excavation spoil material from the recent construction of the Mission sewer project. The east portion of Lot B was noted to support an orchard while the west part of the lot was grass covered and contained a small marshy area located in the northwest corner of this lot.

Observations also indicate the presence of a drainage ditch system located around the perimeter of Lot A as shown on Figure 1. At the time the investigation was conducted, water was noted flowing in the ditches. Discussions indicate that flows are present throughout the year. Further observations indicate the presence of several springs as shown on Figure 1. It is understood that prior to excavation of the ditch system as well as general infilling, Lot A was generally wet and marshy.

Discussions and observations indicate the presence a discharge pipe located along the northeast side of Lot B as shown on Figure 1. During the field investigation, a significant amount of water was noted discharging from the pipe outlet.

Observations indicate that excavation of the ditch along the southeast side of the property and between Lots A and B has resulted in near vertical cut slopes. The combination of the steepness of the cut together with the soil conditions as well as groundwater seepage discharge has resulted in the occurrence of local slope failures.

Details of the proposed subdivision development are not known, however it is understood that consideration is being given to establishing a mixture of single and multi family units on the property as well as a care facility.

2.0 SUBSURFACE CONDITIONS

Detailed descriptions of the soil and groundwater conditions encountered in the test pits are presented on the attached Record of Test pit and Soil Exposure in Cut Face log

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sheets. The following provides a brief description of the subsurface conditions in order of increasing depth.

2.1 Fill

As indicate above, observations indicate that Lot A is overlain by a variable thickness of fill materials. The test pit results indicate the fill thickness ranges from 1.3 to 2.9 m. Observations indicate the fills located in the south portion of Lot A are as much as 5.0 to 7.0 m thick. It is understood that these thick fills have been in place for a number of years.

In general, the fills were noted to consist primarily of a mixture of sand and gravel containing a variable silt, cobble and boulder content. Lumps of silt or clay as well as miscellaneous debris were also noted to be mixed with the granular component of the fills. Typical grain size distribution curves of the granular portion of the fills are shown on Figure 2.

Fill materials were not observed on Lot B.

2.2 Topsoll

The surficial organic soils encountered beneath the fills in Test pits 1 to 3, inclusive and Test pit 7 ranged in thickness from 0.1 to 0.4 m. On Lot B, the surficial organic soils

ranged from 0.2 to 0.6 m thick.

2.3 Peat

The areas underlain by loose peat deposits are approximately delineated on Figure 1. In general, the test pit results indicate the peats to range from 0.3 to 2.3 m thick. It is expected that thicker peat deposits could be present in local areas. These deposits were encountered at ground surface and at depths of 1.7 to 2.9 m below the existing ground surface.

2.4 Interlayered Deposits

2.4.1 Lot A

In general, the above fills and surficial organic soils or peats are followed by a mixed and interlayered sequence of sand and/or silt. The relative density of these granular deposits was estimated to range from loose to compact. In Test pits 3, 6 and 7, the above granular deposits are interlayered with layers of firm to stiff clay.

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All the test pits excavated on this lot were terminated in the above soils at depths between 3.1 and 4.9 m below the existing ground surface. Test pit 7 was terminated at 4.2 m due to refusal on an unknown surface.

2.4.2 Lot B

Based on the test pits excavated by others as well as the soil conditions observed in the cut slopes located along the north and west side of the lot, it is inferred that this lot is underlain by an interlayered and mixed sequence of clay, silt sand and gravel. The relative density of the granular deposits was estimated to range from loose to compact while the consistency of the fine grained soils varied from firm to stiff.

2.5 Groundwater Conditions

The following table summarizes the depths where the groundwater table and/or seepage levels were measured at the time the investigation was conducted.

<u>TEST PIT NO.</u>	<u>DEPTH TO STANDING WATER LEVEL, m</u>	<u>DEPTH TO SEEPAGE LEVEL, m</u>
TP1	-	1.0
TP2	1.6	-
TP3	-	2.3
TP4	-	2.8
TP5	0.3	-
TP6	1.6	-
TP8	2.5	-
TP12	0.6	-
TP13	0.6	-
TP14	0.6	-
TP15	0.7	-
TP16	1.0	-

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At the time the investigation was conducted, the groundwater/seepage levels within Lot A were encountered at the original ground surface or within the surficial fill materials. Only in Test pits 3 and 4 was groundwater seepage observed at depth in the peat deposits.

On Lot B, the groundwater levels in the west portion of this lot were observed within 0.6 to 1.0 m of the existing ground surface. The water levels in the east part of the lot are expected to be encountered at deeper depths.

It is anticipated that the groundwater/seepage levels will be subject to seasonal variations. High groundwater conditions are expected to occur during the late spring/early summer months or during periods of heavy or sustained precipitation.

3.0 DISCUSSION

Based on the results of the investigation, the following presents our preliminary comments and recommendations regarding development of the property for residential purposes.

- Buildings constructed over the existing conditions encountered in Lot A without prefoundation treatment will result in unacceptable total and differential settlements. In order to eliminate or reduce the post construction settlements, the following foundation treatments are presented for consideration:
 - (a) Complete subexcavation of the fills, surficial topsoil layer and/or peat deposits from beneath the plan area of the buildings and replacement with compacted granular grade fills.
 - (b) Pile support of the structure and floor slab. A further investigation including drilling of boreholes would be required to determine the appropriate length and capacity of the piles at the proposed building locations.
 - (c) Subexcavation of only the fills and surficial topsoil layer, replacement with compacted granular fill and provide a preload fill over the proposed building site to reduce the post construction settlements that will occur in the peat deposits. It should be noted that after completion of preloading to remove the majority of the primary settlements, the proposed buildings will still be subject to long term residual settlements that could be detrimental to the settlement sensitive features of the structure. As indicated in Section 2.1, relatively thick fills are present within the south portion of Lot A and have been in place for a number of years. It is considered likely that if the peat deposits are present beneath the fills that the majority of the primary settlements have been removed and only long term residual settlements will occur. Providing the site grade is not raised, it is considered possible that structures could be built in this area with minimal

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prefoundation treatment. Prefoundation treatment in this case would consist of construction of a minimum 1.0 m thick compacted sand and gravel pad under the structure. As indicated above buildings constructed under the foregoing conditions will be subject to long term residual settlements.

- Based on the results of the investigation, it is our opinion that prefoundation treatment in Lot B will essentially consist of stripping the relatively thin surficial organic soils from the proposed building and/or grade fill areas. Upon completion of this, the proposed structures can be supported on conventional strip and/or spread footings founded on the native firm clay strata, compact sand and silt or compact sand and gravel deposits.
- To provide a relatively settlement free pavement structure, it is recommended that the fills, topsoil and/or peats should be completely subexcavated from beneath the proposed roadways and replaced with compacted granular grade fill. Alternatively, the proposed road right of ways underlain by the peat deposits could be preloaded to reduce the post construction settlements. The existing fills and topsoil should be removed prior to preloading. As indicated above, the roads underlain by the peats will be subject to long term residual settlements. This could require periodic remedial repairs to the pavement structure.
- Based on the results of the investigation, it would appear that the majority of the granular fills excluding the miscellaneous debris and lumps of silt and clay will be suitable for use as grade fills under roadways. Some of the granular fills could be used as structural fills under building locations, however it is recommended that a geotechnical engineer inspect and approve the fills prior to its use.
- Subexcavation of the peat deposits will result in excavations extending below the groundwater table. Based on our experience in the immediate vicinity of the property, it is anticipated that dewatering during excavation and placement of granular grade fill can be carried out using standard sump pump techniques.
- As indicated in Section 2.5, high groundwater levels were encountered in Lot A as well as the west portion of Lot B. An existing ditch system constructed around the perimeter of Lot A appears to partially control the groundwater flows across the property. It is recommended that provisions be made during design of the subdivision to provide adequate subsurface drainage measures. This should include design and construction of seepage interceptor trenches along the upslope side of Lot B as well as along the toe of the cut slope where a number of springs/seepage areas were observed. In addition to the foregoing, all buildings should be provided with a perimeter foundation and/or underslab drainage systems. In Lot A, it may be necessary to raise the site grade to ensure that the main floor of the buildings are at least 1.5 m above the highest anticipated site water level.

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- The ditch excavation cut along the southeast side of Lot A will require stabilization works to prevent further occurrences of slope failures. This work could include construction of either a granular toe berm, retaining structure such as a concrete gravity wall or interlocking modular concrete blocks with geosynthetic reinforcement and/or flattening the excavation cut slope. In the foregoing considerations suitable subsurface drainage works should be included in the design.

Yours very truly,

GOLDER ASSOCIATES LTD.


(FOR) G. Imada, P. Eng.


B. Carlsen, P.Eng.

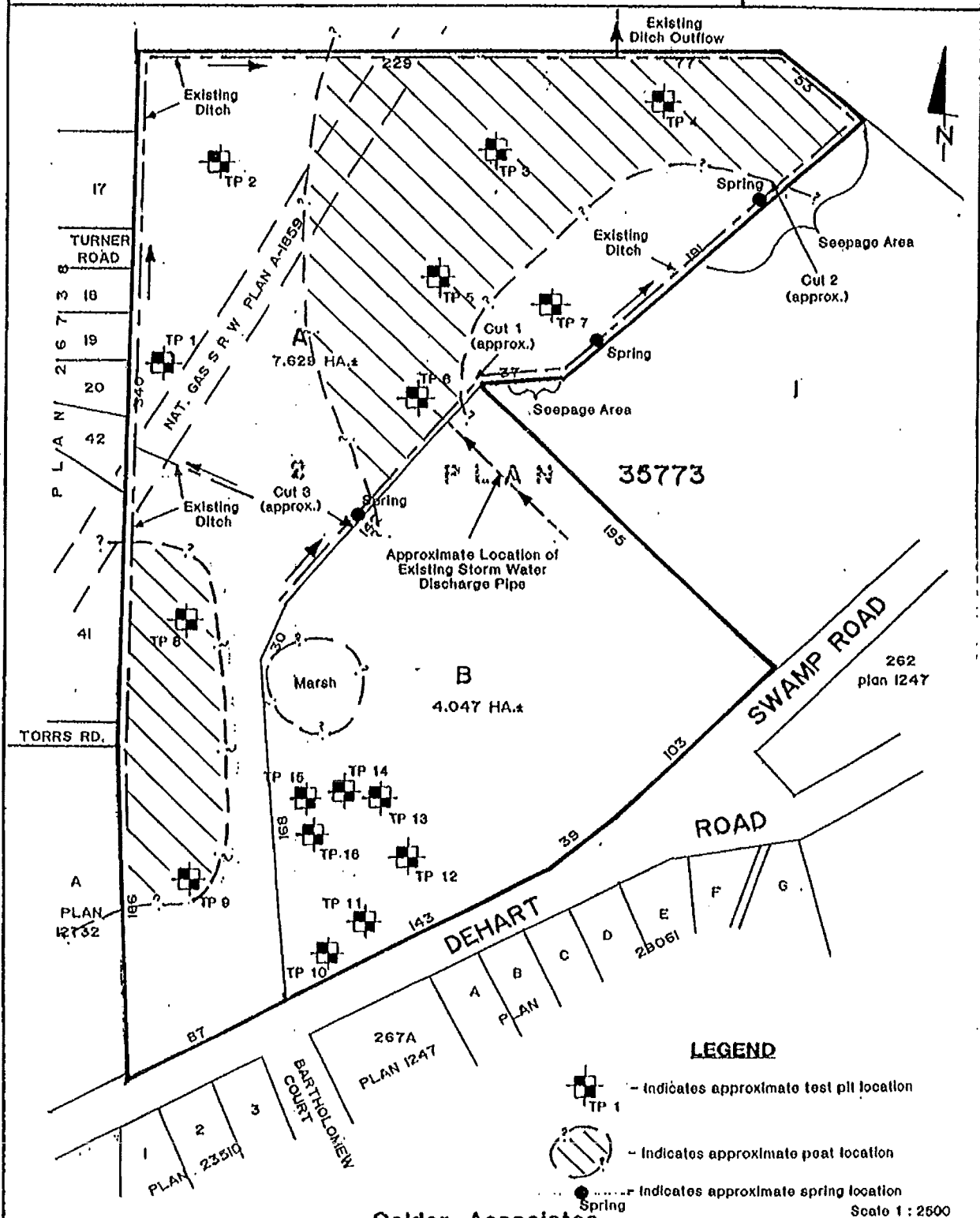
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


SITE PLAN

Figure 1



PROJECT No. 962-4008 DRAWN AR REVIEWED GTI DATE JAN/98

LEGEND

-  - Indicates approximate test pit location
-  - Indicates approximate poat location
-  - Indicates approximate spring location

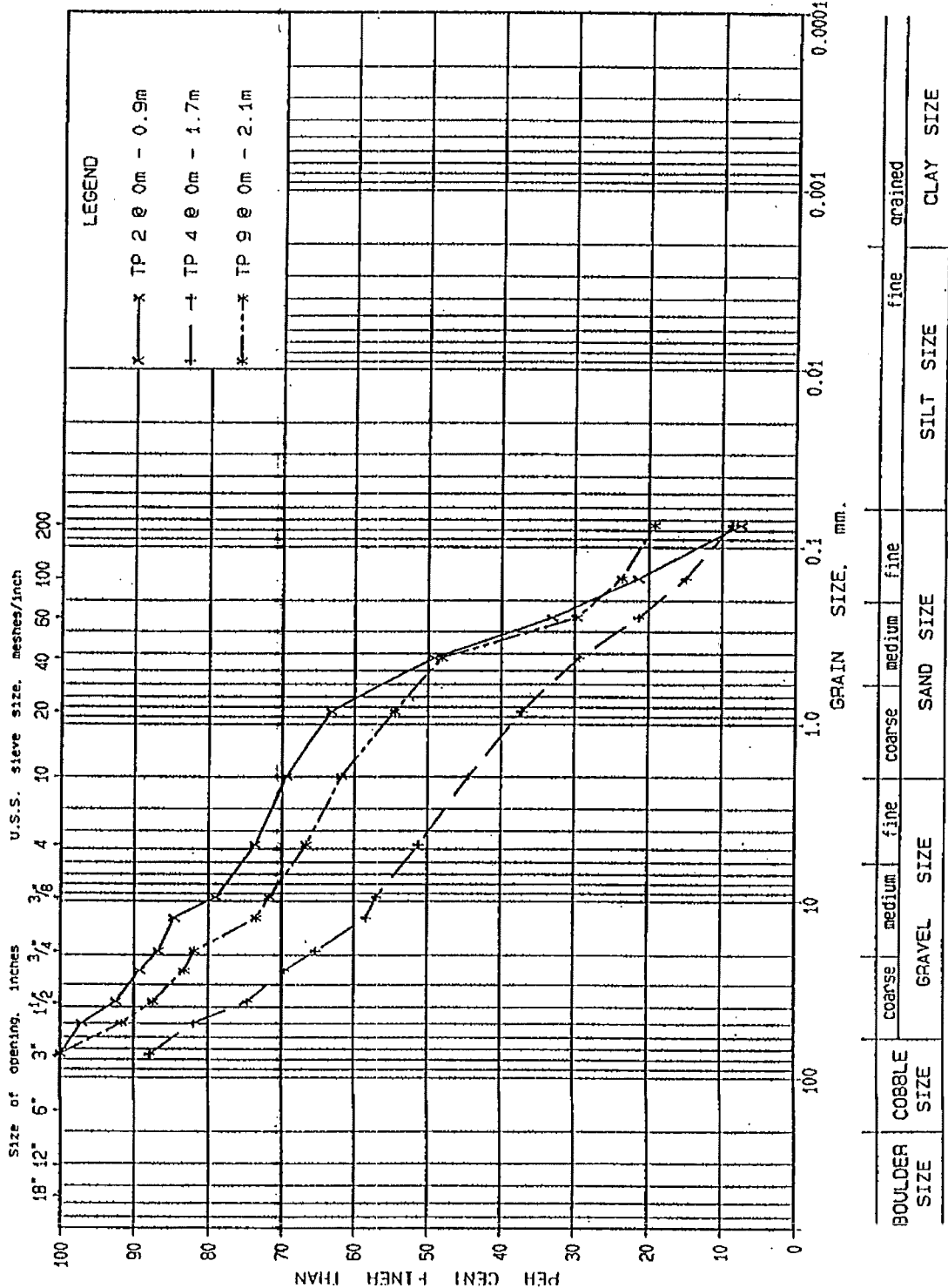
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Scale 1 : 2500

GRAIN SIZE DISTRIBUTION

Figure: 2

M.I.T. GRAIN SIZE SCALE



PROJECT NO. 962-4008 DRAWN BY REVIEWED GTI DATE Jan./96

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RECORD OF TEST PITS		
January 24, 1996		962-4008
TEST PIT NO.	DEPTH, (m)	SOIL DESCRIPTION
TP 1	0.0 - 1.4	Loose brown SAND and GRAVEL, trace silt and cobbles, occasional boulder, root and piece of wood. (FILL)
	1.4 - 1.8	Loose dark brown organic SILT and SAND. (TOPSOIL)
	1.8 - 2.7	Loose to compact mottled grey fine to medium SAND, some silt with medium SAND seams, occasional rootlet.
	2.7 - 4.2	Loose grey SAND. Minor groundwater seepage at 1.0 m. (January 17, 1996)
TP 2	0.0 - 1.5	Loose brown SAND and GRAVEL, trace to some silt, cobbles, roots and occasional pieces of wood and pipe. (FILL)
	1.5 - 1.8	Loose dark brown organic SILT and SAND. (TOPSOIL)
	1.8 - 3.1	Loose to compact mottled grey fine to medium SAND, some silt interlayered with medium SAND, trace rootlets. Standing groundwater level at 1.64 m. (January 18, 1996)
TP 3	0.0 - 1.3	Loose brown gravelly SAND, some silt, trace cobbles, organics, occasional boulder, pieces of pipe and wood. (FILL)
	1.3 - 1.6	Loose dark brown organic SILT and SAND. (TOPSOIL)
	1.6 - 1.7	Compact grey fine sandy SILT, some rootlets.
	1.7 - 2.0	Loose brown medium SAND.
	2.0 - 3.8	Loose brown to dark brown fibrous PEAT, occasional piece of wood.
	3.8 - 4.1	Loose grey SAND, some silt.
	4.1 - 4.7	Firm grey CLAY, some silt and trace rootlets. Minor groundwater seepage at 2.3 m. (January 17, 1996)

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RECORD OF TEST PITS		
January 24, 1996		962-4008
TEST PIT NO.	DEPTH, (m)	SOIL DESCRIPTION
TP 4	0.0 - 1.7	Loose brown SAND and GRAVEL, some cobbles, trace silt and organics, occasional boulder, pieces of pipe and asphalt. (FILL)
	1.7 - 4.0	Loose brown fibrous PEAT.
	4.0 - 4.9	Compact grey silty fine SAND interlayered with grey SAND, trace silt grading to a fine to medium SAND, some silt. Minor groundwater seepage at 2.8 m. (January 17, 1996)
TP 5	0.0 - 1.9	Loose brown SAND and GRAVEL, some silt, trace to some cobbles, wood debris, organics and occasional boulder. (FILL)
	1.9 - 3.1	Loose dark brown fibrous PEAT.
	3.1 - 3.9	Loose to compact grey medium SAND interlayered with silty fine SAND containing fibrous PEAT and organic sandy SILT layers.
	3.9 - 4.2	Loose to compact grey SAND, trace silt. Standing groundwater level at 0.3 m. (January 18, 1996)
TP 6	0.0 - 1.9	Loose brown SAND and GRAVEL, some silt, trace cobbles, occasional pieces of wood and metal. (FILL)
	1.9 - 3.1	Loose brown fibrous PEAT.
	3.1 - 4.4	Firm mottled grey silty CLAY, some sand, trace gravel and rootlets.
	4.4 - 4.7	Loose brown SAND, trace gravel with some mottled grey silty SAND layers.
	4.7 - 4.9	Firm grey CLAY, some silt and sand. Standing groundwater level at 1.64 m. (January 18, 1996)

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RECORD OF TEST PITS		
January 24, 1996		962-4008
TEST PIT NO.	DEPTH, (m)	SOIL DESCRIPTION
TP 7	0.0 - 2.0	Loose brown SAND, some silt, organics and roots, occasional boulder. (FILL)
	2.0 - 2.1	Loose dark brown organic SILT and SAND. (TOPSOIL)
	2.1 - 3.5	Compact mottled brown SILT, some sand interlayered with SAND, some silt.
	3.5 - 3.9	Stiff mottled grey CLAY, some silt with brown SAND layers, trace rootlets.
	3.9 - 4.2	Compact mottled brown silty SAND, some gravel and occasional cobble.
	4.2	Refusal at 4.2 m. Test pit dry at time of excavation.
TP 8	0.0 - 2.9	Loose brown SAND and GRAVEL, trace silt and cobbles, occasional pieces of wood, brick and concrete. (FILL)
	2.9 - 5.1	Loose dark brown fibrous PEAT interlayered with compact grey silty fine SAND, trace rootlets. Standing groundwater level at 2.5 m. (January 18, 1996)
TP 9	0.0 - 2.2	Loose brown SAND and GRAVEL, trace to some cobbles, occasional boulder intermixed with grey CLAY, some silt, trace organics, occasional pieces of wood and asphalt. (FILL) Test pit dry at time of excavation.
TP 10	0.0 - 1.0	Loose to compact brown SAND and GRAVEL. Test pit excavated by others and dry at time inspection. (January 18, 1996)
TP 11	0.0 - 1.0	Loose to compact brown SAND and GRAVEL. Test pit excavated by others and dry at time inspection. (January 18, 1996)

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RECORD OF TEST PITS		
January 24, 1996		962-4008
TEST PIT NO.	DEPTH, (m)	SOIL DESCRIPTION
TP 12	0.0 - 0.6	Loose dark brown organic SILT and SAND, some rootlets and trace gravel. Test pit excavated bt others. Standing groundwater level at 0.6 m. (January 18, 1996)
TP 13	0.0 - 0.6	Loose dark brown organic SILT and SAND, some rootlets and trace gravel. Test pit excavated by others. Standing groundwater level at 0.6 m. (January 18, 1996)
TP 14	0.0 - 0.4	Loose dark brown organic SILT and SAND, some rootlets and gravel. Test pit excavated by others. Standing groundwater level at 0.4 m. (January 18, 1996)
TP 15	0.0 - 0.2	Loose dark brown organic SILT and SAND, some rootlets and gravel.
	0.2 - 0.7	Compact brown SAND and GRAVEL. Test pit excavated by others. Standing groundwater level at 0.7 m. (January 18, 1996)
TP 16	0.0 - 0.2	Loose dark brown organic SILT and SAND, trace gravel.
	0.2 - 1.0	Compact brown SAND and GRAVEL, some silt. Test pit excavated by others. Standing groundwater level at 1.0 m. (January 18, 1996)

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RECORD OF SOIL EXPOSURE IN CUT FACES		
January 24, 1996		962-4008
CUT LOCATION	DEPTH, (m)	SOIL DESCRIPTION
CUT 1	0.0 - 0.4	Loose dark brown organic SILT and SAND. (TOPSOIL)
	0.4 - 2.8	Firm brown CLAY, some silt with sand seams.
	2.8 - 3.7	Loose to compact brown SAND and GRAVEL with thin CLAY and SILT layers.
	3.7 - 4.1	Compact mottled brown silty fine SAND. Groundwater level at 4.1 m.
CUT 2	0.0 - 0.3	Loose dark brown fibrous PEAT.
	0.3 - 1.3	Compact light brown to brown fine SAND interlayered with sandy SILT. Groundwater level at 1.3 m.
CUT 3	0.0 - 0.4	Loose dark brown to brown organic silty SAND, some rootlets. (TOPSOIL)
	0.4 - 4.3	Compact mottled brown grey fine sandy SILT interlayered with fine SAND, grey CLAY, some silt and brown SAND and GRAVEL. Groundwater level at 4.3 m.

End of Document

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CITY OF KELOWNA

BYLAW NO. 10964

Official Community Plan Amendment No. OCP13-0011 -
Sherwood Mission Developments Ltd., Inc. No. BC0795928
984 Dehart Road

A bylaw to amend the "Kelowna 2030 - Official Community Plan Bylaw No. 10500".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT Map 4.1 - **GENERALIZED FUTURE LAND USE** of "Kelowna 2030 - Official Community Plan Bylaw No. 10500" be amended by changing the Generalized Future Land Use designation of Lot 1, Section 31, Township 29, ODYD, Plan KAP62654, located on Dehart Road, Kelowna, B.C., from the Multi-Unit Residential Low Density (MRL) designation to the Single/Two Unit Residential (S2RES) designation, and a portion of Single/Two Unit Residential (S2RES) be changed to Multi-Unit Residential Low Density (MRL) designation as per Map "A" attached to and forming part of this bylaw;
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this

Considered at a Public Hearing on the

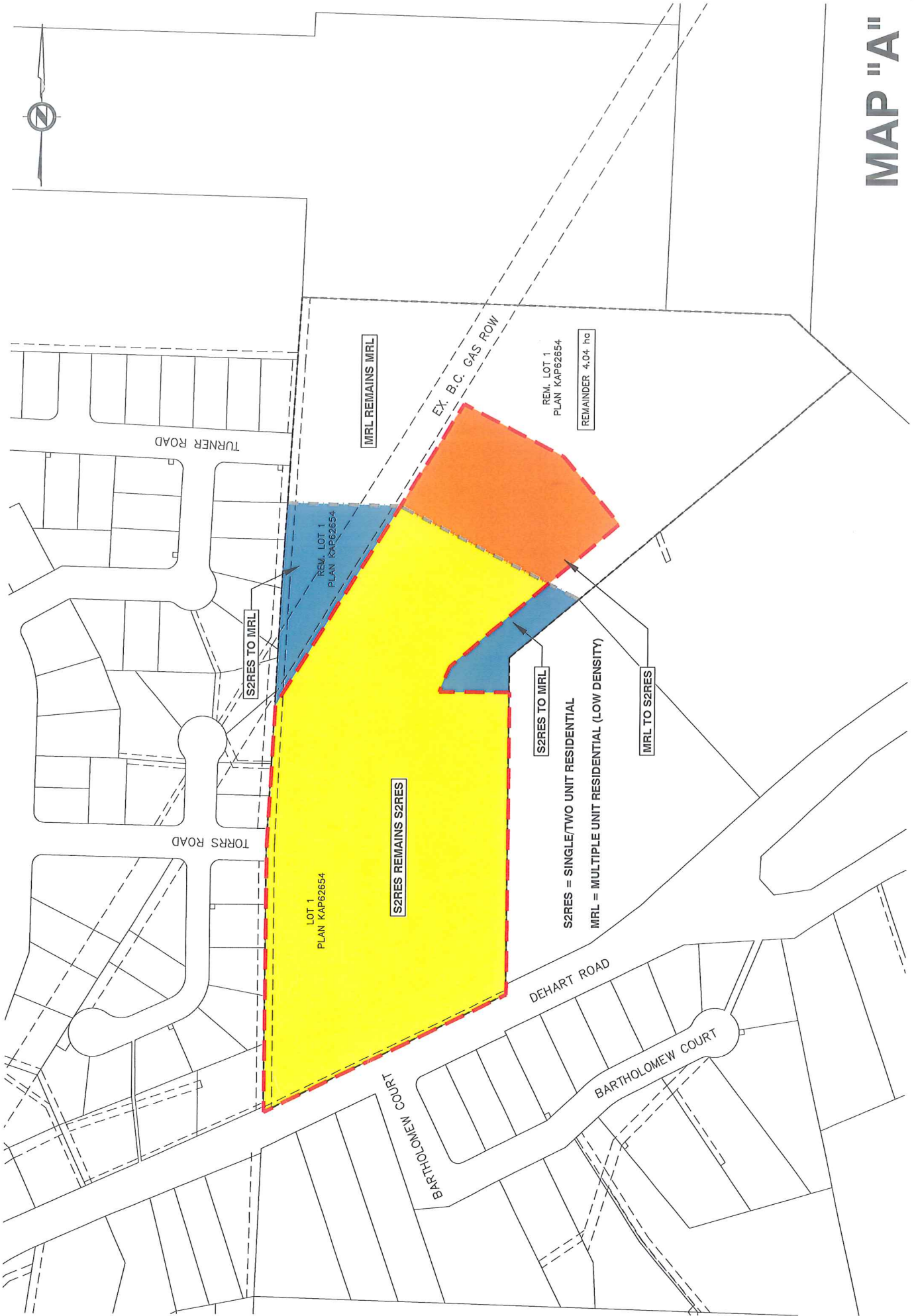
Read a second and third time by the Municipal Council this

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

MAP "A"



CITY OF KELOWNA

BYLAW NO. 10965

Z13-0016 - Sherwood Mission Developments Ltd., Inc. No.

BC0795928

984 Dehart Road

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of part of Lot 1, Section 31, Township 29, ODYD, Plan KAP62654 located on Dehart Road, Kelowna, B.C., from the A1 - Agriculture 1 zone to the RU1 - Large Lot Housing zone as per Map "B" attached to and forming part of this bylaw.
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this

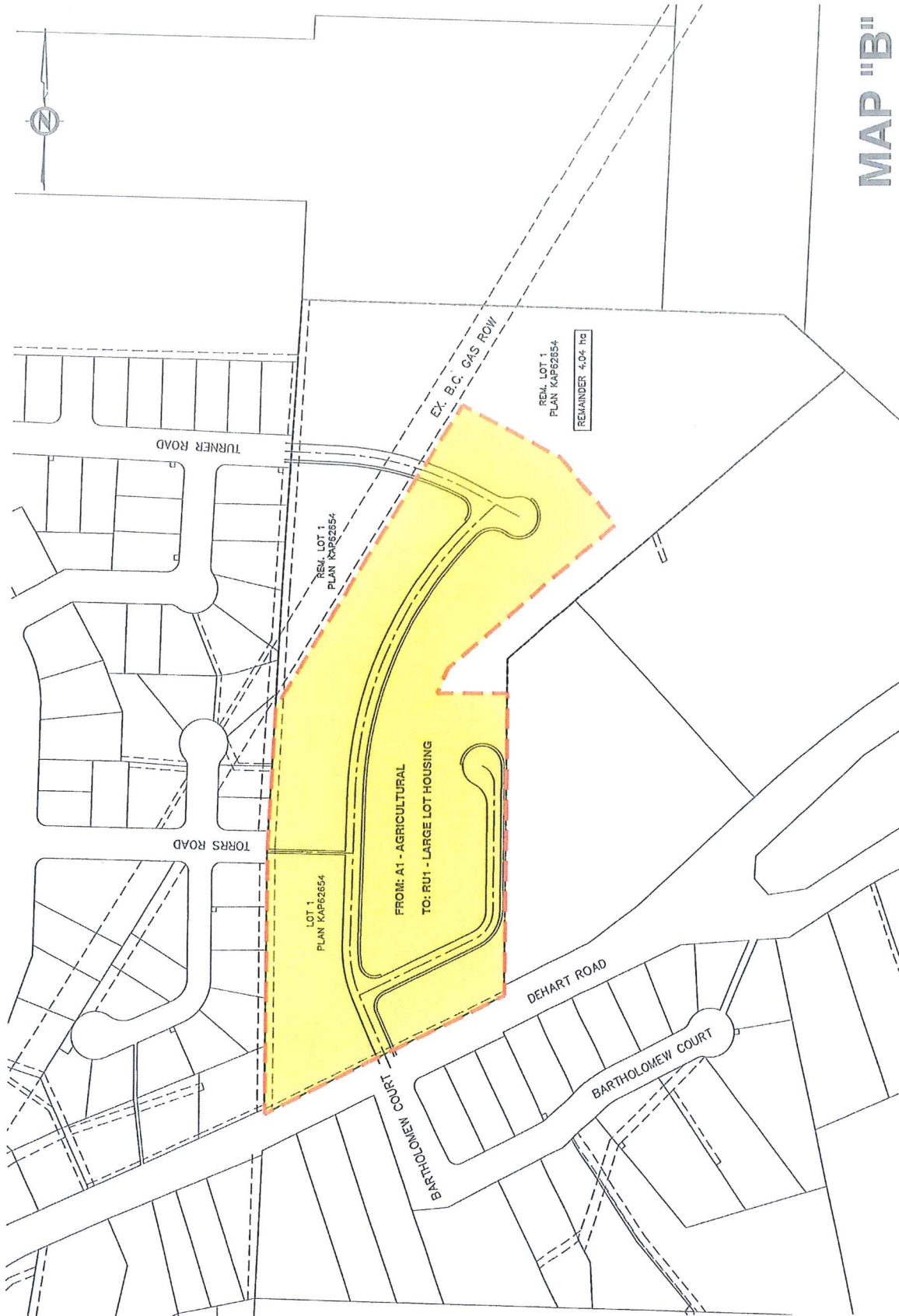
Considered at a Public Hearing on the

Read a second and third time by the Municipal Council this

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk



MAP "B"

Report to Council



Date: 5/7/2014
File: 1620-15
To: City Manager
From: Mariko Siggers, Event Development Supervisor
Subject: Center of Gravity 2014

Recommendation:

That Council receives, for information, the report from the Event Development Supervisor dated May 7, 2014 regarding the 2014 Center of Gravity Festival.

Purpose:

To provide Council with an update on the plans for the 2014 Center of Gravity Festival as requested by Council at the October 28, 2013 Council meeting.

Background:

Center of Gravity is organized by Wet Ape Productions (Wet Ape) and has been held in Kelowna since 2007. It is a showcase for sports and music featuring high level athletes in seven different sports and highly recognized musical acts. In recent years, the event has sold out bringing many visitors into the community. Following the 2013 Center of Gravity Festival, an extensive review of the festival was conducted to gain an understanding of the benefits and challenges of the event. These findings were summarized and presented to Council on October 28, 2013. It was recognized that while the festival has many positive attributes and benefits to the community, there are also concerns regarding the demands on emergency services as well as the impact on the surrounding neighbourhood.

In the months following the report to Council, extensive planning has taken place. An "Off-Site Security" sub-committee was formed. Multiple meetings took place with the residents in the neighbourhood south of City Park. Wet Ape in conjunction with the Outdoor Events Committee revised and implemented strategies to improve the event both on and off-site. Below is the list of recommendations included in the October 2013 Report to Council along with the progress that has been made to date.

- *Move Center of Gravity off the August long weekend*
 - The new date for Center of Gravity is July 25-27, 2014

- *Change the music genre and schedule more live (versus DJ) entertainment to promote a more diverse audience*
 - Wet Ape has committed to at least 50 per cent of talent booked on the main stage to be live acts. Staff are kept apprised as talent is booked.
- *Align event capacity numbers with available security and emergency services resources*
 - Prior to approving a capacity of 8000, Wet Ape was required to meet a list of pre-permit conditions (Appendix A). These conditions were met on March 12, 2014 as determined by the Outdoor Events Committee.
 - RCMP compiled a list of additional resources required to effectively police both the event site and the downtown core. The extra costs for these resources are approximately \$18,200 and will be covered by Wet Ape.
- *Implement strategies to minimize damages to the sport field*
 - The concert has been moved from the Sports Field to the North Field which will reduce the requirement for extensive rehabilitation to the sports field because the only programmed activity on this field will be grass volleyball.
- *Evaluate and augment the current security plan in collaboration with Wet Ape, RCMP and security to addresses security checks, contraband controls, site layout and emergency access*
 - Wet Ape hired a dedicated Security Director who has developed a comprehensive on-site security plan with feedback from members of the Outdoor Event Committee including representatives from the RCMP.
- *Focus marketing on the daytime sport events*
 - Wet Ape has committed to putting more emphasis on the sports as opposed to the concert.
- *Neighbourhood patrols to increase security presence*
 - An “Off-Site Security” sub-committee was formed consisting of the Fire Department, BC Liquor Control and Licensing Branch, RCMP, Bylaw and Parking Services to get a complete understanding of the critical issues, times and locations and to develop a plan to address them.
 - A number of meetings have taken place with the residents who live in the neighbourhood south of City Park along the Abbot corridor. One meeting included over 30 residents.
 - Bylaw Services will be increasing patrols throughout the neighbourhood and monitoring the activities taking place. The additional patrols have been estimated to cost an additional \$7,200 which will be paid for by Wet Ape.
- *Enforcement of parking regulations (permit only) for residential areas around the downtown*
 - In collaboration with Bylaw Services, there will be temporary “resident only” event parking regulations to deter people from monopolizing parking and essentially “camping” in the neighbourhood. Installation of temporary parking signage has been estimated at \$1,000 which will be paid for by Wet Ape.

- Increased promotion of available downtown parking will be done through Wet Ape's communication streams.
- *Work with RCMP, Bylaw, Transportation and Parking Services to create an integrated off-site security plan including road closures on Leon and Lawrence Avenues to increase safety for pedestrians and emergency service personnel downtown*
 - In the event that a road closure is required, RCMP will implement this.
- *Improve transportation options during late night/early morning hours*
 - Wet Ape will be increasing the number of downtown pick-up locations and extending their shuttle bus to pick up passengers until 3:00 am from downtown (previously late-night service was only provided to facilitate guests leaving COG House at the Curling Club).
- *Better waste management strategies and improved implementation*
 - Wet Ape has committed volunteers to do litter patrol twice a day in the neighbourhood south of the bridge.

In addition, the City, RCMP and BC Liquor Control & Licensing Branch will not approve any late night liquor extensions for the nightclubs during the Center of Gravity weekend. Bars will be closed by 2:00 a.m. resulting in crowds vacating the downtown earlier than previous years. Wet Ape will operate the Center of Gravity House at the Curling Club again this year which also helps to reduce the number of people downtown once the festival finishes for the night.

In total, over \$26,000 in additional enforcement services will be provided to the community, all of which will be expensed to Wet Ape. These services were assessed carefully by the respective departments, independently of Wet Ape.

Through this process, there has been a tremendous amount of cooperation from all members of the Outdoor Events Committee, the residents and Wet Ape. The intent of implementing the above mentioned changes is to reduce the negative issues associated with the event. It is anticipated that Center of Gravity will continue to benefit businesses, hotels and participants as it has for the past seven years.

Internal Circulation: Divisional Director, Active Living & Culture; Communications Advisor; Parking Manager; Bylaw Services Manager; Parks Community Relations Coordinator

Submitted by: M. Siggers, Event Development Supervisor

Approved for inclusion by Jim Gabriel, Divisional Director, Active Living & Culture

Attachments:
Appendix A - Pre-permit Conditions

cc: Sport and Event Development Manager

Considerations not applicable to this report:

External Agency/Public Comments:

Legal/Statutory Authority:

Legal/Statutory Procedural Requirements:

Existing Policy:

Financial/Budgetary Considerations:

Personnel Implications:

Communications Comments:

Alternate Recommendation:

2014 Center of Gravity Pre-Permit Conditions

By meeting the conditions listed below by the date specified, Wet Ape can continue to proceed with the outdoor application process to plan for an event with,

1. An overall site capacity of 8000 people.
2. An overall liquor licence for 2000 people
 - Urban zone (350)
 - Beach zone (1350)
 - Dirt zone (300)
 - Mainstage (2000)
3. A capacity of 1000-1332 for Center of Gravity House at the Curling Rink (pending assessment by KFD and LCLB)

Conditions:

- Appointment of a qualified Security Director
- Preliminary review of talent booked
- A detailed on-site security plan
 - Created with input from BC Ambulance, Fire, BC LCLB & RCMP (and gang task force if appropriate)
 - Job descriptions for each security position/post
 - Private security that is fully able to handle the demands of Center of Gravity based on approved job descriptions
 - A plan to audit the event to ensure all areas are staffed appropriately according to schedule and job descriptions
- A detailed site map including
 - Service headquarters (RCMP, Ambulance, First Aid, Security etc)
 - All entrances/exits
 - Emergency access/meet points
 - Generators, infrastructure, additional lighting
 - Portapotties
 - Moat in concert area
- A late-night transportation plan
 - Continuous shuttle service coordinated with bars/nightclubs
- An off-site security plan (to be led by RCMP)
 - Neighbourhood patrols
 - Adequate staffing
 - Road closures
 - Parking restrictions

All documents must be received by the Outdoor Events Committee by Feb 28 for review at the OE Committee Meeting March 5, 2014.

Report to Council



Date: 5/7/2014
File: 1620-15
To: City Manager
From: Mariko Siggers, Event Development Supervisor
Subject: 2014 Outdoor Events

Recommendation:

THAT Council receives, for information, the report from the Event Development Supervisor dated May 7, 2014 regarding the 2014 Outdoor Event season.

Purpose:

To provide Council with an update on the 2014 event schedule.

Background:

Events add a dynamic element to Kelowna's landscape with opportunities for culture, recreation and community building. Permits for 80 to 100 events each year are approved, most of which take place between April and October. The City strives to be "event friendly" by linking organizers with the tools, information and resources they require to be successful. At the same time, the integrity and accessibility of public spaces is a priority. The goal is to create a well-balanced schedule that offers a variety of high-quality events without "over-programming" public spaces and roadways in the downtown core.

The Event & Festivals Framework, endorsed by Council in 2012, has provided guidance for a proactive approach to event management. It was developed to align City and stakeholder efforts and reduce barriers for event organizers in order to improve their chance of success. This increases in benefits to the community and also positions the City as a strong competitor with other municipalities when attracting events.

Some of the strategies and initiatives which are continuing to advance the framework are listed below.

Special Events Volunteer Program

- Development of the program is almost complete.
- Components of the program include:

- Online marketplace
 - Templates and resources
 - Training
 - Staff support
- Volunteer Program will engage both volunteers and event organizers and provide a consistent look and feel for volunteer management across all events.
- The program will increase marketing and communication opportunities for events and volunteers.
- The program will be piloted with two to three events in 2014.

Event Development Fund

- The two grant streams support both ongoing and new events and have been well received by the event community
 - City Services Offset Grant
 - Close to \$25,000 will be delivered to twelve events in 2014.
 - This grant provides an annual opportunity to connect with event organizers regarding their planning and development of events and how they align with event development priorities.
 - Strategic Events Grant
 - Skate Canada International (\$10,000) and Keloha Music and Arts Festival (\$15,000) have each received funding from this grant in 2014.
 - Discovery process is ongoing for enhancing existing events or securing new ones.

2014 Highlights

There is a wide variety of events that will take place in Kelowna in 2014. These include races, festivals, one-day celebrations, fundraisers, parades, awareness events, tournaments, films, weddings and churches. Between April and Thanksgiving weekend in October, there is only one weekend that an event does not take place: May 10 and 11. There is always something to see and do in Kelowna.

A number of inaugural events are taking place this year including a brand new triathlon, a cycling camp and a rugby sevens tournament. In addition, some one-time opportunities such as *Skate Canada International*, the *Snowbirds* and the “*Red Bull Tour Bus*” will also be in the community this year. These events are balanced with “home-grown” events that have become cultural and recreational pillars in our landscape, such as the *Kelowna Apple Triathlon* and the *Fat Cat Children’s Festival*. In addition, with the big festivals such as *Canada Day*, *KELOHA* and *Center of Gravity* lined up and complimented by the exceptional talent booked through *Parks Alive!*, the summer schedule has a vibrant music element.

A complete list of events is included in Appendix A.

Opportunities

Staff are consistently doing research as to what events are gaining momentum across Canada and the USA. Specialty running and obstacle course type events of all kinds are very popular and embraced by a wide demographic, regardless of athletic ability. A number of inquiries are received to offer these unique experiences in Kelowna. Selection of these types of events requires careful evaluation in order to maximize benefit to the community and event organizer.

Embracing the unique climate and culture of Kelowna to establish events that complement both the people and the environment is essential. With that in mind, water-based or multi-sport events are a great fit. Partnerships with the Paddle Centre and triathlon organizers are being fostered to build event hosting capacity.

Staff will continue to work with stakeholders to discover, develop or procure events that will bring new experiences to residents and visitors while enhancing the reputation of Kelowna as a vibrant and active community.

Internal Circulation: Divisional Director, Active Living and Cultural Services; Manager, Sport & Event Development Manager;

Considerations not applicable to this report:

Financial/Budgetary Considerations:

External Agency/Public Comments:

Considerations not applicable to this report:

Legal/Statutory Authority:

Legal/Statutory Procedural Requirements:

Personnel Implications:

Alternate Recommendation:

Existing Policy:

Communications Comments:

Submitted by:

M. Siggers, Event Development Supervisor

Approved for inclusion by J. Gabriel, Divisional Director, Active Living and Culture

Attachments:

1. Appendix A -List of events
2. Presentation - Events Overview

cc: Divisional Director, Active Living & Culture
Manager, Cultural Services
Manager, Sport & Event Development
Communications Advisor



Active Living & Culture
 Parkinson Rec Centre
 1800 Parkinson Way
 Kelowna, BC V1Y 4P9
 outdoorevents@kelowna.ca

2014 Outdoor Events Schedule

Event	Start Date	End Date	Location
Maple Fest	29-Mar-14	29-Mar-14	Rotary Arts Common + Art Walk
Okanagan College Half Marathon	06-Apr-14	06-Apr-14	Ok College on KLO Road & Roadways
Ski 2 Sea Race	13-Apr-14	13-Apr-14	Big White to City Park + roadways
Splash of Colour	19-Apr-14	19-Apr-14	City Park
Vaisahki Parade	26-Apr-14	26-Apr-14	Rutland Roadways
Day of Mourning	28-Apr-14	28-Apr-14	Ben Lee Park
<i>Spring Wine Festival</i>	<i>01-May-14</i>	<i>11-May-14</i>	<i>Various</i>
MS Walk	04-May-14	04-May-14	Stuart Park
Walk So Kids Can Talk	04-May-14	04-May-14	Waterfront Park & Promenade
Cherry Blossom Triathlon	04-May-14	04-May-14	Mission Recreation Park & Roads
Criterion/Time Trial	06-May-14	28-Aug-14	Tues - Old Vernon Rd- Time Trials Thurs-Jim Bailey Road-Crit
Glenmore Footprint Days	06-May-14	09-May-14	City Roadways + Rails with Trails
May Days Parade	15-May-14	18-May-14	Rutland Roadways and Centennial Park
Knox Mountain Hill Climb	17-May-14	18-May-14	Knox Mountain Park
KSS Grad March	16-May-14	16-May-14	Rhapsody Plaza & Promenade
MBSS Grad March	17-May-14	17-May-14	
George Eliot Grad March	18-May-14	18-May-14	
RSS Grad March	19-May-14	19-May-14	
Westie Walk	18-May-14	18-May-14	City Park - northfield
Hometown Bus Tour	19-May-14	19-May-14	City Park
Big Bike Ride	21-May-14	24-May-14	RCA Common & roadways
Play On! Hockey	24-May-14	25-May-14	WFP - Rhapsody Plaza & Water St Closure
T-Bird Show & Shine	24-May-14	24-May-14	City Park - North Field
Dog Agility Trials	25-May-14	25-May-14	City Park - North Field
Family Fun Day	25-May-14	25-May-14	Parkinson Rec Centre
Great Strides Walk	25-May-14	25-May-14	City Park - Jubilee Bowl
<i>Bike to Work Week</i>	26-May-14	01-Jun-14	<i>various</i>
<i>Bike to Work Celebration Station</i>	26-May-14	26-May-14	<i>RCA Common</i>
<i>Midsun Junior High Band Performance</i>	29-May-14	29-May-14	<i>Kerry Park - Stage</i>



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2014 Outdoor Events Schedule

Event	Start Date	End Date	Location
<i>Infertility Awareness Walk and Pram Push</i>	31-May-14	31-May-14	Stuart Park & Promenade
<i>Race for Kids</i>	31-May-14	31-May-14	City Park waterpark; PRC pickleball courts; Knox Mtn disc golf course
Relay for Life	31-May-14	1-Jun-14	City Park - North Field
<i>Namaste Yoga Day</i>	01-Jun-14	01-Jun-14	Stuart Park
Healthy Kids Day	1-Jun-14	1-Jun-14	Rutland Sportsfield & portion of Parking Lot
Fat Cat Children's Festival & Parade	6-Jun-14	7-Jun-14	Waterfront Park & Cawston Ave
Five Hole for Food Street Hockey	7-Jun-14	7-Jun-14	Stuart Park
Motorcycle Ride for Dad	8-Jun-14	8-Jun-14	Hwy 97 & Various DT Roadways
Walk to Fight Arthritis	08-Jun-14	08-Jun-14	Stuart Park + Promenade
Kelowna Global Day of Prayer	08-Jun-14	08-Jun-14	City Park - Jubilee Grandstands
5/10K Walk/Run for Women	8-Jun-14	8-Jun-14	SE Kelowna Diamonds and Roadways
Push to End Homelessness	14-Jun-14	14-Jun-14	Kerry Park
Walk For ALS	14-Jun-14	14-Jun-14	Rhapsody Plaza & Promenade
Father's Day Car Show	15-Jun-14	15-Jun-14	City Park (northfield)
National Aboriginal Day	21-Jun-14	21-Jun-14	Ellis St / Lawrence Ave Parking Lot and Alley Way behind the Ki-Low-Na Friendship Society
National Aboriginal Days Celebration (Métis)	21-Jun-14	21-Jun-14	Stuart Park
Variety Boat for Hope	21-Jun-14	21-Jun-14	Kerry Park
Yoga for Stroke	22-Jun-14	22-Jun-14	Stuart Park - Plaza
Ride Don't Hide Community Bike Ride	22-Jun-14	22-Jun-14	RCA Common & roadways
Celebrate Canada Day	1-Jul-14	1-Jul-14	WFP, Kerry Park, Stuart Park
Parks Alive	2-Jul-14	31-Aug-14	WFP, Kerry Park & various neighbourhood parks
UFC Experience	5-Jul-14	5-Jul-14	
Mid Summer 8K Run	6-Jul-14	6-Jul-14	Gyro Beach + Promenade + City Park
Kelowna Rugby Daze	10-Jul-14	12-Jul-14	City Park on July 10 Parkinson/Apple Bowl - Jul 11/12
Keloha Music & Arts Festival	11-Jul-14	13-Jul-14	Waterfront Park
Okanagan's Largest Yoga Class	13-Jul-14	13-Jul-14	Stuart Park
	17-Jul-14	20-Jul-14	
	24-Jul-14	27-Jul-14	
Kelowna Summer Theatre Festival			Waterfront Park - Island Stage



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2014 Outdoor Events Schedule

Event	Start Date	End Date	Location
Kelowna Summer Theatre Festival	31-Jul-14 7-Aug-14	3-Aug-14 10-Aug-14	Waterfront Park - Island Stage
Across the Lake Swim	19-Jul-14	19-Jul-14	City Park Hot Sands Beach & Northfield
Okanagan Caribbean Festival	19-Jul-14	19-Jul-14	City Park, Jubilee Grandstands
Okanagan VW Car Show	20-Jul-14	20-Jul-14	City Park, South Plaza, South Parking area and Sportsfield
TRi KiDS Triathlon	20-Jul-14	20-Jul-14	Mission Recreation Park
Kelowna Dragon Boat Festival	19-Jul-14	20-Jul-14	WFP Tugboat Beach & Concession Plaza
Center of Gravity	25-Jul-14	27-Jul-13	City Park - Jubilee Bowl, North Field, Hot Sands Beach, City Park Parking Lot
Snowbirds	30-Jul-14	30-Jul-14	Waterfront Park
Grizzly Cup Tour	3-Aug-14	3-Aug-14	City Park - northfield
Downtown Kelowna Block Party	9-Aug-14	9-Aug-14	Bernard Ave & Kerry Park
ALS Cycle of Hope	10-Aug-14	10-Aug-14	Groves Ave (Tutt-Pandosy) & roadways in Mission & SE Kel
Moonlight Movie	13-Aug-14	13-Aug-14	City Park - North Field
Kelowna Apple Triathlon	15-Aug-14	17-Aug-14	WFP / Strathcona - Kids Camp (Aug 11-16)
Okanagan Pride Picnic in the Park	16-Aug-14	16-Aug-14	City Park - Jubilee Grandstands & Basketball Courts
URBA Car Show	16-Aug-14	16-Aug-14	Rutland Lions Park
Step Your Game Up - 3 x 3	29-Aug-14	31-Aug-14	City Park - Jubilee Grandstands
Cracker Cross Series	4-Sep-14	30-Oct-13	Knox Mountain
Wine Country 1/2 Marathon	6-Sep-14	6-Sep-14	Waterfront Park + roadways
Parkinson SuperWalk	07-Sep-14	07-Sep-14	Waterfront Park
Paws For A Cause	7-Sep-14	7-Sep-14	City Park & Promenade
Spin for Strong Kids	13-Sep-14	13-Sep-14	H2O Adventure & Fitness Center, Parking Lot
Terry Fox Run	14-Sep-14	14-Sep-14	Mission Sportsfields
Canadian Police Canine Time Trials	14-Sep-14	14-Sep-14	Apple Bowl
Open Air Legal Clinic	16-Sep-14	16-Sep-14	City Park - Rose Garden
Take Back The Night March	18-Sep-14	18-Sep-14	Stuart Park & Downtown Sidewalks
Run in Colour Kelowna	20-Sep-14	20-Sep-14	City Park + parking lots
Kelowna Walk Now for Autism Speaks	21-Sep-14	21-Sep-14	Rhapsody Plaza & Promenade
Recovery Day	21-Sep-14	21-Sep-14	Stuart Park
Peak to Beak Run	21-Sep-14	21-Sep-14	Kelowna roadways
Canadian Student Leadership Conference	24-Sep-14	25-Sep-14	24th @ Gyro 26th @ Stuart Park
Walk to Remember	27-Sep-14	27-Sep-14	City Park - North Field
<i>Culture Days</i>	<i>26-Sep-14</i>	<i>28-Sep-14</i>	
<i>Okanagan Fall Wine Festival</i>	<i>3-Oct-14</i>	<i>12-Oct-14</i>	



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2014 Outdoor Events Schedule

Event	Start Date	End Date	Location
Kettle Mettle	4-Oct-14	4-Oct-14	Mission Recreation Park
Scarecrow Festival	5-Oct-14	5-Oct-14	Rutland Lions Park
Run for the Cure	5-Oct-14	5-Oct-14	City Park
Okanagan Marathon	11-Oct-14	12-Oct-14	City Park
School of Hard Knox	19-Oct-14	19-Oct-14	Knox Mtn Park
Remembrance Day Ceremony & Parade	11-Nov-14	11-Nov-14	Lions Park and City Roadways
Remembrance Day Ceremony & Parade	11-Nov-14	11-Nov-14	City Park - Veendam Garden
Hanukkah / Menorah Display & Lighting	17-Dec-14	24-Dec-14	Stuart Park
Downtown Light-Up	6-Dec-14	6-Dec-14	Kerry Park
Uptown Rutland Light-up	7-Dec-14	7-Dec-14	Roxby Park
New York New Year's Eve	31-Dec-14	31-Dec-14	Stuart Park

EVENTS OVERVIEW

2014



BACKGROUND

Outdoor Events office issues 80-100 permits each year

- ▶ Festivals
- ▶ Celebrations
- ▶ Parades
- ▶ Races
- ▶ Tournaments
- ▶ Fundraisers
- ▶ Awareness Events
- ▶ Weddings, churches, fitness permits & films

Event & Festivals Framework

Strategic
Direction

Core Event
Properties

Volunteer
Support

Regulatory
Facilitative

Resources

VOLUNTEER PROGRAM



Online marketplace

Tools & Resources

Training

Staff support

LONGEST RUNNING EVENTS



34
Years

Ski 2 Sea



32
Years

Apple Triathlon



56
Years

Knox Mtn Hill Climb



66
Years

Across the Lake Swim

LARGE PARTICIPATION NUMBERS



Father's Day Car Show
20,000 participants!

Canada Day
60,000 participants!



RACES

BMO Okanagan
Marathon



Wine Country Half
Marathon



FUNDRAISERS



Easter Seals - Splash Of Colour
KGH – Run in Colour



BDO Human Bonspiel

COMMUNITY CELEBRATIONS

New York
New Years



Scarecrow
Festival



Vaisakhi
Parade



NEW EVENTS



Cherry Blossom Triathlon
May 4, 2014



NEW EVENTS

Rugby 7's Daze
July 10-12, 2014



RUGBYDAZE.COM

ONE TIME EVENTS



Snowbirds
July 30



SKATECANADA


Teaching Canadians to skate since 1887.

Skate Canada
International
Oct 31 – Nov 2

Hometown Bus Tour
May 19



OPPORTUNITIES



Develop existing events
Procure established events
Strategic development of new homegrown events
Open and prepared for any opportunity

QUESTIONS?



Report to Council



Date: 5/12/2014
File: 1820-20
To: City Manager
From: Utilities Planning Manager
Subject: Vernon Creek Risk Assessment

Recommendation:

THAT Council receives, for information, the report from the Utilities Planning Manager dated May 12th, 2014 regarding the Vernon Creek risk assessment;

AND THAT Council direct staff to follow Option 1 as identified in the Utilities Planning Manager report, dated May 12th, 2014, regarding the Vernon Creek risk assessment;

AND FURTHER THAT the 2014 Financial Plan be amended by \$47,450 to provide for the creation of a design, operation and maintenance manual for the Vernon Creek Flume with funding from General Reserves.

Purpose:

The purpose of this report is to identify the current condition of the Vernon Creek flume and associated risks as directed by Council at the December 12, 2013 meeting, SR Request 278675. Further, this report recommends a course of action in order to manage these risks:

THAT staff report back to Council with the risk assessment for the "Vernon Creek Flume Repair" project

Background:

In the 1970s Vernon Creek was diverted from its natural path into a long concrete channel (flume) that was part of the original water diversion used by Hiram Walker Distillery. This channel was subsequently turned over the City and provides erosion and flood protection for the North Industrial Park. The City has undertaken a number of repairs in recent years to the concrete structure, installing concrete patches in the holes developing within the flume. New holes in the concrete structure seem to be occurring with increasing frequency, and size. In 2012/2013 a risk assessment was commissioned in order to determine the nature and risk the failures and prescribe the best method to fix the same. The consultant recommended a series

of options in order to fix the flume, and recommendations concerning risk and managing the maintenance of the flume.

The consultant identified portions of the flume are worn out and need to be replaced or repaired and are required to keep the City's asset in an acceptable condition. There is limited public risk at this time however there is some risk of flooding in the downstream portions of the flume where materials have deposited. The bottom of the flume is likely to completely disintegrate, and scour of the creek bed will continue. Materials deposited at the downstream portion of the flume should be removed and holes in the flume immediately downstream of the Dubna Road crossing should be filled and the concrete repaired. The recommend scope of the work is shown in Schedule A attached as Phase 1. While it is not in the text of the report, the consultant has recommended that the Phase 1 works should be completed within the next five years. The remainder of the flume can be replaced at future date with the possibility of the flume becoming naturalized. Future condition assessments will be required to determine when the end of the concrete's serviceable life has been reached in areas downstream of Dubna Road.

The options for the areas prescribed for replacement of the concrete flume upstream of Dubna Road can be summarized as follows:

Option 1: Rip-rap Lined Channel

The scope of the work involves: the removal of the existing concrete flume, the installation of a liner along the creek channel to prevent the migration of fines, and the placement of large rock integrated together to provide erosion control blanket on the channel banks, or rip-rap, see Schedule B. This liner can be designed to prevent the flow of water into the substrate should there be a need. There is limited width within the existing right of way available to restore the channel into a natural creek. Should more land be obtained in the future rip-rap could easily be peeled back to allow for a vegetative bench to be planted. Approximate Cost \$283,500

Staff have met with the Ministry of Environment to review options for the repair and replacement of the concrete flume. Option 1 is seen by the Provincial and Federal Environment and Fisheries groups as an environmental enhancement, and are supportive of the same. Staff have reviewed the ongoing maintenance requirements associated with managing a rip-rap structure with the Provincial and Federal Governments.

As part of the regular maintenance, trees will need to be occasionally removed from the rip-rap and the rip rap structure itself will need occasional work. While the capital cost of option 1 is more economical than replacing the concrete flume, the cost of obtaining environmental approvals and habitat compensation for regular ongoing maintenance of the rip-rap materials could be more costly. Further, should the vegetation removal deemed to be a Harmful Alteration Disruption and Destruction (HADD) of habitat by the Province or the Federal Government, then the maintenance of rip-rap structure may become untenable if this vegetation cannot be removed. The City would have a substantial liability should we construct Option 1, but not be able to provide maintenance on the same.

As an innovative means of ensuring that we are able to efficiently and cost effectively maintain the constructed works, staff have suggested to the Province and Federal governments that we tie the approval of the works and ongoing maintenance to a water license. In this scenario, the design and operations and maintenance manual will be created

and attached to a water diversion license. The method and environmental requirements for maintaining the works will be prescribed within the operations and maintenance manual.

Option 2: Concrete Filled Geoweb

The scope of the work involves: the removal of the existing concrete flume, the installation of geoweb and anchors, and the installation of a new concrete flume. See Schedule B. Approximate Cost \$302,700

The Provincial and Federal governments are not in favour of this option as it perpetuates the continuation of a structure within Vernon Creek that does not allow any vegetative growth and is not conducive to the passage of fish within the channel. It is not clear to the writer, whether or not the Provincial or Federal governments could prevent the City of Kelowna from constructing Option 2 should Council wish to pursue the same. This option will not enhance the environment, and staff does not believe this project would be eligible for grant funding at this time.

While a vegetated gabion channel was identified within the text of the report as potential option, staff does not believe that this is a viable solution. The gabion cages used to hold rock are made of galvanized steel. Typically these materials do not last very long in a wet environment and would pose a significant liability should the steel fail. For these reasons we have not presented this as a viable option.

Conclusion:

Staff is recommending Option 1 as the best solution for the City. This option improves the Environment and has the potential for grant funding as an environmental enhancement project. Our solicitor has reviewed the method of tying the environmental approvals to a water license and believes that this will provide a viable method of approval for the City.

Next Steps:

Should Council direct staff to pursue Option 1, the following steps are envisioned:

- The preparation of the design, operation and maintenance manual of the rip rap works. The design and the manual will include a hydrological, hydro-geotechnical and environmental considerations
- The application for a water license. This will include environmental approvals as part of this process. Should staff fail to obtain a water license within a reasonable period of time, then a progress report would be brought back to Council.
- It is not anticipated that the design, operations and maintenance manual and water license approval will be completed before 2015.
- Bring back any grant opportunities to Council for their consideration.
- Continue to monitor Vernon Creek to determine if the condition of the flume continues to deteriorate and pose an increased public risk.

Internal Circulation:

Director, Financial Services
Manager, Utilities Services
Manager, Grants and Partnerships
Communications Manager

Financial/Budgetary Considerations:

General Reserve funding of \$47,450 remains available from an earlier budget request to do a condition assessment on the Vernon Creek Flume. These funds are now requested to create the design and operations and maintenance manual needed to apply for a water license.

Considerations not applicable to this report:

Alternate Recommendation:

Communications Comments:

Existing Policy:

External Agency/Public Comments:

Legal/Statutory Authority:

Legal/Statutory Procedural Requirements:

Personnel Implications:

Submitted by:

A. Reeder, Utilities Planning Manager

Approved for inclusion:

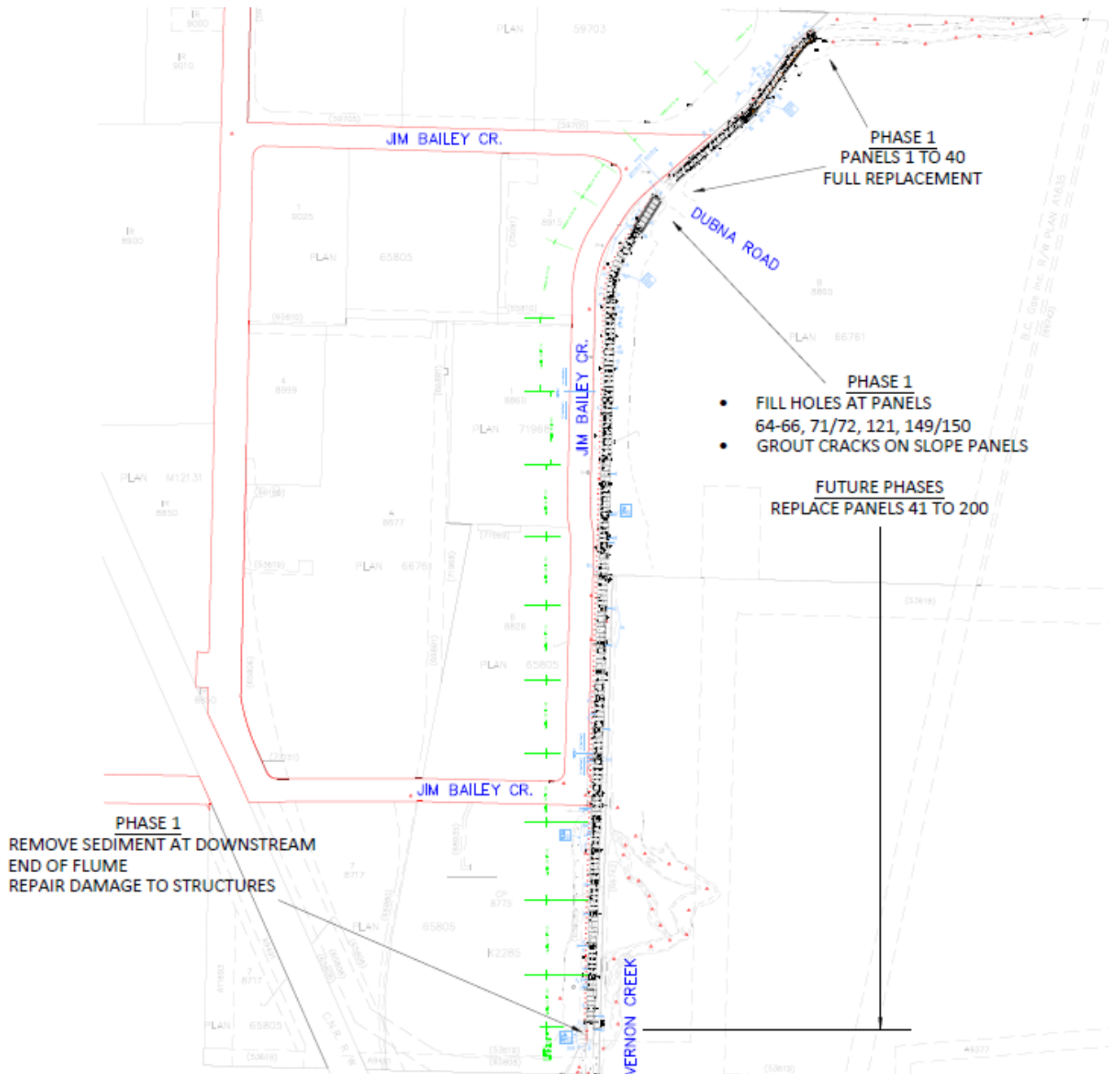


J. Vos, General Manager Infrastructure

Attachments: Schedule A -Scope of Work
Schedule B- Options

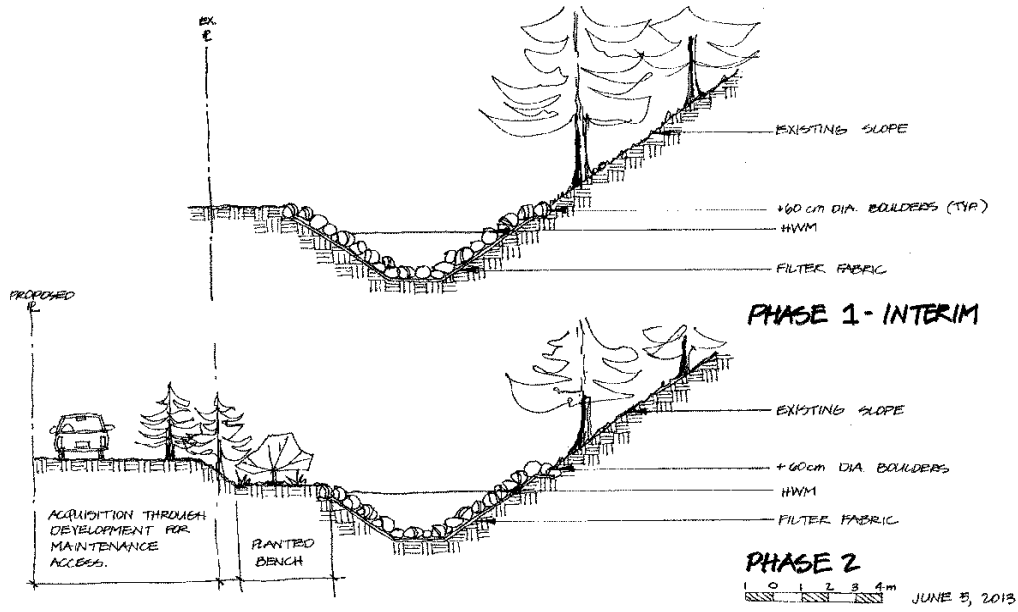
CC: Director, Financial Services
Manager, Utilities Services
Manager, Grants and Partnerships

Schedule A - Scope of Work

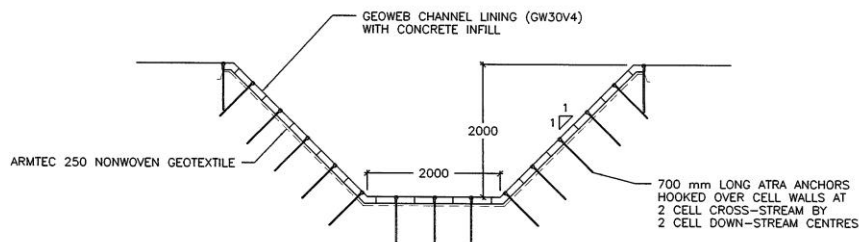


Schedule B- Options

Option 1 - Rip-rap Lined Channel



Option 2 - Concrete Filled Geoweb





City of
Kelowna

VERNON CREEK EROSION CONTROL



BACKGROUND

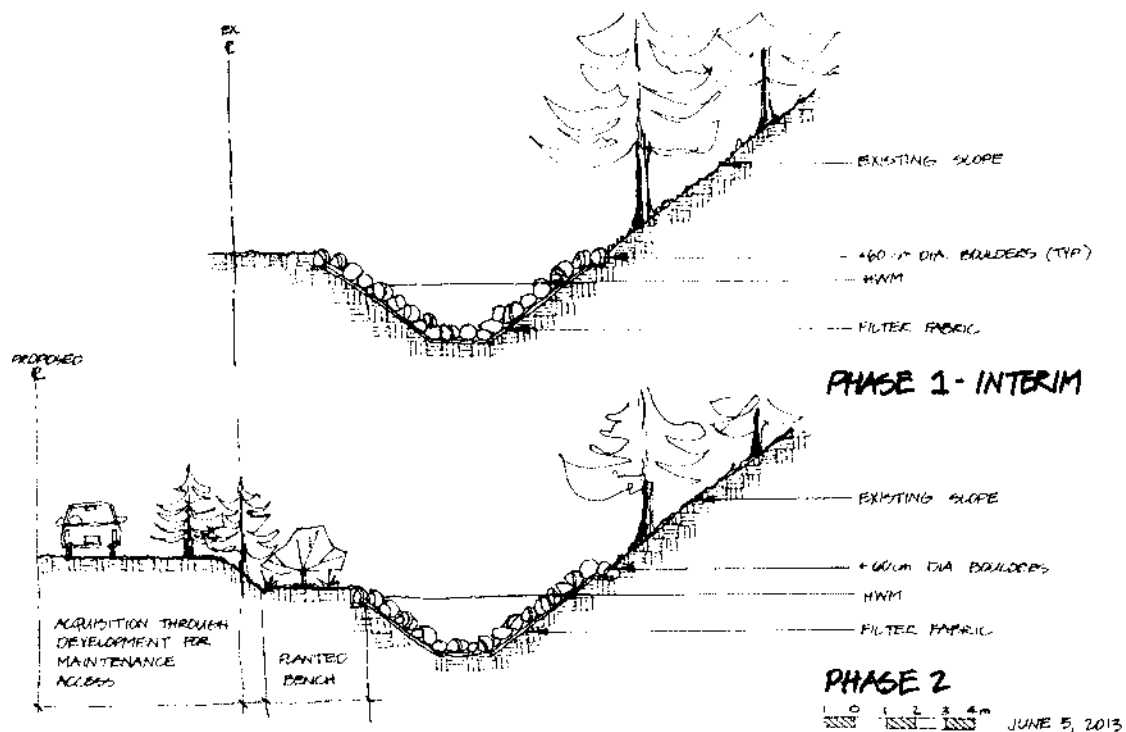
- The City has inherited a concrete flume used to change the path of Vernon Creek and to divert flows, once used for a distillery.
- Council have directed staff to undertake a risk assessment.
- The risk assessment is now complete, and a portion of the flume has reached the end of its serviceable life.
- Staff have been working with the Ministry of Environment to pursue an environmental solution.

BACKGROUND



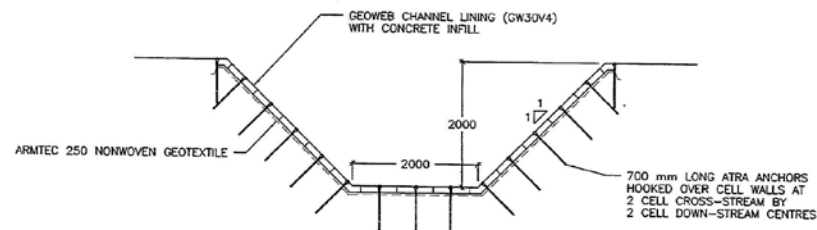
OPTION 1

- Remove existing concrete and replace with riprap and as more land becomes available create vegetative benching.
- Some form of erosion protection will be required because of the creek grade and limited channel width available



OPTION 2

- Repair the existing flume by anchoring a new concrete flume on to the existing channel



PRELIMINARY DESIGN

TIONS
 MEAN DISCHARGE - 57 cu.m/s
 SLOPE - 2%
 RINGS $n = 0.02$ (ROUGH RAISED CONCRETE)
 SPACE FRICTION ANGLE - 26°
 EDITION SOIL PROPERTIES:
 LE OF INTERNAL FRICTION - 30°
 c SOIL - 15 kN/m²
 ϕ WEIGHT - 18 kN/cu.m
 SHEAR RESISTANCE OF ANCHOR STAKES - 0.53 kN/STAKE
 INEL. SIDE SLOPES ARE ASSUMED TO BE INTERNALLY
 FRIC. (TO BE DETERMINED BY OTHERS)

ALL DIMENSIONS IN mm

NO.	DATE	DESCRIPTION OF REVISION	BY	CHKD
THIS DRAWING IS THE PROPERTY OF ARMTREC AND IS LOANED WITHOUT CONSIDERATION OTHER THAN THE BORROWER'S AGREEMENT THAT IT SHALL NOT BE REPRODUCED, COPIED, LENT OR IMPROVED, OR DIRECTLY OR INDIRECTLY, FOR ANY PURPOSE, OTHER THAN THAT FOR WHICH IT IS SPECIFICALLY PREPARED. THE BAKER IS LOANED SUBJECT TO RETURN ON DEMAND.				
WAL-MART				
PROPOSED GEOWEB DITCH LINING		DATE	Mar '03	
		DRAWN	JCW	
		CHECKED		
		REVIEWED		
		APPROVED		
		SCALE	NTS	
		DRAWING NO.	WALMART.DWG	REV.



ISSUES

- The City has an obligation/ liability to maintain any works that it constructs.
- Option 1 is a more environmental solution and will improve habitat, but will require more routine management. Some of this management includes the routine remove of trees in the rip rap.
- Any removal of habitat could constitute a HADD and make the maintenance of the rip rap unsustainable.

FUNDING

- The City has existing reserve \$47K for a risk assessment of the flume. Staff are recommending that we use these funds to create a design and operations and maintenance manual that will allow us to build option 1.
- Option 1 is an environmental enhancement to Vernon Creek that also has flood protection benefits. Staff believe that this project is well suited to receive grants directly to the City, or through a NGO partnership.

NEXT STEPS

1. Create a design that includes hydrological (1:200 conveyance, erosion control), hydro - geotechnical (review need for a liner), Environmental (current and future environmental improvements)
2. Create an Operation & Maintenance manual. Needs to meet the hydrological objectives and follow environmental best practices.
3. Create a water license that ties the design and the O&M manual to the license. This gives us the ability to maintain the works sustainably.
4. Apply for grants/ Review the condition of the flume while waiting for grants
5. Construct the works

TIMING

1. The design, operations and maintenance manual and water license will likely take until late 2015 to complete. The water license process is very slow.
2. We don't know when the next grant program will be available. Staff will review the condition of the flume periodically to determine if the risk level has changed significantly.

Report to Council



Date: 05/06/2014
File: 1862-01
To: City Manager
From: D. Duncan, Manager, Parking Services
Subject: 2014-05-06 Report - Small Shop Promotion Days

Recommendation:

THAT Council receives, for information, the report from the Manager, Parking Services dated May 6, 2014, with respect to free downtown parking on two (2) dates in 2014;

AND FURTHER THAT Council approves free on-street parking in the downtown area on the following two (2) dates in 2014: Thursday June 19th, commencing at 2:00 p.m. and Saturday October 25th, all day.

Purpose:

To obtain approval from Council to provide free parking in the downtown area on two (2) dates in 2014.

Background:

In 2013, council approved free on-street parking on three (3) Saturdays in support of the "Small Shop Saturday" initiative, at the request of the Downtown Kelowna Association (DKA). Off-street parking in City-owned parking lots downtown is already free on Saturdays, with the exception of the Water Street Boat Launch Lot.

The DKA has requested the City's approval for complimentary downtown on-street parking again this year.

External Agency/Public Comments:

Letter received from the Executive Director of the Downtown Kelowna Association is attached to this report.

Communications Comments:

Press release will be issued to notify the public

Considerations not applicable to this report:

Internal Circulation:

Legal/Statutory Authority:

Legal/Statutory Procedural Requirements:

Existing Policy:

Financial/Budgetary Considerations:

Personnel Implications:

Alternate Recommendation:

Submitted by: D. Duncan, Manager, Parking Services

Approved for inclusion: D. Edstrom, Director, Real Estate

Attachment:

1. Letter from DKA

cc: T. Wilson, Corporate Communications Supervisor
K. Kay, Communications Advisor

DOWNTOWN KELOWNA

April 7, 2014

Dave Duncan
City of Kelowna
435 Water Street
Kelowna, BC V1Y 1J4

Re: Free Parking for 2014 Small Shop promotion days in Downtown Kelowna

Dear Dave Duncan,

Please accept this letter as our formal request to offer free parking in Downtown Kelowna to our visitors in conjunction with Downtown Kelowna's Small Shop promotion days.

Downtown Kelowna recognizes that small businesses play a vital role in Kelowna's local economy by providing jobs and preserving the Downtown neighborhood. As a result, the Downtown Kelowna Association will be organizing two (2) "Downtown Kelowna Small Shop" promotions in 2014 to promote shopping local, supporting small businesses and the local economy, as well as raise awareness for Downtown Kelowna.

We would like to request complimentary Downtown Kelowna on-street parking on Thursday, June 19, 2014 for Small Shop Evening (commencing at 2:00pm) and one Saturday in October 25, 2014 for Small Shop Saturday (all day). This is an important value added initiative for our Downtown retailers, merchants and restaurants during this campaign. We would appreciate the City's support and approval on this very important program.

Please do not hesitate to call if you have any questions or concerns. Thank you in advance for your time and attention to this request.

Sincerely,



Peggy Athans EXECUTIVE DIRECTOR

Report to Council



Date: 5/12/2014
File: 0705-25
To: City Manager
From: Danielle Noble-Brandt, Department Manager, Policy and Planning
Subject: Regional Growth Strategy Bylaw 1336
Report Prepared by: Gary Stephen, Long Range Planning Manager

Recommendation:

THAT Council receives the report on the RDCO Regional Growth Strategy Bylaw 1336 from the Department Manager, Policy and Planning dated May 12, 2014;

AND THAT Council accepts the Regional Growth Strategy Bylaw 1336 as outlined in the report dated May 12, 2014.

Purpose:

To provide the Regional District of Central Okanagan the City of Kelowna response to the referral of the Regional Growth Strategy Bylaw.

Background:

The Regional District of Central Okanagan (RDCO) has been working on the update of the Regional Growth Strategy (RGS) since early 2010, as the existing version was adopted in 2000. As per the *Local Government Act*, the "purpose of a RGS is to promote human settlement that is socially, economically, and environmentally healthy and that makes efficient use of public facilities and services, land and other resources".

The previous draft of the RGS Bylaw was given second reading by the RDCO on October 28, 2013 and referred to member municipalities at that time. The City of Kelowna had some concerns with the RGS policy wording and subsequently, the RDCO rescinded second reading to allow continuing dialogue with member municipalities in January 2014. As a result of that dialogue a number of amendments were made to the document that satisfied the municipal staff and CAO's.

At the Regular Meeting of April 7, 2014 Council passed the following resolutions:

THAT Council receive for information the report from the Department Manager, Policy and Planning dated April 7, 2014 on the status of the Regional Growth Strategy Bylaw process;

AND THAT the Regional District be advised that it would be appropriate to move forward with second reading and the formal referral of the Regional Growth Strategy Bylaw 1336.

The Regional Board gave the updated RGS Bylaw second reading on April 28, 2014 and formally referred Bylaw No. 1336 to the City of Kelowna for consideration and acceptance on April 30, 2014 (RDCO submission letter is provided in Attachment 1). The City of Kelowna has up to 60 days from the receipt of the submission letter to respond, with a deadline of June 27, 2014. Following the adoption of the RGS, the City will have up to two (2) years to update the Regional Context Statement in our OCP to indicate how the OCP reflects and implements the RGS. That Regional Context Statement must be submitted to the Regional Board for their review.

It would now be in order to advise the Regional District of Central Okanagan that the City of Kelowna accepts the Regional Growth Strategy Bylaw No. 1336.

Internal Circulation:

Divisional Director, Community Planning and Real Estate
Infrastructure Division Director
Utilities Planning Manager
Transportation and Mobility Manager
Director Regional Services / Strategic Services
Executive Director of Business Development
Manager - Subdivision, Agriculture & Environment Services
Sustainability Coordinator

Legal/Statutory Authority:

Local Government Act, Part 25, Division 2 - Section 857

Legal/Statutory Procedural Requirements:

Local Government Act -Section 857(4) states that:

- (4) After receiving a proposed regional growth strategy under subsection (3), each affected local government must:
 - a) review the regional growth strategy in the context of any official community plans and regional growth strategies for its jurisdiction, both those that are current and those in preparation, and in the context of any other matters that affect its jurisdiction, and

- b) subject to an extension under section 858 (3), within 60 days of receipt either
 - i) accept the regional growth strategy, or
 - ii) respond, by resolution, to the proposing board indicating that the local government refuses to accept the regional growth strategy.
- (5) An acceptance under subsection (4) (b) becomes effective
 - (a) when all affected local governments have accepted the regional growth strategy, or
 - (b) at the end of the period for acceptance or refusal under that subsection if, at the end of that period, all affected local governments have not accepted the regional growth strategy.
- (6) If an affected local government fails to act under subsection (4) (b) within the period for acceptance or refusal, the local government is deemed to have accepted the regional growth strategy.
- (7) In the resolution under subsection (4) (b) (ii), the affected local government must indicate
 - (a) each provision to which it objects,
 - (b) the reasons for its objection, and
 - (c) whether it is willing that a provision to which it objects be included in the regional growth strategy on the basis that the provision will not apply to its jurisdiction, as referred to in section 853 (2).
- (7.1) An affected local government is deemed to have accepted any provision of the regional growth strategy to which it does not indicate an objection under subsection (7).

Existing Policy:

Official Community Plan Bylaw No. 10500 (2011)
Regional Growth Management Strategy Bylaw No. 851 (2000)

Considerations not applicable to this report:

Financial/Budgetary Considerations:
Personnel Implications:
External Agency/Public Comments:
Communications Comments:
Alternate Recommendation:

Submitted by:

Gary Stephen, Long Range Planning Manager

Approved for inclusion

Danielle Noble-Brandt
Department Manager - Policy & Planning



Attachments:

1. 2014-04-30 RDCO RGS Bylaw Referral Letter
2. 2014-04-28 RGS Bylaw - Schedule A (Final)
3. 2014-01-27 Map A-1 Political Boundaries
4. 2014-01-27 Map A-2 Extent of Private Crown Land
5. 2014-01-27 Map A-3 Provincial Regional Municipal Parks
6. 2014-01-27 Map A-4 RGS Constraint Areas 2012 OCP
7. 2014-01-27 Map A-5 RGS Land Inventory 2012 OCP
8. 2014-01-27 Map A-6 ALR Map
9. 2014-01-27 Map A-7 Biodiversity Strategy - Relative Biodiversity
10. 2014-01-27 Map A-8 Biodiversity Strategy - Conservation Ranking
11. 2014-01-27 Map A-9 Biodiversity Strategy - Habitat Connectivity

cc: Divisional Director, Community Planning and Real Estate



CITY OF KELOWNA

MAY - 1 2014

ADMINISTRATION
DEPARTMENT

Community Services
Planning Section

1450 K.L.O. Road
Kelowna, B.C. V1W 3Z4

Telephone: (250) 469-6227
Fax: (250) 762-7011
www.regionaldistrict.com

Danielle
RD CO ??

April 30, 2014

Mayor and Council
c/o Ron Mattiussi
City Manager
City of Kelowna
1435 Water Street,
Kelowna, BC V1Y 1J4

Original to: ~~Communications~~
Copied: ~~Mayor~~
~~City Manager~~
Councillors

File Number: 0705-20
Initials: *Jim* Date: *May 1/14*

Dear Mayor and Council:

Re: Submission of the Central Okanagan Regional Growth Strategy Bylaw No. 1336, 2013 for Acceptance by Affected Local Governments

The Central Okanagan Regional Growth Strategy Bylaw No. 1336, 2013 received Second reading at the Regional Board meeting on April 28, 2014. Pursuant to the *Local Government Act, Section 857*, the Bylaw is being forwarded to your local government for consideration and acceptance.

Under the provisions of the *Local Government Act*, affected local governments, which include member municipalities and adjacent Regional Districts, have **60 days** from the receipt of this letter in **which to consider acceptance of Bylaw No. 1336, 2013**. At the conclusion of the 60 day period, Regional District staff will report to the Regional Board on the status of acceptance, and if appropriate, present Bylaw No. 1336, 2013 for Third reading and Adoption.

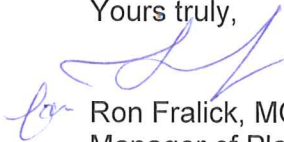
Each affected local government is requested to respond to the Regional Growth Strategy Bylaw No. 1336, 2013 by resolution. For information, *Section 857 (4)* of the *Local Government Act* states that upon receipt of the Regional Growth Strategy Bylaw No. 1336, 2013, each affected local government must:

- a) Review the Central Okanagan Regional Growth Strategy Bylaw No. 1336, 2013 in the context of any Official Community Plans in its jurisdiction, both those that are current and those in preparation, and in the context of any other matters that affect its jurisdiction; and
- b) Within 60 days of receipt of the referral, affected local governments are required to, by resolution, either:
 - (i) Accept Bylaw No. 1336, 2013; or
 - (ii) Respond to the Regional District of Central Okanagan Regional Board indicating specifically the sections or policies of Bylaw No. 1336, 2013 that your local government will not accept.

If a resolution is not brought forward at the end of the 60 day acceptance period, then according to *Section 857* of the *Local Government Act*, an affected local government is deemed to have accepted the Regional Growth Strategy.

If you have any questions regarding the Regional Growth Strategy process, the formal acceptance process or any matters associated with the Regional Growth Strategy Bylaw No. 1336, 2013, please contact the undersigned at (250) 469-6227 or ron.fralick@cord.bc.ca.

Yours truly,


Ron Fralick, MCIP
Manager of Planning

Enclosures: RDCO Resolution, April 28, 2014
Regional Growth Strategy Bylaw No. 1336, 2013

Cc Robert Hobson, Chair, Central Okanagan Regional Board
Brian Reardon, Chief Administrative Officer, RDCO
Chris Radford, Director of Community Services, RDCO
Meggin Messenger, Ministry of Community, Sport and Culture
Brent Mueller, Ministry of Community, Sport and Culture

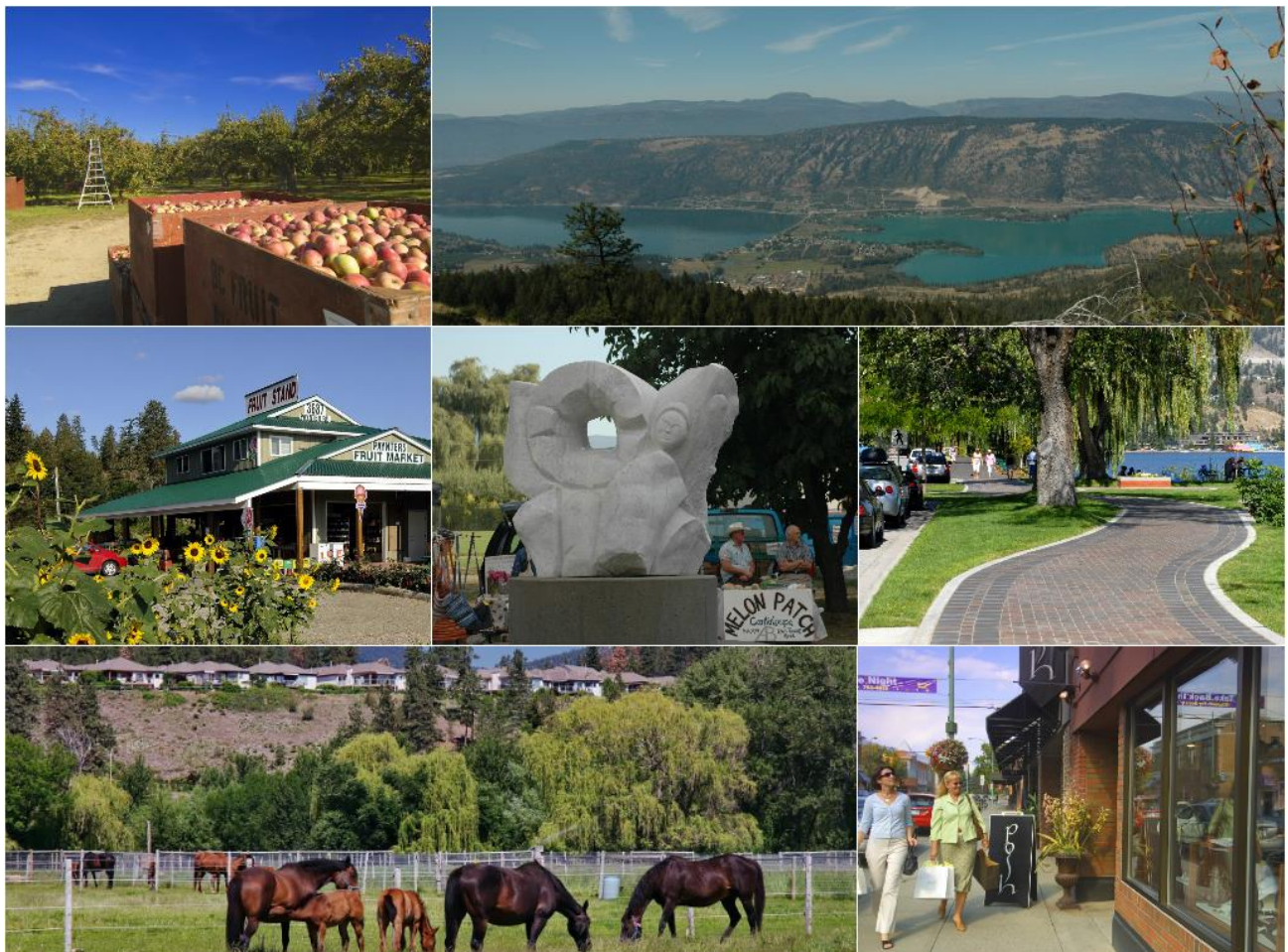
Regional District of Central Okanagan Regional Growth Strategy

"Our Home, Our Future"



Schedule 'A'

Bylaw No.1336, 2013



Source: Pictures BC

Acknowledgements

The Regional District of Central Okanagan (RDCO) recognizes and acknowledges the complex planning environment that exists within the Central Okanagan and respected these planning processes as the Region updated the Central Okanagan Regional Growth Strategy. The Regional District of Central Okanagan thanks the planning staff, committee and working group members, member municipal councils and Board of Directors, consultants, all residents, and those individuals who dedicated their time and expertise to make the project possible.



Graphic Artist: Stina Brown

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Source: Regional District of Central Okanagan

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Source: Regional District of Central Okanagan

1.0 INTRODUCTION

1.1 Regional Growth Strategy

The Regional Growth Strategy (RGS) is a long-range planning tool to help regional districts and local governments plan a coordinated future for their communities while dealing with regional issues and decisions that cross local political boundaries. The strategy is a collective vision from the regional partners for how they see the future in order to create a region that promotes growth that is economically, environmentally and socially healthy over a twenty (20) year time horizon.

There are three criteria for the determination of regional issues to be addressed through the RGS Review and Update process:

- **Mandatory content:** Provincial legislation requires that the following issues be addressed:
 - housing;
 - transportation;
 - regional district services;
 - parks and natural areas;
 - economic development; and,
 - reduction of greenhouse gas (GHG) emissions.
- **Regional Board, Intergovernmental Agency Committee (IAC) and RGS Steering Committee direction:** In addition to the mandatory content, the Regional Board and RGS Steering Committee helped to identify important regional issues as they relate to the local governments.

- **Cross boundary issues:** Focus on issues that cannot be addressed by one local government because the issue affects more than one local government (and in some cases, cross regional district boundaries). For example, air quality, environmental protection, watershed management and economic development are issues that transcend municipal boundaries.

1.2 Structure of the RGS

Joint Planning Approach and Agreement Among all Parties:

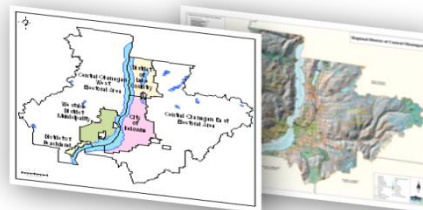
The RGS represents a joint-planning approach to addressing the growth issues that go beyond local government boundaries. This RGS update was a collaboration from the member municipalities, First Nations and Provincial agencies that utilized the consultations and research, as well the updated Official Community Plans to provide a directional document that allows each member municipality the ability to choose how they will implement the future direction into their land use decisions. Many agencies and organizations contributed their expertise to this RGS update including, but not limiting to, the School District No. 23, Interior Health and the Agricultural Land Commission.

The strategy is an agreement among governments and agencies to work together on common issues to find common solutions. The strategy reflects a shared regional vision for the future, as well as goals and policies necessary to achieve effective growth management.



Policy Statements

+



Series of Maps

= **RGS**

The RGS content:

Section 1 Introduction

Section 2 Regional Context: A profile of the RDCO is provided as a baseline. This provides both the qualitative and quantitative groundwork for the review of the RGS.

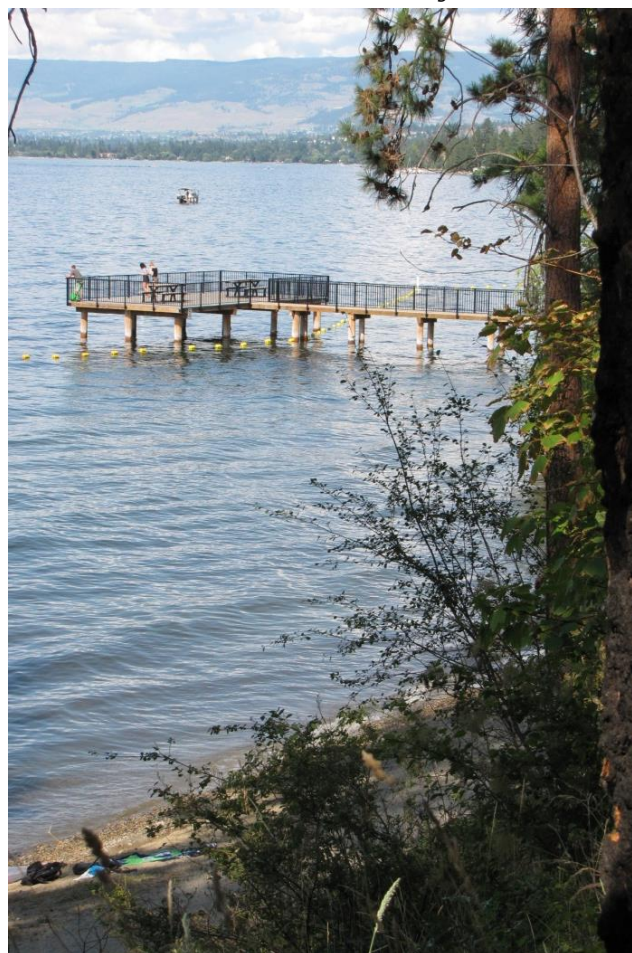
Section 3 Regional Growth Strategy: Ten issue areas form the policy structure and each issue area includes:

- **Goal** - Overall long-term goal of the issue area that reflects the input and responds to the Region’s future opportunities and challenges around growth management;
- **Synopsis** - Summarizes the issue area within the regional context based on the background research; and,
- **Policies** - The statements that help achieve the issue area goal.

Section 4 Monitoring and Evaluation: The essential responsibility associated with the implementation priorities, and timing, monitoring and accountability are documented.

Appendices Technical Appendices: provides reference material to inform and supplement the RGS Review and Update. Information that is more detailed can be obtained from the RDCO Community Services Department.

Source: Regional District of Central Okanagan



2.0 CONTEXT

2.1 Central Okanagan Directive

A regional district's primary role is determined by its member jurisdictions. Regional districts act only in response to the expressed needs, interests and instructions of their members and address issues that cross political boundaries. Regional districts serve three primary roles with the relative importance of each role varying from region to region:

- To serve as the local government for their unincorporated (electoral) areas;
- To provide services to, and on behalf of, different combinations of member municipalities and electoral areas; and,
- To provide services to, and on behalf of, their entire regions (i.e., all member jurisdictions).

The RDCO provides services to residents in the Electoral Areas, including water, planning and solid waste collection. The District also provides region

wide services such as dog control, solid waste collection and recycling, 9-1-1, Regional Parks and the Emergency Response Plan for the Central Okanagan. The illustration in Figure 2.1 shows the RDCO's departments and responsibilities.

In 2010, the Regional Board adopted the Regional District's Strategic Plan that sets priorities for the Region. The Strategic Plan's intentions are for the Regional District to focus energies and resources on:

- Establishing and promoting a vision and priorities for the broader, regional community;
- Developing regional plans and strategies to address joint issues; and,
- Coordinating municipal efforts that are taken to achieve joint services goals.

Figure 2.1: RDCO Services



The Mission Statement created for the Strategic Plan provides overall guidance to the Regional District and is as follows:

“The Regional District of Central Okanagan will provide effective and efficient services that meet the needs of our citizens, in a manner that nurtures growth, opportunities and prosperity, while maintaining and enhancing the unique Central Okanagan lifestyle and environment.”

The Board identified priority areas to pursue in the 2010 Strategic Plan. The term “priority area” was used to define a theme, topic or area of service that the Board viewed as important for the Region and that would best be addressed on a regional level. In 2012, the Board reviewed priority areas of the Strategic Plan and endorsed the following areas:

1. Growth Management;
2. Transportation;
3. Intergovernmental Affairs; and,
4. Hazard Management.

These priority areas provide the general directions for the Region, while more defined roles and tasks are defined through budgets, work plans and agreements.

2.2 Regional Overview

The RDCO is situated on both sides of the mid-section of Okanagan Lake. The Okanagan is characterized by cool humid air and cloudy skies in the winter and by dry air and bright skies in summer. The warm summers with fairly low humidity, as well as the relatively mild winters provide an attractive environment for agriculture and recreation.

Since the adoption of the initial RGS in the year 2000, there have been a number of changes in the Central Okanagan, some highlights include:

- Steady growth in population, housing, and employment (population grew by 20%);
- Opening of UBC Okanagan in 2005;
- Incorporation of the District of West Kelowna on December 6, 2007;
- Expansion of Kelowna International Airport and improvements along the Highway 97 Corridor;
- Replacement of the three-lane Okanagan Lake Bridge with the five-lane W.R. Bennett Bridge in 2008;
- Increased efficiency and expansion of public transit;
- Protection of key recreation and conservation lands (e.g. Mission Creek Greenway);
- Expansion of Kelowna General Hospital;



Source: Regional District of Central Okanagan

- Increased density of settlement areas, bringing about efficiencies in servicing and infrastructure costs; and
- Endorsement of a regional growth vision and implementation through Regional Context Statements in the municipalities’ community plans.

2.2.1 DEMOGRAPHICS

The total population for the Central Okanagan from the 2011 Census was 179,839. Population growth in the Central Okanagan has historically outpaced growth in the province overall. This trend continued in the period between Census counts, where it outpaced the provincial average by 4% over the 5-year period from 2006 to 2011 (Table 2.1, on page 6). Additionally, over the census period the Region showed the fourth highest population growth in the country.

Population growth in the Region has been primarily due to in-migration (Table 2.2, on page 6). High inflows of migrants have resulted in the population of the Region doubling in the past 25 years from 89,730 in 1986 to 179,839 in 2011. The Region was one of the highest growth areas in the province through the 1990s. The past several years have seen slower, but steady growth. The average rate of population growth from 2000 to 2011 has been 2.1% per annum compared to 4.0% per annum in the 1990s.

Intra-provincial migration has historically been the largest segment of migration to the Central Okanagan. However, interprovincial migration has been an increasingly significant migration component over the past eight years. The Region has experienced a smaller but steady flow of international migration over the past seven years as well.

The RDCO has experienced a decline in the net natural increase between 2001 and 2006, however in recent years the trend is starting to increase as shown in Table 2.3, on page 6. The Region is seeing a healthier, active senior which is translating to an increasing life expectancy. There has also been an unprecedented increase in births. Projections predict that the net natural increase will be a positive number over the next 10 years. The net natural increase could accelerate over the next 20 years as the majority of the 65 and over age category will reach the age of 85.

The Central Okanagan has been older than the provincial population throughout the last twenty years. Table 2.4 (on page 7) highlights the fact that the proportion of the population aged 45 years and over increased significantly from 1996 to 2006 by 43.3%¹. This increase can be partly attributed to the area's large retirement base. The Region is a popular retirement centre, and the increase in the proportion of retirees in the population will support more service sector employment. Over the long term, the Region is expected to continue to receive strong net inflows of population. The age group 15 to 24 showed an increase that could be influenced by the creation of the UBC Okanagan campus in 2005, which had a starting population of 2,800 students. UBC-O Enrolment in 2010 was 7,075².

Population projection numbers for the Region are expected to exceed 270,000 by 2036, an increase of 45% from 2011 (Table 2.5, on page 7). The net population growth projection from 2011 to 2036

indicates that growth in the Region will result from positive net migration.



Source: Regional District of Central Okanagan



Source: District of West Kelowna

1 Source: Statistics Canada, Census 1991, 1996, 2001 and 2006

2 Source: UBCO Webpage, Facts and Figures, accessed March 7, 2011

Table 2.1 Population Estimates, Census Years Source: BC Stats, Census

	1996	2001	2006	2011	% Change 2006-11	% Change 2001-11
British Columbia	3,724,500	3,907,738	4,113,487	4,400,057	7.0%	12.6%
Central Okanagan	136,537	147,748	161,959	179,839	11.0%	21.7%
Kelowna	89,445	96,290	106,707	117,312	9.9%	21.8%
West Kelowna**			27,095	30,892	14.0%	**
Lake Country	9,005	9,270	9,610	11,708	21.8%	26.3%
Peachland	4,525	4,660	4,885	5,200	6.4%	11.6%
Electoral Areas**	26,347	29,673	5,527	5,742	3.9%	-80.6%
First Nations (including IR7, IR9 and IR 10)	7,215	7,855	8,135	8,985	10.4%	14.4%

Source: BC Stats, Census

Table 2.2: Central Okanagan Migration Components of Population Growth 1996 to 2011

Year	International (Net)	Interprovincial (Net)	Intra-Provincial (Net)	Total Net Migration
1996-97	335	719	1,818	2,872
1997-98	258	-179	2,240	2,319
1998-99	72	-271	2,177	1,978
1999-00	128	-291	1,668	1,505
2000-01	134	-182	1,593	1,545
2001-02	119	-28	2,063	2,154
2002-03	-136	829	2,245	2,938
2003-04	322	1,280	1,487	3,089
2004-05	400	1,210	1,249	2,859
2005-06	422	1,547	1,810	3,779
2006-07	408	2,201	1,184	3,793
2007-08	654	2,202	1,093	3,949
2008-09	717	1,214	1,040	2,971
2009-10	548	1,234	1,049	2,831
2010-11	312	804	1,049	2,165

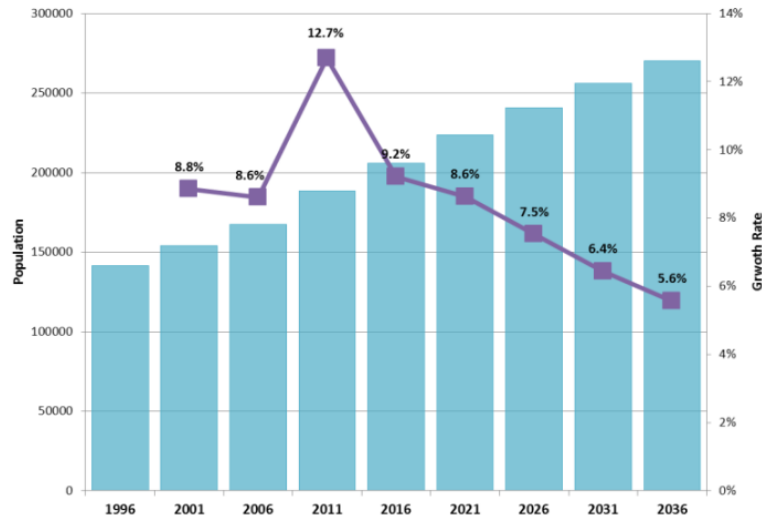
Source: BC Stats - <http://www.bcstats.gov.bc.ca/StatisticsBySubject/Demography/Mobility.aspx>

Table 2.3: Central Okanagan Natural Increase Components of Population Growth 1996 to 2011,

Year	Births	Deaths	Net Natural Increase
1996-97	1,433	1,065	368
1997-98	1,380	1,166	214
1998-99	1,348	1,147	201
1999-00	1,414	1,158	256
2000-01	1,344	1,171	173
2001-02	1,294	1,328	-34
2002-03	1,322	1,335	-13
2003-04	1,355	1,456	-101
2004-05	1,346	1,417	-71
2005-06	1,427	1,448	-21
2006-07	1,533	1,464	69
2007-08	1,577	1,526	51
2008-09	1,654	1,511	143
2009-10	1,732	1,513	219
2010-11	1,637	1,534	103

Source: BC Stats

Figure 2.2: RDCO Population Projection, BC Stats



Source: BC Stats

Table 2.4: Population Estimates for Age Groups, Census Years, BC Stats and Census

Age Groups	Census Years				Percent Change		
	1996	2001	2006	2011	2001-2006	2006-2011	2001-2011
0 - 4	8,015	7,145	7,190	8,305	0.6%	15.5%	16.2%
5 - 9	9,040	8,805	8,330	8,520	-5.4%	2.3%	-3.2%
10 - 14	9,400	9,940	10,000	9,525	0.6%	-4.7%	-4.2%
15 - 19	8,390	9,950	10,760	11,015	8.1%	2.4%	10.7%
20 - 24	7,985	8,070	10,050	11,670	24.5%	16.1%	44.6%
25 - 34	18,330	16,210	16,805	21,055	3.7%	25.3%	29.9%
35 - 44	21,790	23,160	22,340	21,765	-3.5%	-2.6%	-6.0%
45 - 54	16,880	21,390	25,355	28,140	18.5%	11.0%	31.6%
55 - 64	13,370	15,760	20,595	25,400	30.7%	23.3%	61.2%
65+	23,355	27,295	30,840	34,450	13.0%	11.7%	26.2%
Central Okanagan	136,540	147,740	162,280	179,840	9.8%	10.8%	21.7%

Source: BC Stats

Table 2.5: Population Projections for Age Groups, BC Stats

Year	Age Groups							Total
	Under 5	5-14	15-24	25-34	35-49	50-64	65 plus	
2011	8,492	17,900	24,847	23,558	37,949	41,142	33,346	187,234
2016	9,825	19,541	22,212	29,263	39,116	44,912	41,158	206,027
2021	10,591	22,093	21,591	29,388	45,557	46,233	48,352	223,805
2026	10,773	24,077	23,125	27,396	53,214	45,816	56,237	240,638
2031	10,725	24,980	25,646	26,985	57,158	47,056	63,576	256,126
2036	10,954	25,140	27,653	28,580	56,466	53,572	68,028	270,393
% change 2011 to 2036	29.0%	40.4%	11.3%	21.3%	48.8%	30.2%	104.0%	44.4%

2.2.2 EMPLOYMENT PROJECTIONS

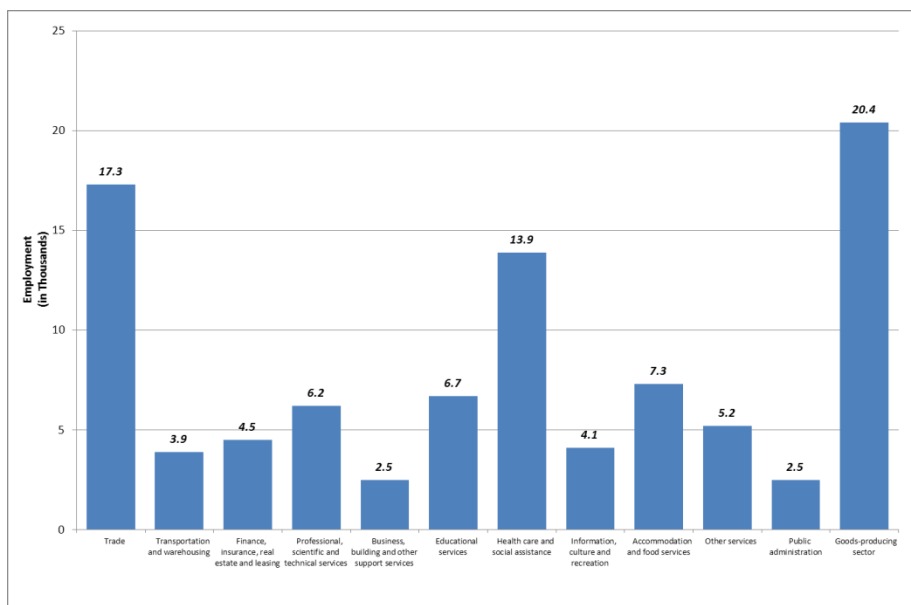
The Central Okanagan’s economic diversity has been its strength in the past and its future will depend on continuing to enhance that diversity (Figure 2.2). The traditional employment generators will still be important in the future, but they will be complemented by the growth of other industries such as health care, education, research and development, tourism, aerospace and high tech sectors. Most job creation is coming from smaller business employers. The trend towards smaller businesses may lead to more interest in the clean, high tech/business parks, office complexes and even home occupations; the latter of which has seen significant growth in the Central Okanagan in recent years.

Although unemployment has risen slightly in recent years, the private sector identifies a concern that succession planning, or the need for filling the void in certain occupations, will be felt more dramatically as the current workforce ages. There is a projection of a reduced labour supply within the 25 – 49 age group. The trend is one that is being faced across the country. However, this region may experience a greater impact due to the attraction of retirees to this area. It will be important to attract and retain youth or young skilled labour and professionals graduating from local educational institutions by employers in both public and private sectors. This will require the development of strategies amongst member municipalities and various partners to create communities and employment opportunities that are attractive for the long term. One example of a program in action



Source: Regional District of Central Okanagan targeting the younger labour force is the work of the Okanagan Young Professionals program. The EDC helps support the program to help attract, assist and retain talented professionals in their 20’s and 30’s to the Okanagan Valley as this works toward filling the employment opportunities in the Region.

Figure 2.3: Employment in the Central Okanagan by Industry, Census 2011



3.0 REGIONAL GROWTH STRATEGY

3.1 Our Regional Vision

In order to protect quality of life, the Region is committed to working together in creating and supporting economic opportunities, addressing issues of environmental quality and planning for future services such as transportation and water systems. The RGS Vision Statement describes the ideal outcome for the Region. This vision sets out direction for the management of future growth with subsequent policies and actions for implementation.

"The Central Okanagan is a region of urban and rural communities that are interconnected, distinct, healthy, vibrant and welcoming. The citizens, businesses, First Nations Councils and local governments understand and accept that they are individually and jointly responsible to effectively and efficiently manage the Region's future growth that ensures the health and well-being of its residents. Together and from this time forward, the citizens and governments of the Central Okanagan will work in partnership to promote a complete healthy region with a sustainable and diversified economy that provides a range of economic opportunities while protecting the natural environment and water resources for today's and tomorrow's residents."

3.2 Regional Issue Areas

Many of the regional issues of the RGS require cross jurisdictional collaboration in order to plan for the long term growth in the Central Okanagan. The following issue areas and goals were identified through stakeholder and public input to respond to the future challenges and opportunities in the Central Okanagan:

Issue Area	Goal
Our Land	To manage the land base effectively to protect natural resources and limit urban sprawl
Our Economy	To develop and enhance a positive business environment in the region to achieve a dynamic, resilient and sustainable economy
Our Water Resources	To manage and protect water resources
Our Health	To contribute to the improvement of community health, safety and social well-being
Our Food	To support a regional food system that is healthy, resilient and sustainable
Our Housing	To improve the range of housing types and tenures to meet the social and economic needs of the region
Our Climate	To minimize regional greenhouse gas emissions and respond to the impacts of climate change
Our Ecosystems	Be responsible stewards of natural ecosystems to protect, enhance and restore biodiversity in the region
Our Transportation	To enhance the regional transportation system to ensure that it is accessible, affordable, and efficient
Our Governance	To respond to the needs of the region with an effective and efficient governance service model

3.2.1 OUR LAND

Goal

To manage the land base effectively to protect natural resources and limit urban sprawl

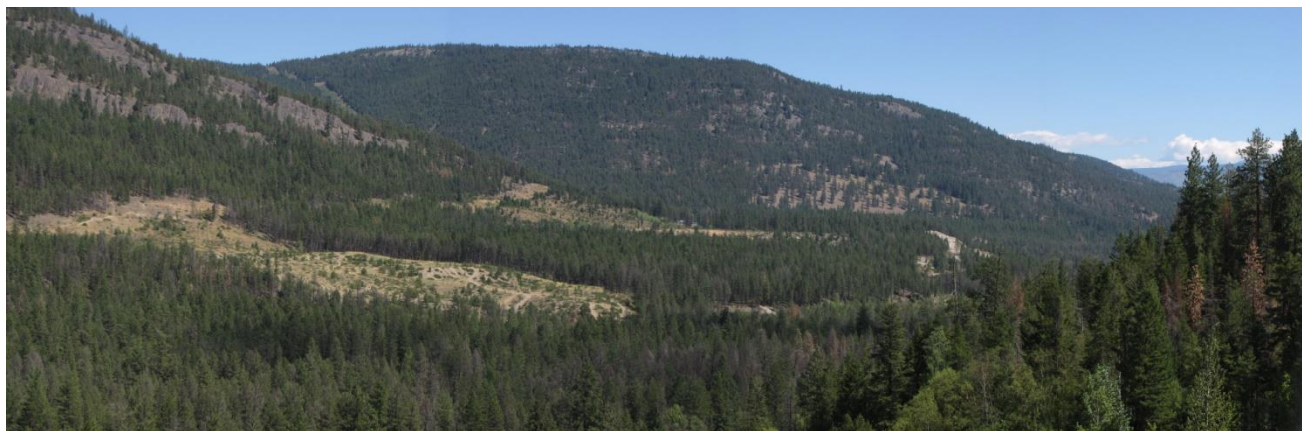
Synopsis

The Central Okanagan’s land base is largely defined by its mix of mountains, lakes, wetlands, watercourses, forests, grasslands, vineyards and orchards. These features form the significant surroundings that add to the character of the Region and help define the Central Okanagan lifestyle. Maintaining and managing the land base, both the rural and urban lands, is essential to ensure the preservation of the lifestyle that residents and tourists value. There are several areas and lands adjacent and outside the urban areas that provide for drinking water sources, agriculture, recreation, wildlife, and flora and fauna habitat as well as opportunities for the forestry and mining industries. Continued growth in this region will push development to these areas; however, focusing growth to areas of existing services can minimize impact and effectively manage opportunities for source water protection, protection of environment and habitats as well as manage opportunities for resource development.

Policies

The Central Okanagan regional partners agree to:

- .1 Support logical and sequential growth patterns that minimize urban encroachment into rural areas;
- .2 Proposals for new growth areas should consider the impacts on existing services and facilities in the community;
- .3 Support the protection of the rural areas that offer a rural lifestyle choice;
- .4 Encourage access to and opportunity for development of Crown resources and rural land that provide economic opportunities that contribute revenues to support community social, health, education and transportation services for the citizens of the Central Okanagan while having minimal impacts to the land, wildlife, and sensitive environmental areas;
- .5 Support the protection of water supply on crown land and/or rural areas for all water users;
- .6 Support the continued exchange of information with provincial agencies on future land use decisions that impact the interface of the boundaries of crown land adjacent to municipalities and regional districts and best management practices for resource extraction to minimize negative impacts in the Region (e.g., truck traffic, environmental considerations, land use conflicts, nuisances);
- .7 Support urban and rural land uses that provide affordable, effective and efficient services and infrastructure that conserve land, water and energy resources;
- .8 Support the protection of ALR lands and land uses which are supportive and/or complimentary to agricultural use;



Source: Regional District of Central Okanagan

- .9 Promote the identified urban/village centres in current community plans to provide a range of amenities and services;
- .10 Encourage the examination of future land use designations that consider the Region’s aggregate resource based on the site suitability checklist prepared in the Aggregate Supply and Demand Update (2013);
- .11 Encourage cooperation and information sharing with regional partners on hazard identification and mapping programs to provide better and more information on hazard management;
- .12 Encourage cooperation and information sharing with regional partners to identify data and information necessary to improve hazard and resiliency planning; and,
- .13 Ensure there is an appropriate supply of land used for economic opportunities, public facilities and infrastructure and protection of natural environment for the regions projected population growth.



Source: District of West Kelowna

3.2.2 OUR ECONOMY

Goal

To develop and enhance a positive business environment in the region to achieve a dynamic, resilient and sustainable economy

Synopsis

The diversity of the Central Okanagan’s economy has historically been its strength. A healthy, strong and diverse economy is one of the cornerstones of a sustainable community. The Region’s future will depend on supporting the diversity along with investment opportunities for new and upcoming economic sectors that complement the existing sectors. Local governments have the ability to influence investment climate and economic viability in the Region and as such, working together will enhance the Regions opportunities to compete regionally, nationally and globally. Attracting, maintaining and promoting economic activity in this region is important as it helps retain existing jobs and creates a solid tax base. As the Central Okanagan endeavours to become a more sustainable region and protect its quality of life, coordination and collaborating efforts will help in creating, supporting and enhancing economic opportunities throughout the Region. The growth strategy provides an opportunity to encourage and support development that enhances economic diversity for a healthier local economy.

Policies

The Central Okanagan regional partners agree to:

- .1 Support a comprehensive regional approach to create an attractive investment climate for the Region by working in partnership with the province, local municipalities, First Nations and economic development agencies;
- .2 Encourage and support related economic development initiatives that address the needs and challenges in the Region;
- .3 Support efforts in building a strong regional economy to:
 - promote and stimulate innovation;
 - create jobs
 - improve residents’ quality of life
 - foster a diverse and balanced economic base;
 - help reduce the Region’s GHG emissions; and,
 - increase economic development and regional prosperity.
- .4 Support each community’s interest to develop vital and dynamic communities with vibrant town and village centres;
- .5 Support economic plans that focus on retention, attraction and facilitation of business within the Region;
- .6 Encourage cooperation with educational institutions and the private sector to build a profile of the Region as a knowledge base/education centre;
- .7 Encourage cooperation among research facilities, educational institutions, local governments and businesses technology to explore opportunities in new economic sectors and new investment in the form of human and business capital;
- .8 Support for the provisions in the housing section are considered within the context of economic development, as the diversity of housing opportunities and affordable housing options are important to support economic development;
- .9 Support effectively managing and protecting the integrity of the Region’s critical assets such as the lakes, natural environment and agricultural lands that promote attraction of employment and investment;
- .10 Promote reliable and efficient development application processes and a commitment to long term financial planning for servicing and infrastructure (roads, sewer and water) that attracts and retains business and private investment and employment; and,
- .11 Promote land development patterns that support a diverse regional economy.

3.2.3 OUR WATER RESOURCES

Goal

To manage and protect water resources

Synopsis

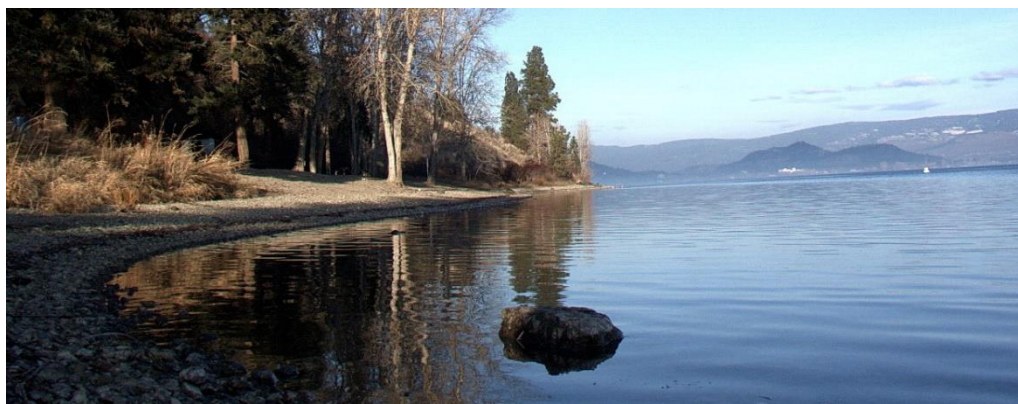
Water is an essential resource for people, the economy, and for the natural environment. Okanagan Lake is the main central feature in the Central Okanagan that provides a unique and beautiful landscape as well as the basic sustenance needed for a vibrant community and economy. Water is a collective resource that is shared by people and the environment and supports the economy. Studies show that climate change will impact water levels in the Region, which means competition for water will increase. Considering the importance water has in sustaining life and the lifestyle residents value, it is important that we continue to manage water resources effectively to ensure the Region and the Okanagan Valley can accommodate the needs for all users, including plants and animals in the environment, now and in the future.

Policies

The Central Okanagan regional partners agree to:

- .1 Consider water resources in land use planning decisions;
- .2 Encourage the development of water source protection plans to improve drinking water quality, quantity and timing of flow of water sources;

- .3 Work with local governments, provincial agencies to assess and mitigate the risks in floodplains;
- .4 Utilize best management practices to protect and manage water resources, groundwater and surface water, through integrated watershed planning to improve water quality, and adequate supply for the Region;
- .5 Continue in partnership with the Okanagan Basin Water Board (OBWB) and regional partners to encourage valley wide cooperation and coordination regarding the conservation of water and protection of all water sources;
- .6 Encourage the preparation of a water management plan by the Okanagan Basin Water Board on the Region’s water supply in response to the impacts of climate change and future population growth; and,
- .7 Encourage opportunities to utilize “on-site” recycled water for landscaping and other uses within new and redevelopment projects.



Source: District of West Kelowna

3.2.4 OUR HEALTH

Goal

To contribute to the improvement of community health, safety and social well-being

Synopsis

The Region is made up of various communities in which people live, work and play and how local governments design the communities is very important to the health and well-being of its citizens. The land use decisions that have created the current land use patterns have directed and influenced investment in infrastructure and transportation systems, which in turn have contributed to impacts on personal health. Historical land use decisions have tended to support lower-density, automobile-oriented and urban fringe development. These decisions and land use patterns have created our travel behaviours, level of activity and have had an impact on health through a person's level of physical fitness, pollution exposure and community interaction. Growth management and careful community design to encourage more physical activity will help reduce the risk for developing chronic conditions and see multiple benefits in the form of increased physical activity, less sedentary time in cars, and less air pollution. Promoting a healthy community and a corresponding healthy lifestyle will make our regional community an attractive place where people want to live, work and play.

Policies

The Central Okanagan regional partners agree to:

- .1 Actively promote and support physical activity, sense of place, social interaction and neighbourliness as these encourage the growth of the Region as a place that is safe, diverse and inclusive;
- .2 Support regional partners, including schools and businesses in strengthening healthy living opportunities that focus on physical activity;
- .3 Support active transportation initiatives of the regional partners;

- .4 Continue efforts to improve air quality by supporting the Regional Air Quality Management Plan;
- .5 Promote community health and safety through investments in education, recreation, health, community development, social support, civic design, environmental design, maintenance and economic development;
- .6 Encourage cooperation with Interior Health to explore opportunities to develop healthy community strategies;
- .7 Support Interior Health in efforts to increase public education and awareness around the links between population health and land use planning;
- .8 Incorporate health and principles of a Healthy Built Environment into the development of comprehensive plans;
- .9 Encourage joint use agreements with School District No. 23 in projects, programs and facilities to improve and maximize the use of existing parks, facilities and community services; and,
- .10 Encourage cooperation with School District No. 23 to locate and design schools to be the focal point of community life in the neighbourhoods in which they are located.



Source: District of West Kelowna

3.2.5 OUR FOOD

Goal

To support a regional food system that is healthy, resilient and sustainable

Synopsis

The Central Okanagan has strong agricultural roots and this sector has been important in defining the region and its growth pattern. With changes in population, pressures of development, increased climate impacts, water pressures, and more focus on local food production for sustainability, these changes have raised more awareness on food systems from cultivating and planting to consumption to the compost heap, and back again. Today, food policies appear on the agenda of dozens of municipal governments across North America and beyond. Future planning can help ensure food systems are adequately addressed in growth management decisions.

Policies

The Central Okanagan regional partners agree to:

- .1 Encourage cooperation with regional partners, provincial ministries and stakeholders that consider a regional agricultural strategy to identify issues and the present and future needs of the agricultural sector that will contribute to the well being of all residents to guide food security, economic opportunity and the protection of agricultural land;
- .2 Preserve and support sustainable agricultural activities and land base that enhances local agriculture through the strengthening of best practices, support of local and regional food systems and the expansion of local food markets and agri-tourism;
- .3 Encourage cooperation with the Agricultural Land Commission and the Ministry of Agriculture to promote consistency among bylaws, policies, regulations, and decisions that will be made regarding agriculture;

- .4 Support appropriate water supply for the agriculture industry through the continued efforts to have efficient irrigation infrastructure and proper functioning and healthy watersheds;
- .5 Encourage cooperation that considers the potential to use public lands for community gardens and/or creation of edible landscapes to encourage grassroots civic agriculture within urban areas;
- .6 Promote the use of agriculture and ALR lands for food production and ancillary agriculture processing and retailing consistent with uses outlined in the Agricultural Land Commission Act and Regulation and,
- .7 Protect the supply of agricultural land and promote agricultural viability.



Source: Regional District of Central Okanagan

3.2.6 OUR HOUSING

Goal

To improve the range of housing opportunities to meet the social and economic needs of the region

Synopsis

The Central Okanagan regional partners have been actively addressing local housing needs (market and non-market). Housing location, type, affordability, and choice are important factors that affect long term economic health and community sustainability. Focusing new housing units to areas with existing services can reduce infrastructure costs and support public transit services, as well as more active, healthier transportation choices such as walking and biking. Ensuring a diversity of housing options irrespective of demographics, lifestyle interests or financial situation allows people and families to live and stay in the Okanagan Valley. The identified need for more affordable housing for those who live and work in the Region continues to be a concern. It is important to consider land use decisions that focus new residential developments in areas with existing services, as this assists in housing affordability, reducing reliance on vehicles in the Region, and creates opportunities for residents to be closer to work and recreation opportunities, which in turn contributes to a healthier region.

Policies

The Central Okanagan regional partners agree to:

- .1 Preserve and enhance existing neighbourhoods through encouragement of a variety of housing types, densities, choices and affordability;
- .2 Encourage new residential units and retrofits of older residential units to incorporate building materials and products that reduce energy and water consumption;
- .3 Encourage new growth areas to include sustainable community and neighbourhood design practices that demonstrate housing affordability/housing choices for the full housing spectrum;
- .4 Build capacity within the Region to advance affordable housing initiatives and increase the amount of transition and supportive housing in the Region.



Source: Regional District of Central Okanagan

3.2.7 OUR CLIMATE

Goal

To minimize regional greenhouse gas emissions and respond to the impacts of climate change

Synopsis

The changing climate will create a challenge as the Region continues to see further climate change impacts. To help address climate change, action is required by local governments to ensure planning decisions respond to reducing GHG emissions and negative environmental impacts. Planning decisions determine land use development, transportation patterns, building design, public infrastructure and energy supply systems well into the future, and as such will have significant influence on energy consumption and levels of GHG emissions. The current land use patterns, forecasted population growth and identified residential projects in the Region will make it ambitious to achieve the provincial target of 33% reduction in GHG emissions by 2020. However, with refocusing around how the Region manages growth, there is an opportunity to achieve the target to reduce GHG emissions by 80% by 2050 in the Region. Supporting the following policies will help to reduce the impacts of climate change in the Region identified above.

Policies

The Central Okanagan regional partners agree to:

- .1 Work toward meeting the provincial target of reducing GHG emissions by 80% from 2007 levels by 2050;
- .2 Support the use of innovative approaches and technologies to help conserve energy and thus reduce GHG emissions;

- .3 Consider GHG reduction and climate adaptation/mitigation in decision-making as well as in planning policies and regulatory measures;
- .4 Reduce reliance upon fossil fuels by promoting and supporting renewable energy infrastructure, such as solar power and geothermal, and by purchasing energy supplied by renewable sources. Renewable energy systems should be pursued in collaboration with Provincial, Federal and private sector programs;
- .5 Encourage design and “retrofitting” of neighbourhoods to reduce a person’s travel distance and frequency of trips, and increase access to alternative modes of transportation;
- .6 Encourage cooperation with regional partners, provincial ministries and stakeholders on initiatives that improve efforts to reduce GHG emissions, improve energy conservation and mitigate climate change impacts;
- .7 Encourage mixed use compact communities and facilitate the transformation of existing neighbourhoods so that residents can conveniently and safely travel by bus or by foot, bicycle and other forms of active transportation to get to major community destinations while ensuring the efficient movement of goods and services; and,
- .8 Encourage land use and transportation infrastructure that improves the ability to withstand climate change impacts and natural hazard risks.

3.2.8 OUR ECOSYSTEMS

Goal

Be responsible stewards of natural ecosystems to protect, enhance and restore biodiversity in the region

Synopsis

As population grows in the Region, the protection of healthy ecosystems will become increasingly complex and challenging. The Okanagan provides unique habitat to various species of wildlife that are found nowhere else in the country. There are more than 1,597 identified species at risk in BC, including 48 species within the RDCO. The landscape is an intricate balance of natural systems that provides clean water, fresh air and an area that residents highly value. Numerous reports, strategies, and plans have been created, discussed, and adopted throughout the Regional District and across the province on the environmental concerns related to the ecosystem and it is necessary to understand and protect the ecosystems for the health of the Region. Although regional partners have a number of initiatives underway to protect the environment, there is a strong need for a regional and coordinated approach to protection, enhancement and restoration of the Region's ecosystems. The Region's natural ecosystems will continue to face development pressures and it will be important for the Region to manage growth to balance the human need for resources, recreation, enjoyment and aesthetics with the need to protect, conserve and restore natural areas and biodiversity.

Policies

The Central Okanagan regional partners agree to:

- .1 Encourage cooperation for the management of regional biodiversity practices as outlined in the Okanagan Biodiversity Strategy;
- .2 Encourage collaboration to adopt consistent terminology, policies and actions that support the protection and conservation of environmental features and watersheds within the Region;
- .3 Manage growth to minimize disturbance to habitat, watershed and natural drainage areas and systems;
- .4 Encourage collaboration with regional partners on enhancing wildlife corridor linkages to improve habitat connectivity that avoids fragmentation and isolation of important habitats;
- .5 Encourage a mix of parkland and open space that protect regionally significant ecosystems, and natural and cultural attributes;
- .6 Encourage and support coordinated efforts to protect and enhance the Region's forests, environmentally sensitive and significant areas;
- .7 Protect natural environments, parks and water systems, as these systems are essential to the quality of life in the Okanagan that support active and healthy lifestyles;
- .8 Explore funding mechanisms to support regionally significant natural areas, open space and parkland acquisitions;
- .9 Support the provisions of the Water Resources section to be considered in context of all discussions regarding the Region's ecosystems;
- .10 Encourage cooperation with land trusts and local conservation organizations to educate landowners on species at risk;
- .11 Support continued research on local species at risk within the Region and share the information;
- .12 Encourage collaboration with regional partners, provincial ministries and stakeholders to consider regional conservation, watershed and other ecosystem-based plans and strategies that will update existing inventories;
- .13 Support cooperation with regional partners on environmental matters, particularly where there are developments/issues located adjacent to political boundaries; and,
- .14 Support cooperation on the maintenance and update of environmental mapping for terrestrial, foreshore and aquatic areas throughout the Region on a regular basis.

3.2.9 OUR TRANSPORTATION

Goal

To enhance the regional transportation system to ensure that it is accessible, affordable, and efficient

Synopsis

Land use influences travel patterns and transportation systems in turn influence land use and development. Achieving the goals of the RGS requires the alignment of land use and transportation strategies. It is important to consider shifting the Region's travel patterns away from auto dependency toward moving people rather than vehicles. Within a region consisting of multiple jurisdictions, transportation that is accessible, affordable, and efficient is not a goal each jurisdiction can achieve in isolation. Everyday many people travel throughout the Region and the entire Okanagan Basin. Partners must work together to effectively plan, coordinate, manage, and monitor the Region's transportation system in order to achieve sustainable transportation goals that are defined in the individual plans and policies of the local governments of the Central Okanagan.

Policies

The Central Okanagan regional partners agree to:

- .1 Support the Sustainable Transportation Partnership of the Central Okanagan as a means of administering, governing and coordinating the delivery of regional transportation planning and services;
- .2 Encourage collaboration with regional partners, neighbouring regional districts, provincial ministries, BC Transit, and federal government to improve inter-regional transportation opportunities;
- .3 Maximize the efficiency of the regional transportation system and reduce GHG emissions through:
 - a. Providing active transportation options and connect residents to facilities, recreation and services throughout the Central Okanagan and its neighbouring communities and region;
 - b. The promotion of programs and incentives that increase sustainable transportation choices and Transportation Demand Management Strategies; and,
 - c. The support for the use of alternate-fueled vehicles (e.g. electric vehicles);
- .4 Support a regional integrated trail system for active transportation that is appropriate for commuting and/or exercise and recreational uses that connects to rural areas, parks, public facilities, and town/village centres;
- .5 Support transit oriented development as well as active transportation amenities in urban land use designations;
- .6 Place increased emphasis on sustainable modes of transportation (walking, cycling, transit) while maintaining efficient automobile, commercial goods and emergency vehicle mobility;
- .7 Encourage new educational facilities to be located central to their catchment area and where they can be accessed by sustainable modes of transportation;
- .8 Prioritize funding for transit and nonmotorized improvements, including projects such as sidewalks, traffic calming, bike lanes, and better transit service or access;
- .9 Prioritize improvements for public buildings in well-connected, compact urban areas (such as schools, government buildings) for access for pedestrians and cyclists, and the provision of bicycle parking and end-of-trip facilities;
- .10 Encourage major employers to create and support programs for active transportation options; and,
- .11 Collaborate with School District No. 23 to provide safe routes to school, including walking, cycling and transit options.

3.2.10 OUR GOVERNANCE

Goal

To respond to the needs of the region with an effective and efficient governance service model

Synopsis

The roles and services for a regional district are determined by its member jurisdictions and regional district's act only in response to the expressed needs, interests and instructions of their members and address issues that cross political boundaries. The mission of a regional district is to help protect the quality of life for all citizens in the region by planning for the future. As the Region grows, there needs to be a balance of local autonomy with effective regional coordination in order to address environmental, economic, transportation and quality of life issues that cross local government boundaries. Long range planning for the Region is necessary to identify opportunities and build partnerships to protect the health and safety of current citizens as well as future generations that will live in the Region.

- .2 Encourage effective governance and service delivery by being transparent, accountable and accessible;
- .3 Continue to build and enhance communications and relationships with local First Nations communities;
- .4 Support opportunities for regional partners to collaborate, communicate and coordinate on matters of regional significance;
- .5 Encourage collaboration among the regional partners on long range land use, infrastructure and financial planning based upon projected growth, emerging priorities and the anticipated needs of communities;
- .6 Ensure the Region maintains effective services, which meets the present and future user demands; and,
- .7 Support the assessment of the social and economic benefits of arts, culture, tourism, and recreation amenities in decision on land use.

Policies

The Central Okanagan regional partners agree to:

- .1 Encourage cooperation, collaboration and partnerships among regional partners in the delivery of effective and efficient public services and/or enhance opportunities of mutual benefit for cost sharing on procurement, delivery of services, and/or capacity building on issues to ensure financial and human resources are invested effectively;



Source: Regional District of Central Okanagan

4.0 MONITORING AND EVALUATION

4.1 Implementation and Monitoring

The RGS presents ten (10) goals and associated policies that will assist in realizing the regional vision. Implementing the goals and policies will depend on cooperation and coordination among regional partners, neighbouring regions, and other agencies. The RGS provides a strategic guide to managing the future growth in the Region. Once adopted, it will be the responsibility of the regional partners to implement the RGS by taking action consistent with the RGS as set out in Section 3. The RGS Steering Committee will need to coordinate the strategic priorities around the Region and align the priorities with the goals and policies of the RGS in order to set out a plan of action for regional growth management. The plan will be included in the RDCO annual budgeting and work plan to outline tasks and budget for RGS implementation to be endorsed by the Regional Board.

Being accountable for progress towards achieving the goals of this RGS requires a commitment to implementation, target-setting, establishing indicators, and monitoring. Undertaking projects to implement this RGS will be subject to available budgets, departmental work plans and in some cases financial contributions from regional partners and/or external agencies. Over the 20 year time horizon of the RGS some projects may be funded solely through the RDCO's Regional Planning function, with no additional costs to the regional partners, while other projects may require financial contributions from the regional partners.

Notwithstanding, the wording in this RGS refers to an "agreement" by the RDCO and regional partners to follow a course of action, neither the RDCO nor individual regional partners will be bound to undertake or participate in any projects identified in this RGS. This is consistent with the direction of the *Local Government Act* (Section 865 (3)) that states "a RGS does not commit or authorize a regional district to proceed with projects specified in the strategy."

With the regional partners working cooperatively, the strategy outlines the following set of tasks on the implementation of the RGS:

1. Update OCP Regional Context Statements
2. Develop a five year action plan
3. Explore Implementation Agreements
4. Prepare a Monitoring and Evaluation Program



Source: Regional District of Central Okanagan

5. Plan for Five-year reviews

4.1.1 UPDATE OCP REGIONAL CONTEXT STATEMENTS

After acceptance and adoption of the RGS, local governments will be required to prepare an update to their OCP to include a regional context statement. The context statement sets out the relationship between the RGS and the OCP and identifies how municipal actions will contribute to achieving the RGS goals and vision. Regional partners will continue to work together so that regional context statements achieve consistency between the RGS and local government OCP's and so that actions specified in the RGS are implemented. The updated regional context statements are to be completed within two years of the adoption of the RGS and must be submitted for acceptance to the Regional District after a review of the RGS.

The RDCO and regional partners will work to ensure OCP policies are consistent with the goals, objectives and policies of the RGS. The process should lead to consistency over time, recognizing the economic, social and environmental benefits of healthy community development, preservation and connection of natural features, and ongoing land use planning cooperation, collaboration and harmonization with First Nations.

4.1.2 FIVE YEAR ACTION PLAN

The RGS is designed to be realized over a twenty year period and implemented through implementation agreements, regional context statements and through budget discussions regarding regional planning initiatives. The review of the RGS (2011) emphasized that we are, individually and jointly, responsible for the effective management of the future growth for the region. To ensure the region responds to this call to action, through a collaborative process, staff and elected officials will develop a 5 year action plan to outline the priority initiatives to implement the RGS. The development of a detailed 5-Year Action Plan is considered a key element of a RGS implementation in order to address the challenges facing the region over the next 20 years. The 5-year action plan will be the framework for RGS implementation and based on the regional initiatives identified within the RGS and by the Regional Board.

4.1.3 IMPLEMENTATION AGREEMENTS

An implementation agreement (IA) is a partnership agreement between a regional district and other levels of government, their agencies or other bodies which spells out the details of how certain aspects of a RGS will be carried out. Implementation agreements are an important tool designed to promote coordination between local governments and provincial agencies. Consideration of agreements in the Central Okanagan should focus on measures to maintain water quantity and quality, regional transportation, regional environmental coordination, stronger regional economic development and providing accessible and affordable housing. IA's can deal with a wide range of matters making them an important tool for coordinating local-provincial partnerships and actions. The Agreements may be used to establish commitments for infrastructure investments, joint planning projects, responsibility and revenue-sharing agreements, and policy development

4.1.4 RGS MONITORING AND EVALUATION

After adopting an RGS, the Regional District, as directed in the LGA section 869, must establish a program to monitor the implementation and progress of the RGS. The effectiveness of the RGS is assessed through a monitoring program. The monitoring program will be established within the first year after RGS adoption. The Regional District and regional partners will need to discuss and agree on a monitoring program with identified performance metrics. The monitoring program is intended to provide the foundation for ongoing monitoring and evaluation of the strategy. It can be added to or modified over time to better meet the needs of the Board of Directors and regional partners. Monitoring is important to ensure the Board and local government partners have feedback on whether or not the goals and policies of the RGS are being achieved.

The program should include performance measures to review the condition, trend or emerging questions under the regional issue areas. A list of performance measures is provided in the appendix for each regional issue area. The measures will help track changes over time relative to the baselines. Reviewing and tracking the changes will allow the regional partners to work toward the regional vision.

The monitoring program will include an annual report and a review of the RGS every five years to reassess the strategy and consider whether amendments are necessary. The annual monitoring report will contain a mixture of the measures outlined in Section 3. A key aspect of the implementation of the RGS is the commitment to designing and implementing a practical plan for performance measurement, monitoring and accountability.



Source: Regional District of Central Okanagan

4.1.5 PLAN FOR FIVE-YEAR REVIEW

The Local Government Act (LGA) provides for a five-year review opportunity for Regional Growth Strategies. The review process provides an opportunity to assess the strengths and weaknesses of the growth strategy, assess performance and re-evaluate solutions to persistent region-wide issues and responses undertaken. The growth strategy five year review will be an inclusive process involving the public, regional partners, organizations and other authorities as listed in section 855 of the LGA. The review will respond to new provincial policies and legislation, as well as to initiatives, research, studies and plans developed responding to the regional issues that will assist with the Region as a whole working toward the RGS vision. The review will also assess the Region’s demographic data, census data, and assessment of the identified initiatives undertaken in RGS.

Analysis of land use, environmental, engineering, transportation and financial issues should be combined into the review process to allow the public and decision-makers to have a more complete understanding of growth impacts in the Region. This comprehensive approach will consider all of the various issues and trade-off’s involved in planning and environmental assessment considerations.

Five-Year Reviews form the basis for regional and local assessment of progress towards the Region’s future growth vision. The Five-Year Reviews should set out short-term implementation strategies to provide for adequate land, infrastructure and public facilities over a minimum of five years, and to encourage maximum utilization of existing infrastructure and development opportunities prior to extending development into the undeveloped greenfield areas of the Region.



Source: Regional District of Central Okanagan

4.2 Amendments to the RGS

4.2.1 STANDARD AMENDMENTS

An amendment to the RGS, other than those considered a minor amendment, is considered a standard amendment and will follow the same process that is required to adopt a RGS as set out in Part 25 of the Local Government Act.

4.2.2 MINOR AMENDMENTS

Recognizing that the RGS will require some flexibility to respond to changing conditions in the Region, the minor amendment process is intended to provide a more streamlined amendment process for minor changes, while ensuring that amendments that substantially change the vision and direction of the strategy remain subject to acceptance by all affected local governments.

The Local Government Act enables minor amendments where a process has been established pursuant to section 857.1 that includes the following:

- criteria for determining whether a proposed amendment is minor for the purposes of allowing the process to apply;
- a means for the views of affected local governments respecting a proposed minor amendment to be obtained and considered;
- a means for providing notice to affected local governments respecting a proposed minor amendment;
- a means for providing public consultation and input into the proposed minor amendment; and,
- procedures for adopting the minor RGS amendment bylaw.

Criteria for Minor Amendments

Criteria under which a proposed amendment to the RGS may be considered a minor amendment include the following:

- a) RGS policy revisions or additions that do not alter the intent, direction or implementation of the Strategy;
- b) Housekeeping amendments to population, dwelling unit and employment projections, housing demand estimates, tables, figures, grammar, numbering or mapping refinements

that do not alter the intent of the Regional Growth Strategy

Any proposal that does not meet the criteria set out above would be considered a standard amendment and will be required to follow the regular process as outlined in the *Local Government Act, Part 25*.

Minor Amendment Process

The process to initiate amendments to the RGS is by resolution of the Board of Directors. Municipalities and the Electoral Areas, by resolution, may request amendments to the RGS. The Board of Directors will not give first reading to a minor or standard amendment bylaw until the regional partners have been given the opportunity to formally comment on the proposed amendment.

On receipt of a request from a member municipality or an Electoral Area to amend the RGS, the request will be provided to the Regional District, for review with due consideration of sub-regional or regional impacts on infrastructure, transportation, land use, precedence and cumulative effects of broad replication. The Regional District staff will provide recommendations to the Board of Directors.

Once a request for a minor amendment has been received, the process for review and adoption is as follows:

- Upon receiving a minor amendment request, the RDCO staff will review the request.
- RDCO staff will prepare a report for review by the RDCO Board of Directors.
- The RDCO Board of Directors will assess any proposed amendment in terms of the minor amendment criteria. The Board of Directors may resolve, by an affirmative vote of 2/3 of the board members present, to proceed with an amendment request as a minor amendment.
- Where the Board of Directors resolves to proceed with an amendment request as a minor amendment, the Board will:
 - Determine the appropriate form of public consultation required in conjunction with the proposed minor amendment.

- Give 45 days’ written notice to each affected local government, including notice that the proposed amendment has been determined to be a minor amendment. The notice shall include a summary of the proposed amendment and any staff reports, other relevant supporting documentation and the date, time and place of the board meeting at which the amending bylaw is to be considered for first reading.
- Consider the written comments provided by the affected local governments prior to giving first reading to the proposed amendment bylaw.
- At the time of consideration of first reading, the Board of Directors will determine whether an opportunity for the public to speak on the RGS minor amendment bylaw is required.
- If the first reading of any minor amendment bylaw receives an affirmative vote from all Board of Directors attending the meeting, then the bylaw shall be adopted in accordance with the procedures that apply to the adoption of a RGS under section 791 of the Local Government Act and Region District of Central Okanagan Regional Board Procedure Bylaw No. 1278, 2010 or as amended.
- If any minor amendment bylaw does not receive affirmative vote of all Board of Directors attending the meeting, the bylaw shall be adopted in accordance with the procedures that apply to the adoption of a RGS under section 857 of the Local Government Act and Regional District of Central Okanagan Regional Board Procedure Bylaw No. 1278, 2010 or as amended.

APPENDICES

Appendix	Contents
A	Maps
B	List of Research Papers
C	Performance Metrics

Appendix A - Maps

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Appendix B – List of Research Papers

Beginning in 2011, the first phase of the RGS Review and Update assessed numerous documents, plans and studies in its background data collection process. These documents include legislation, by-laws and policies, planning references and a number of specific studies and plans throughout the Okanagan Valley and British Columbia.

The RGS Review and Update was an opportunity to coordinate and align community plans, policies, priorities and programs among the local municipalities, provincial ministries and First Nations. In the last five years, local municipalities within the Region have reviewed and updated their Official Community Plans to identify their visions for the long term well being of the communities and plan for their own needs while ensuring the needs of the future generations are met. The RGS Review and Update process analyzed information from local municipalities and First Nations to ensure that all policy directions and consultations will be incorporated to lead the Region into a sustainable future.

The update of the RGS sets the direction for current and future regional decision making with the goal of a sustainable future for the Region. The RGS document is built on the analysis of historical trends and the collection of current and forecasted data. Community planners and consultants attempted to identify all the data required so that the Region and communities can make informed choices and decisions about future growth and change in the Region. The need to review the RGS is to ensure that the most current data is considered and to understand the trends related to issue identification. In some cases, issue identification is achieved through the public consultation process, literature review, and analysis of economic and statistical information. Best practices and lessons learned from other jurisdictions respecting planning and environmental issues and growth management have been included where relevant.

Over the course of the RGS Review and Update process, the RDCO and regional partners completed and gathered information from various studies, assessments, inventories and updated their Official Community Plans. This work has been reviewed to understand broad planning scenarios in the Region and helps provide the necessary detail to set directions and make decisions as required to provide. The following reports and studies completed for the RGS Review and Update have helped enhance the research completed through the previous work:

RGS – Preliminary Consultation Process. EcoPlan International, April 2011

Background and Issues Report. RDCO Planning Department, July 2011

Greenhouse Gas (GHG) Emissions Technical Report. Sustainability Solutions Group, October 2011

Regional GHG Emissions Model. Sustainability Solutions Group, October 2011

RGS Youth Survey. Linda Abbott-Simons, Consultant, December 2011

Regional Discussion Papers:

- *Economic Development* - CTQ Consultants Ltd., Ecoscape Environmental Consultants Ltd., September 2012
- *Environment Protection* - CTQ Consultants Ltd., Ecoscape Environmental Consultants Ltd., September 2012
- *Housing* - Cityspaces Ltd., September 2012
- *Parks & Open Space* - AECOM Consultants, GDH Solutions, September 2012
- *Transportation and Mobility* - Acurere Consulting Inc, Silex Consulting Inc., September 2012
- *Water Resources, Lakes & Streams* - CTQ Consultants Ltd., Ecoscape Environmental Consultants Ltd. Clarke Geoscience Ltd. and Western Water Associates Ltd., September 2012
- *Aggregate Demand and Supply Update* - EBA Consultants, October 2013

RGS Growth Options Consultation. Sustainability Solutions Group, November 2012

Appendix C – Performance Metrics

Performance metrics are an opportunity to highlight the realizations and challenges faced by the Central Okanagan region in planning and managing for growth. The measures were selected in consultation with local governments, in order to track the Central Okanagan progress. Measures help track changes over time and assess future changes relative to an established baseline. Reviewing and tracking the changes in the measures allows the regional partners to gain knowledge and amend directions as necessary to work toward the regional vision. The information gathered with the measures does not facilitate change as it merely indicates past performance. The Region will be able to learn from the information to allow the Region to “correct its course” by modifying and adjusting its policies and tasks as the Region moves forward.

The following table provides some performance metrics for the Central Okanagan. This list is not an exhaustive list as more work and research will be done for the annual and 5 year reporting on the RGS.

Indicator	Indicator Description	Why it is important	Data Source
Our Land			
Population Density	Measuring population per land area within defined OCP areas. 70% of RDCO is crown land.	Higher densities in urban areas are related to more efficient services and less urban sprawl. Communities of the Central Okanagan encourage development within municipalities and serviced areas of the Region as opposed to dispersed development enclaves on outlying crown land.	RDCO GIS – RGS land inventory Census Data – DA’s
Composition of urban and village centres	Measure the commercial floor area and number of residential units within designated urban and village centres as per OCPs.	Designated urban and village centres are places where residents can work, shop and enjoy cultural/educational/recreational amenities close to home. A balance of amenities provides a range of services and opportunities so that residents are less likely to need to travel long distances for basic goods and services.	BC Assessment RDCO GIS – Land inventory OCP – urban and village centres
Our Economy			
Employment Rate	Number of persons employed (15 yrs and older) working	Employment rates indicate the strength of the local economy. High rates of employment result in stability of the community and social well being.	BC Stats
Business Licences	Any person owning or operating a business within the Central Okanagan	Focus on support and retention of businesses in the Region	Regional Partners
Central Okanagan Number of firms	Industry Sector based on NAICS Canada (North American Industrial Classification System)	Shows the Regions businesses according to type of economic activity.	BC Stats
Household Income	Average and Median household income	Average and Median household income provides an assessment of the net effect of economic and employment strategies. Income levels are an indicator of the strength of the local economy. Compare to provincial levels.	BC Stats
Income by Source	This indicator measures the percentage of total income that comes from three categories of income sources: earnings, government transfers and other money	Shows the portion of income from government sources or investments could leave the economy open to vulnerability.	BC Stats

Indicator	Indicator Description	Why it is important	Data Source
Building Permits	Number of building permits issued for residential, commercial, industrial and institutional	Identify the level of building activity in the Region.	Regional Partners
Total employment by sector	This indicator measures the percentage of employed people that are working in each sector as an indication of the employment diversity in the Region.	This indicator shows the concentration of the labour force in various employment sectors. A predominance of people working in one sector could mean the Region is more economically vulnerable. Alternatively, employment diversification increases resilience to economic downturns. By looking at the changing proportions of employment in each sector, this indicator can reveal how the underlying economic health of the Region is changing.	BC Stats

Our Water Resources

Water consumption	Total water consumption from the water purveyor distribution systems (litres/capita/day).	Water availability is a concern and with population growth and associated development in the Central Okanagan, there is a potential shortfall in supply relative to demand.	Regional Partners OBWB
Water Quality Ratings	Groundwater (Monitoring Wells) and Surface Water # of boil water advisory days (total), by water utility; # of water systems that meet provincial and federal standards	Changes in water quality reflect the value we place on and our ability to care for aquatic ecosystems. Changes in the quality of water give us an idea of the success of community efforts to protect water for drinking, swimming, fish, etc. and to respond to factors that affect water quality (e.g.: industrial discharges, sewage treatment methods, development activities, resource extraction, etc.).	Regional Partners
Source Water Protection	# of source water protection plans implemented	The number of plans in place indicates a willingness from public agencies to be proactive in addressing issues associated with growth, multiple stakeholders and limited natural resources.	Regional Partners

Our Health

Chronic diseases	Number of people with chronic disease admitted to KGH		Interior Health
Air Quality	Air Quality Exceeds the Canada-Wide Standards (for PM 2.5 & Ground level Ozone) Average concentrations (seasonal Jan, Mar, Jun, Sept)	Air pollution has been shown to have detrimental effects on human health- particularly to persons with respiratory ailments.	Kelowna
Joint Use Agreements/ projects/ programs/ facilities	Agreements between two or more government entities that set out terms and conditions for use of public property or facilities	Help communities and school districts work together to develop joint use agreements in order to increase access to recreational facilities on school grounds that provide opportunities for various services that support health and social well-being.	Regional Partners SD 23

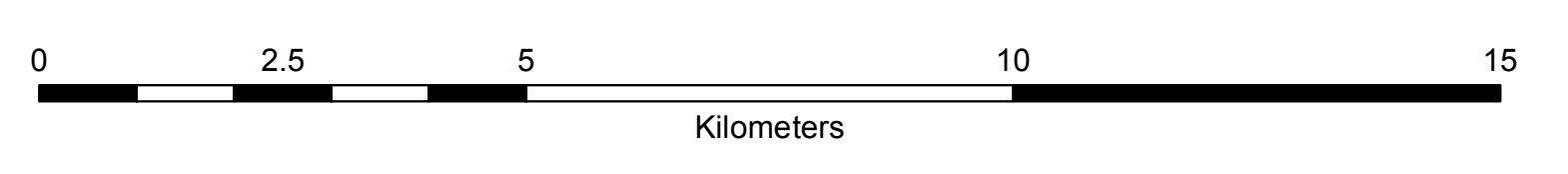
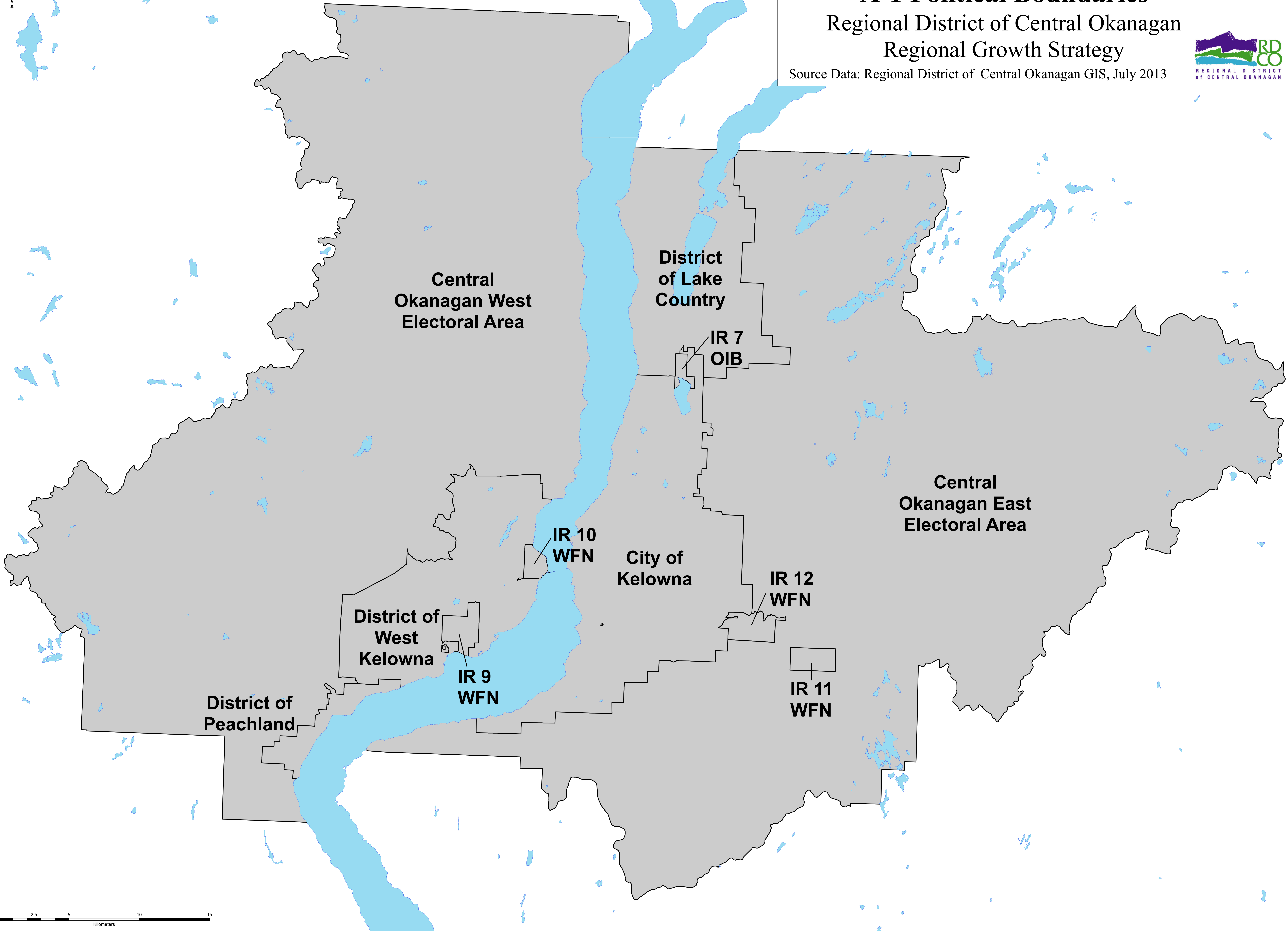
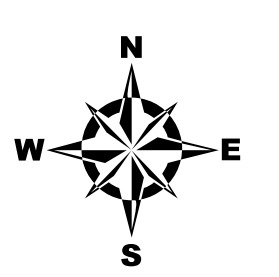
Our Food

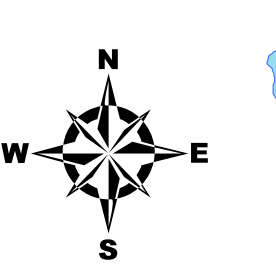
ALR exclusions and inclusions	Tracks the success of the protection of agricultural land	Preservation of land for agriculture is the first and most important step in ensuring agricultural viability.	ALC
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Indicator	Indicator Description	Why it is important	Data Source
Agriculture Farm Receipts	measures the total value of gross farm receipts (in dollars)	Provides a measure of the economic health of the agricultural sector based on financial returns to farm operators and the area of land farmed.	Census
Our Housing			
Housing Starts	the number of new units by structural type	Illustrates economic health, as well as shows the range of housing types being supported in the marketplace.	BC Stats
Mix of dwelling units	housing mix in the Region	A diversity of housing types suggests greater choice for a variety of lifestyles and budgets and supports improved housing affordability	Census
Housing affordability	Difference between median income level and sales price/rental of homes	Review the ability of individuals and families to afford housing within the Region.	BC Stats
%of owner households spending 30% or more of gross income on housing	measures the percentage of owner households spending 30% or more of their gross income on housing.	Paying more than 30% of gross household income often places strain on both individuals and families. Understand how the Region is fairing with affordability.	Census
%of renter households spending 30% or more of gross income on housing	measures the percentage of renter households spending 30% or more of their gross income on housing.	Paying more than 30% of gross household income often places strain on both individuals and families. Understand how the Region is fairing with affordability.	Census
Our Climate			
Community Energy and Emissions Inventory	energy consumption and greenhouse gas emissions from community activities in on-road transportation, buildings and solid waste	Province provides the report that measures the GHG profiles to help local governments meet its Climate Action initiatives.	Province
Our Ecosystem			
Annual and cumulative area of parkland and protected areas	total amount and percentage of total land area of parks and protected natural areas	Parks and protected areas provide habitat and support biological diversity. Where there is limited conservation land, it is more likely to be intensively managed and cultivated potentially at the expense of natural habitat, processes or systems.	Regional Partners
Environmentally Sensitive Lands	the amount of terrestrial environmentally sensitive areas protected by park or protected area	The Okanagan in particular represents a biodiversity “hotspot” as it provides unique habitats to wildlife that are found nowhere else in the country. There are more than 1,597 identified species at risk in BC, including 48 species within the RDCO.	Regional Partners
Municipal solid waste disposal per capita	annual amount of municipal solid waste (MSW) disposed in landfills or incinerated by residential, commercial, institutional, demolition, land clearing or construction source	Demonstrate how well the Region is doing at reducing waste and the impact on the environment.	RDCO

Indicator	Indicator Description	Why it is important	Data Source
Reclaimed wastewater discharge	Annual volume of waste water discharged to Okanagan Lake from water reclamation plants	As population growth increases, potentially increase the amount of waste water being discharged into the lake. Understanding the volume and consider the effluent being discharge will allow monitoring of water quality and identify any potential issues.	MOE
Our Transportation			
Active Transportation Streets	# km of streets with pedestrian and cycling facilities, # km of cycling infrastructure, # km of sidewalks	Providing safe transportation routes encourages an alternative to vehicle travel	Regional Partners
Transit usage	Annual transit trips per community/route	Transit usage illustrates our ability to operate a system that attracts riders, to plan neighbourhoods that are readily serviced by transit, and/or shows a societal shift from dependence on the single occupant vehicle to public transit.	Regional Partners
Passenger Vehicles Per Capita	The number of vehicles per capita is directly related to the number of vehicles on the road.	It provides indirect information regarding energy use, air pollution and levels of investment in public road infrastructure.	ICBC
Our Governance			
Collaborative Governance	Number of regional partnership initiatives	Opportunities for the Regional partners to spread costs and benefits over a larger benefiting area in order to achieve greater economies and efficiencies.	RDCO
Community Initiatives	# of community initiatives supported by Regional District or municipalities	Support initiatives that assist in implementing the RGS.	Regional Partners

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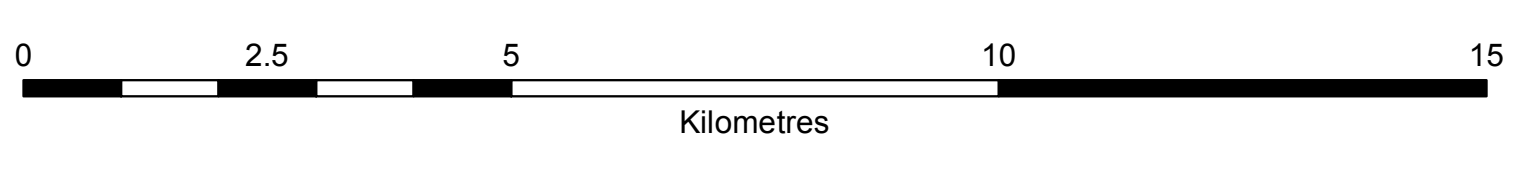
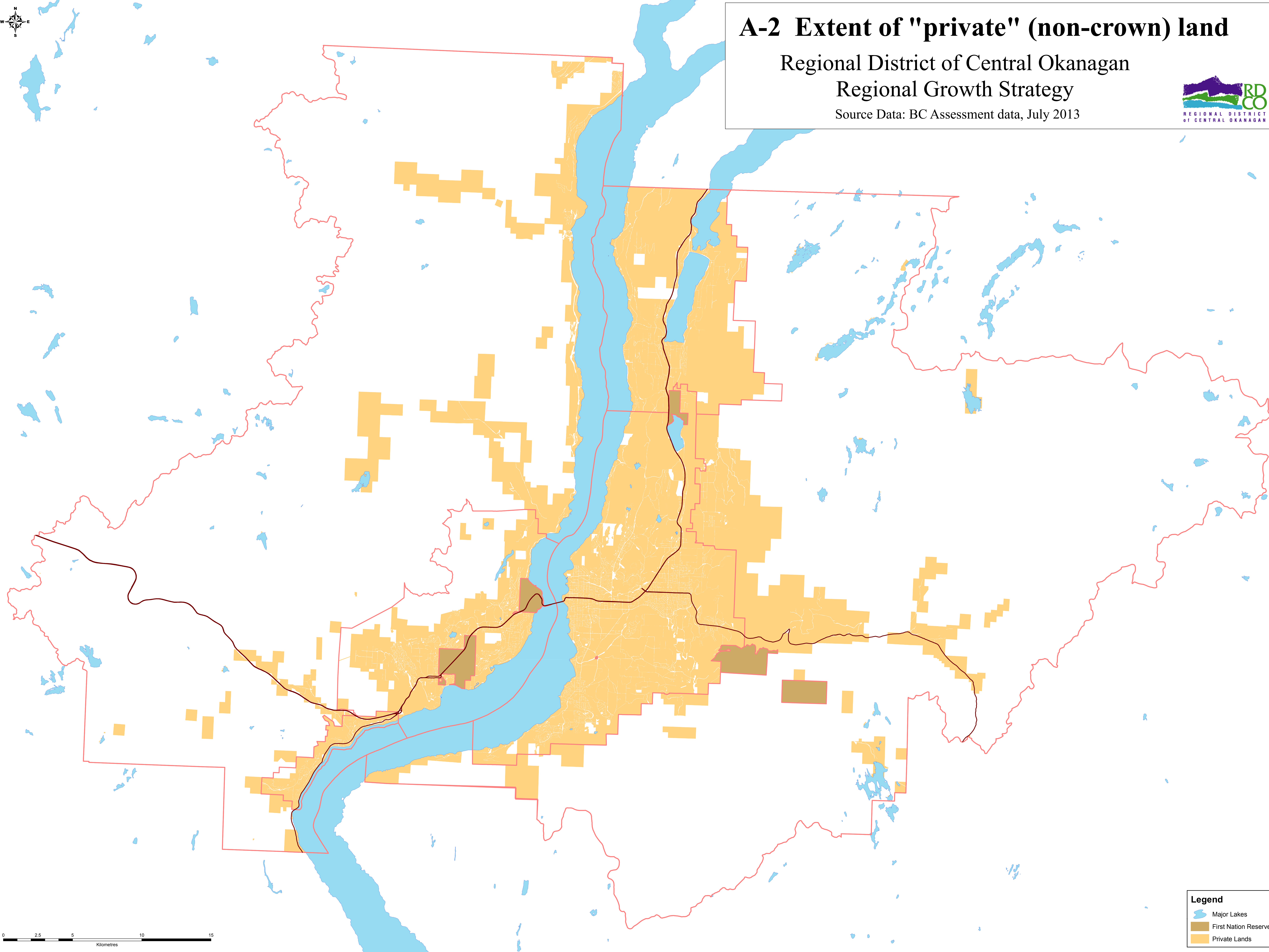


A-2 Extent of "private" (non-crown) land

Regional District of Central Okanagan

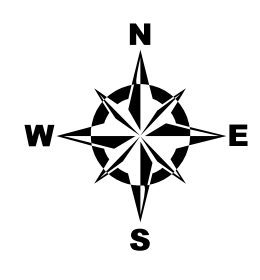
Regional Growth Strategy

Source Data: BC Assessment data, July 2013



Legend

- Major Lakes
- First Nation Reserve
- Private Lands



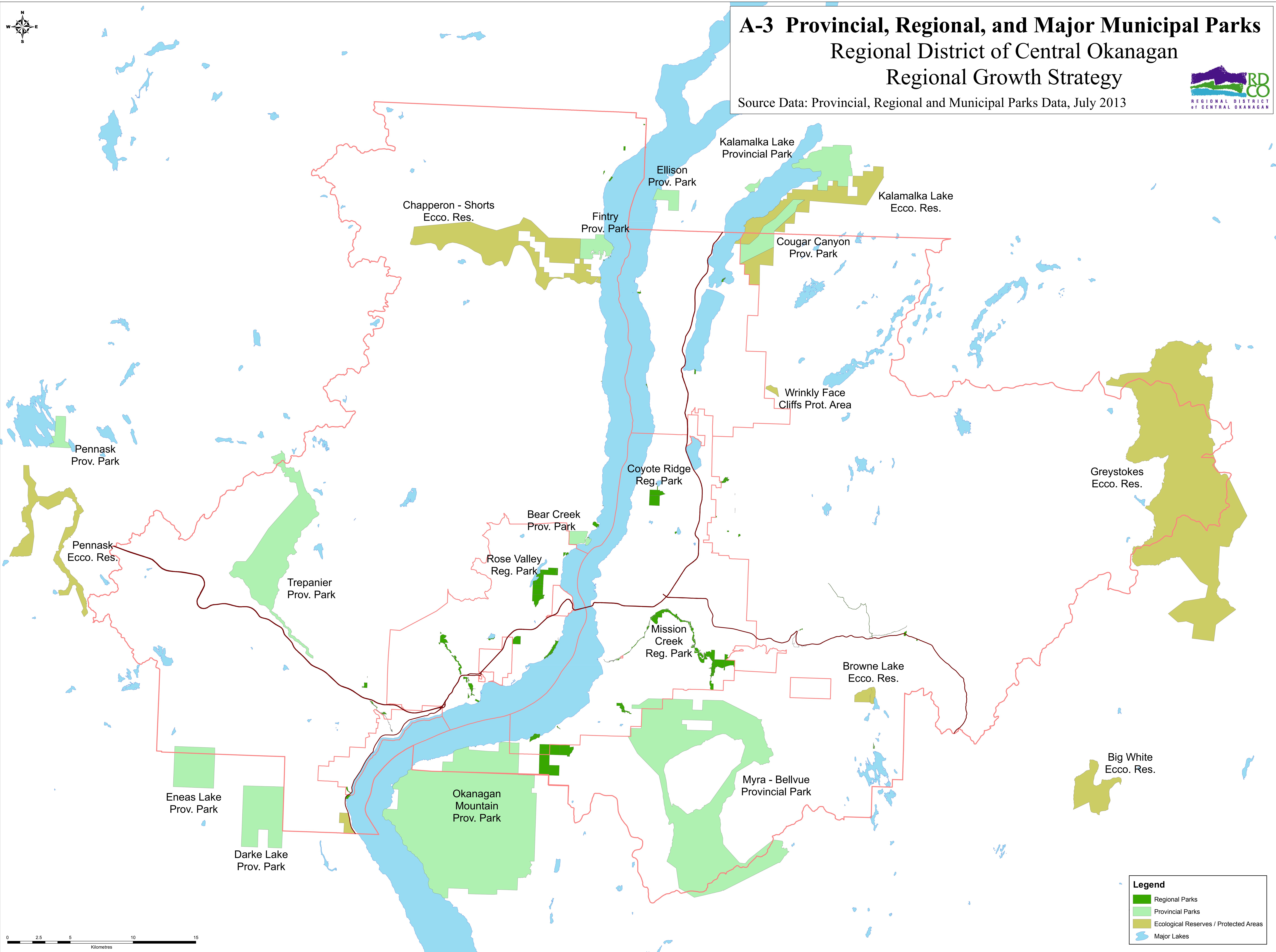
A-3 Provincial, Regional, and Major Municipal Parks

Regional District of Central Okanagan

Regional Growth Strategy

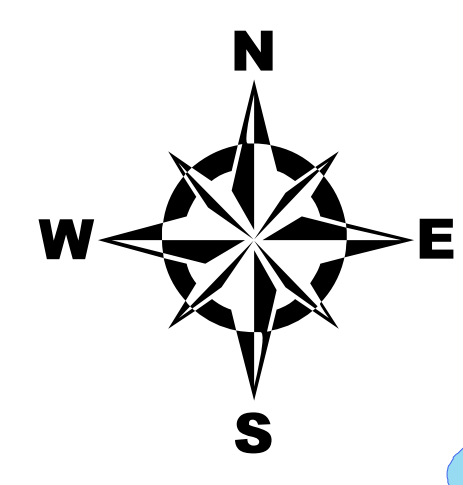


Source Data: Provincial, Regional and Municipal Parks Data, July 2013



Legend

- Regional Parks
- Provincial Parks
- Ecological Reserves / Protected Areas
- Major Lakes



A-4 Preliminary Constraint Areas

Regional District of Central Okanagan

Regional Growth Strategy

Source Data : Current Regional and Municipal Official Community Plans, July 2013



Legend

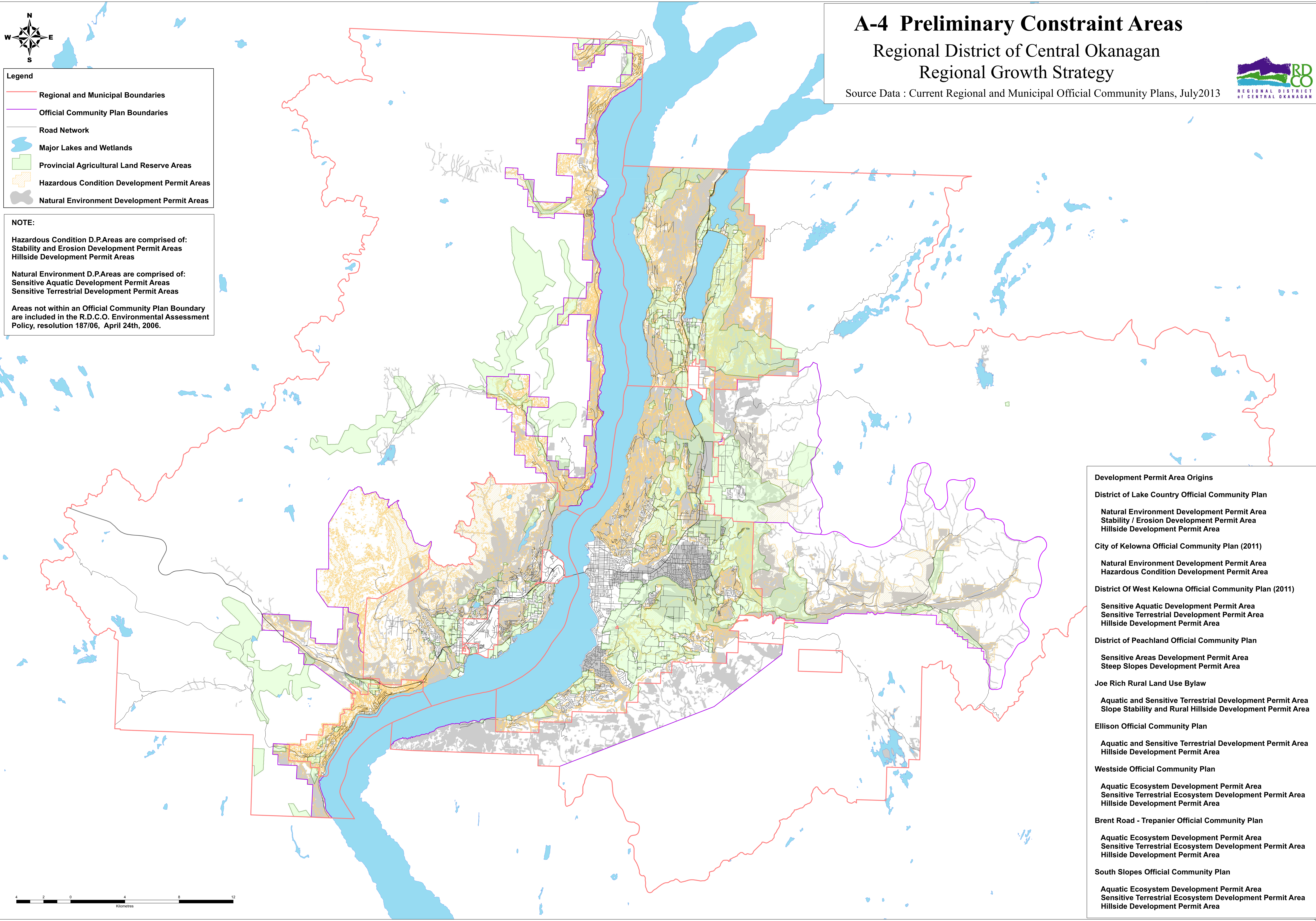
- Regional and Municipal Boundaries
- Official Community Plan Boundaries
- Road Network
- Major Lakes and Wetlands
- Provincial Agricultural Land Reserve Areas
- Hazardous Condition Development Permit Areas
- Natural Environment Development Permit Areas

NOTE:

Hazardous Condition D.P. Areas are comprised of:
 Stability and Erosion Development Permit Areas
 Hillside Development Permit Areas

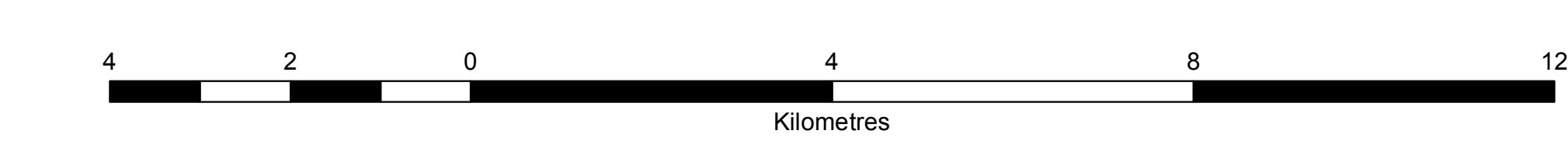
Natural Environment D.P. Areas are comprised of:
 Sensitive Aquatic Development Permit Areas
 Sensitive Terrestrial Development Permit Areas

Areas not within an Official Community Plan Boundary are included in the R.D.C.O. Environmental Assessment Policy, resolution 187/06, April 24th, 2006.



Development Permit Area Origins

- District of Lake Country Official Community Plan
 - Natural Environment Development Permit Area
 - Stability / Erosion Development Permit Area
 - Hillside Development Permit Area
- City of Kelowna Official Community Plan (2011)
 - Natural Environment Development Permit Area
 - Hazardous Condition Development Permit Area
- District Of West Kelowna Official Community Plan (2011)
 - Sensitive Aquatic Development Permit Area
 - Sensitive Terrestrial Development Permit Area
 - Hillside Development Permit Area
- District of Peachland Official Community Plan
 - Sensitive Areas Development Permit Area
 - Steep Slopes Development Permit Area
- Joe Rich Rural Land Use Bylaw
 - Aquatic and Sensitive Terrestrial Development Permit Area
 - Slope Stability and Rural Hillside Development Permit Area
- Ellison Official Community Plan
 - Aquatic and Sensitive Terrestrial Development Permit Area
 - Hillside Development Permit Area
- Westside Official Community Plan
 - Aquatic Ecosystem Development Permit Area
 - Sensitive Terrestrial Ecosystem Development Permit Area
 - Hillside Development Permit Area
- Brent Road - Trepanier Official Community Plan
 - Aquatic Ecosystem Development Permit Area
 - Sensitive Terrestrial Ecosystem Development Permit Area
 - Hillside Development Permit Area
- South Slopes Official Community Plan
 - Aquatic Ecosystem Development Permit Area
 - Sensitive Terrestrial Ecosystem Development Permit Area
 - Hillside Development Permit Area



Legend

- Regional and Municipal boundaries
- Official Community Plan Boundaries
- Area Structure Plans
- Major Lakes and Wetlands

Regional Land Use

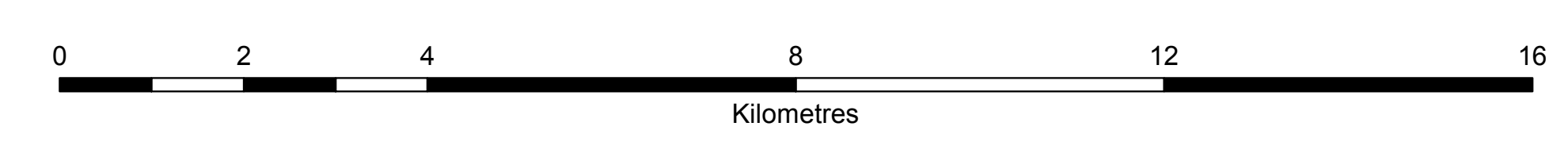
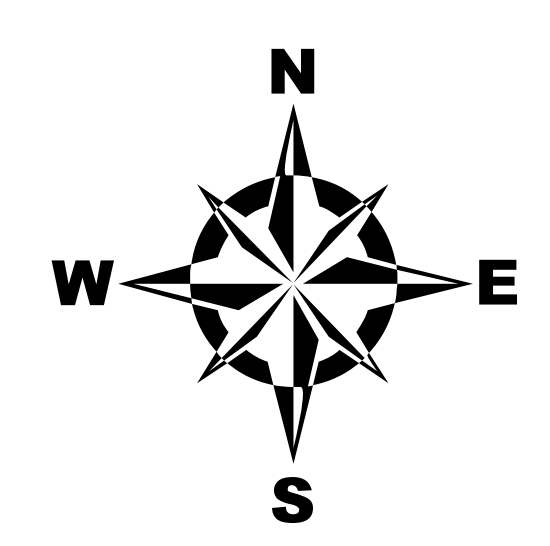
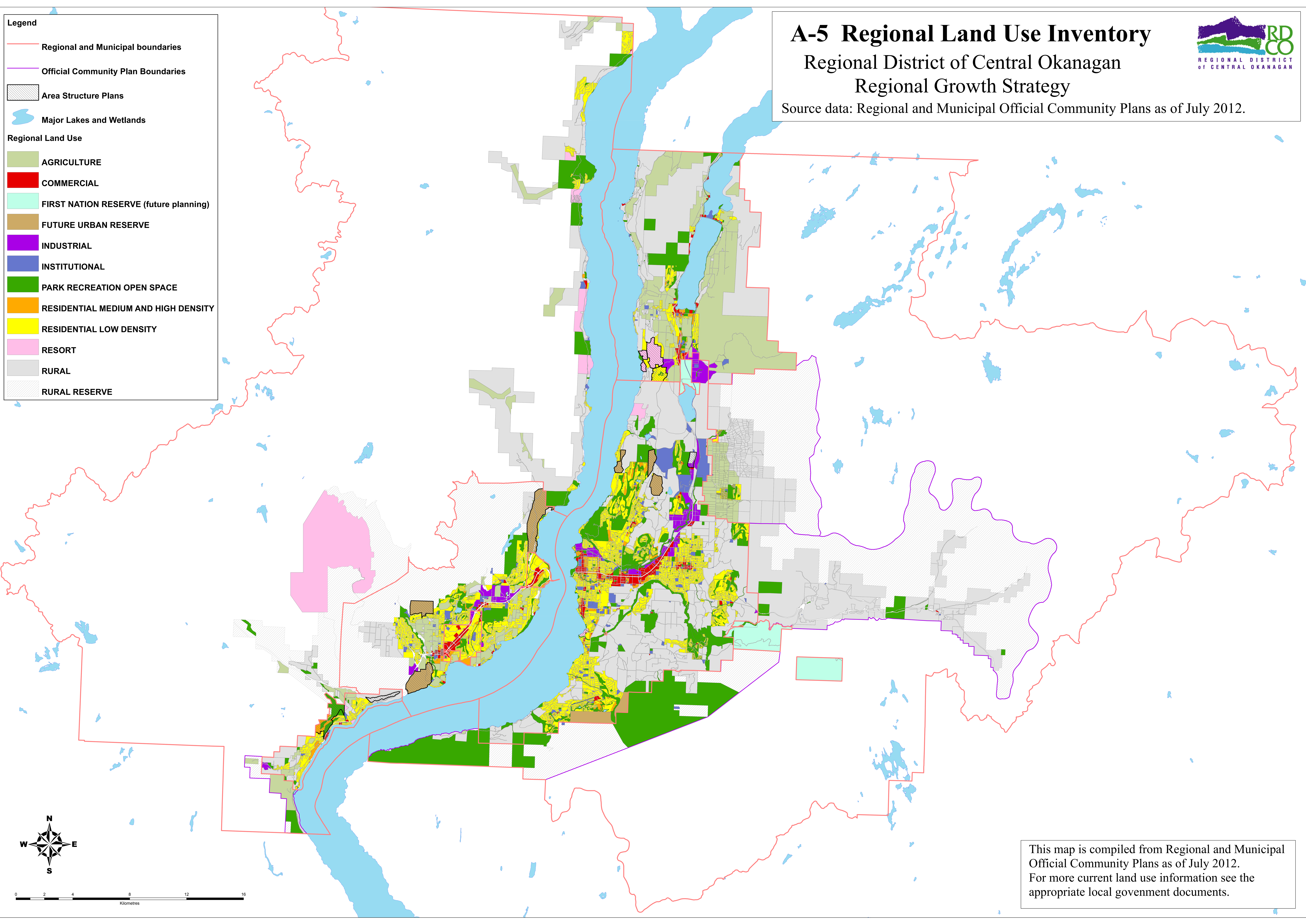
- AGRICULTURE
- COMMERCIAL
- FIRST NATION RESERVE (future planning)
- FUTURE URBAN RESERVE
- INDUSTRIAL
- INSTITUTIONAL
- PARK RECREATION OPEN SPACE
- RESIDENTIAL MEDIUM AND HIGH DENSITY
- RESIDENTIAL LOW DENSITY
- RESORT
- RURAL
- RURAL RESERVE

A-5 Regional Land Use Inventory

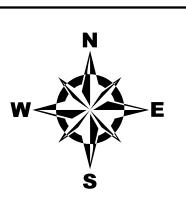
Regional District of Central Okanagan

Regional Growth Strategy

Source data: Regional and Municipal Official Community Plans as of July 2012.



This map is compiled from Regional and Municipal Official Community Plans as of July 2012. For more current land use information see the appropriate local government documents.

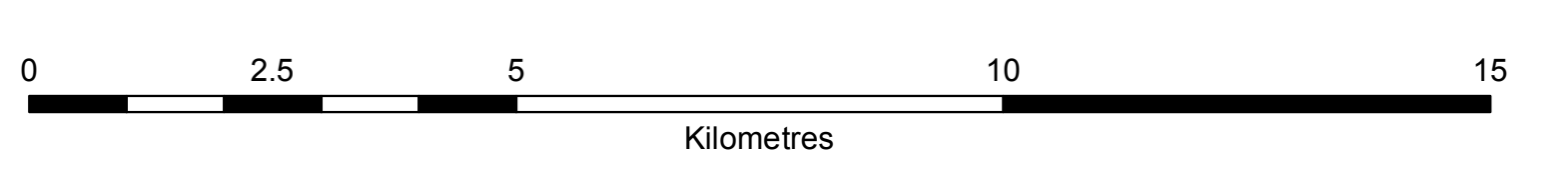
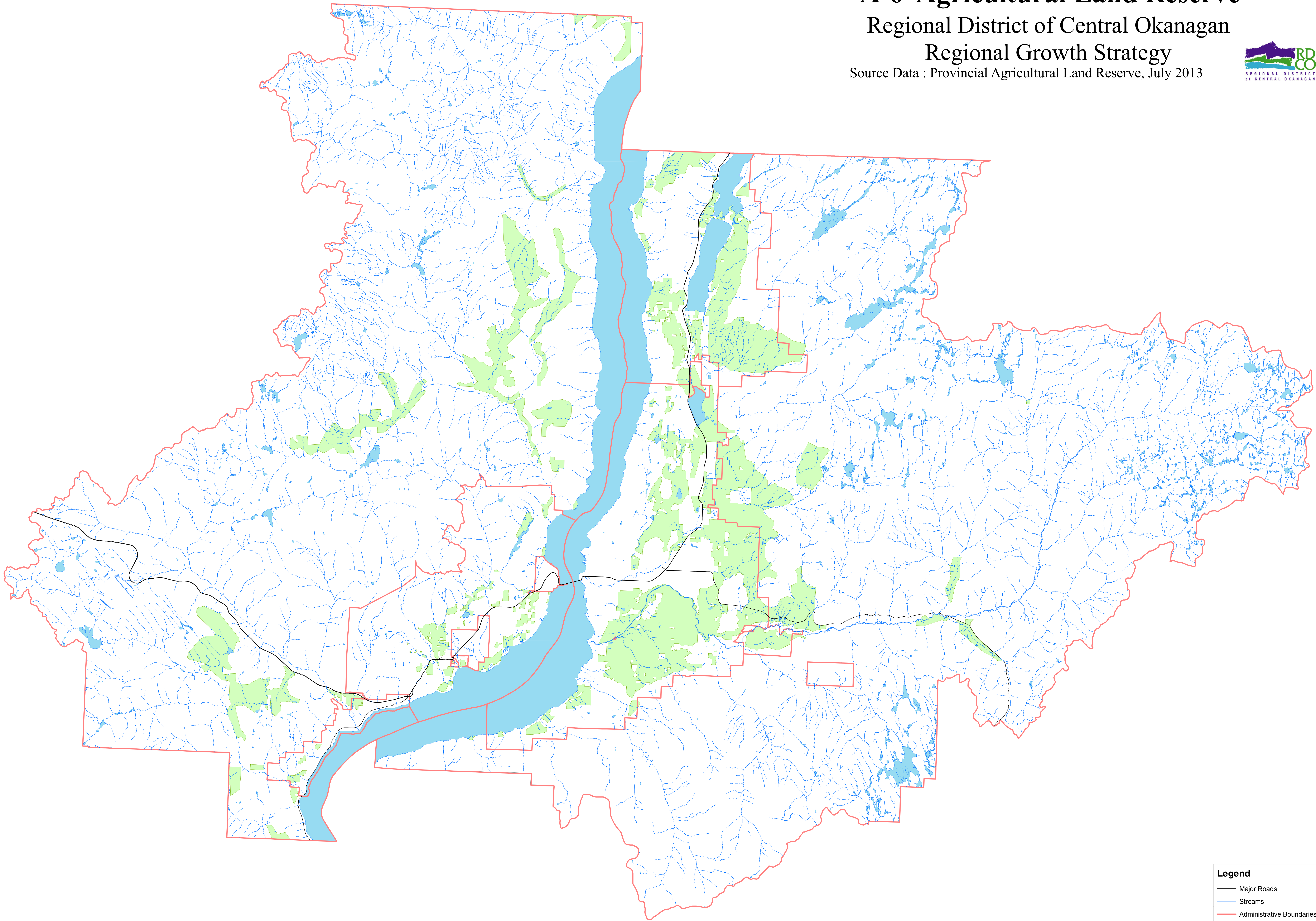


A-6 Agricultural Land Reserve

Regional District of Central Okanagan

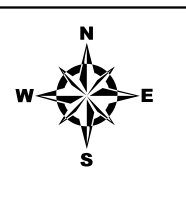
Regional Growth Strategy

Source Data : Provincial Agricultural Land Reserve, July 2013



Legend

- Major Roads
- Streams
- Administrative Boundaries
- Lakes
- Agricultural Land Reserve

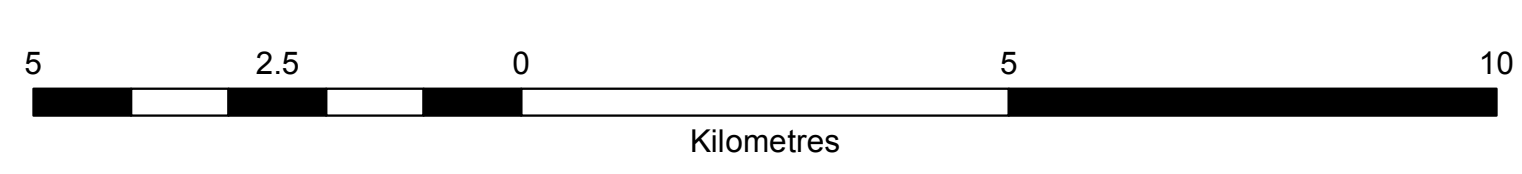
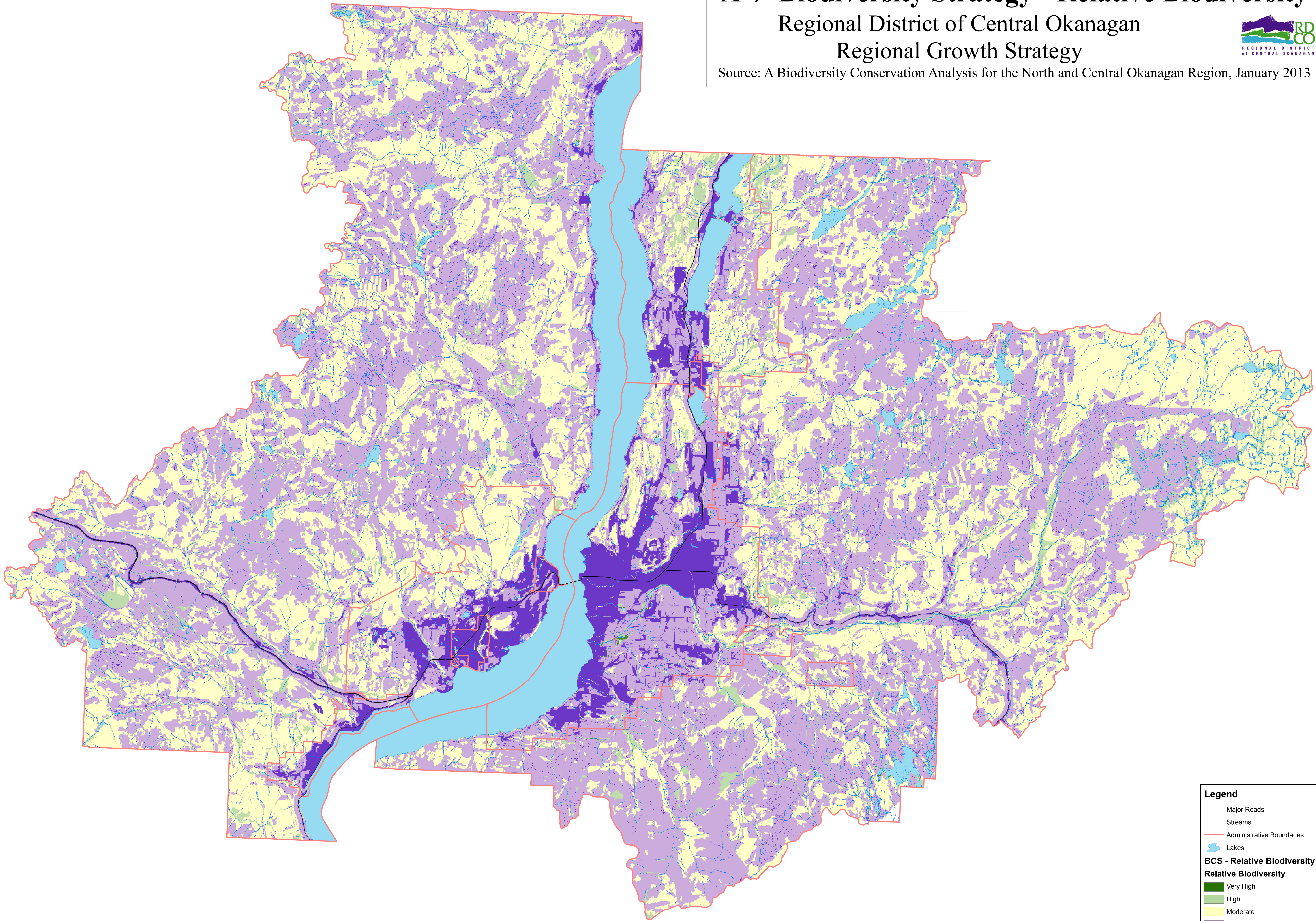


A-7 Biodiversity Strategy - Relative Biodiversity

Regional District of Central Okanagan

Regional Growth Strategy

Source: A Biodiversity Conservation Analysis for the North and Central Okanagan Region, January 2013



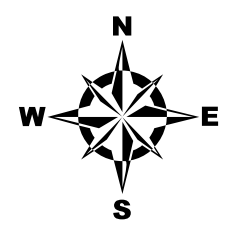
Legend

- Major Roads
- Streams
- Administrative Boundaries
- Lakes

BCS - Relative Biodiversity

Relative Biodiversity

- Very High
- High
- Moderate
- Low
- Very Low



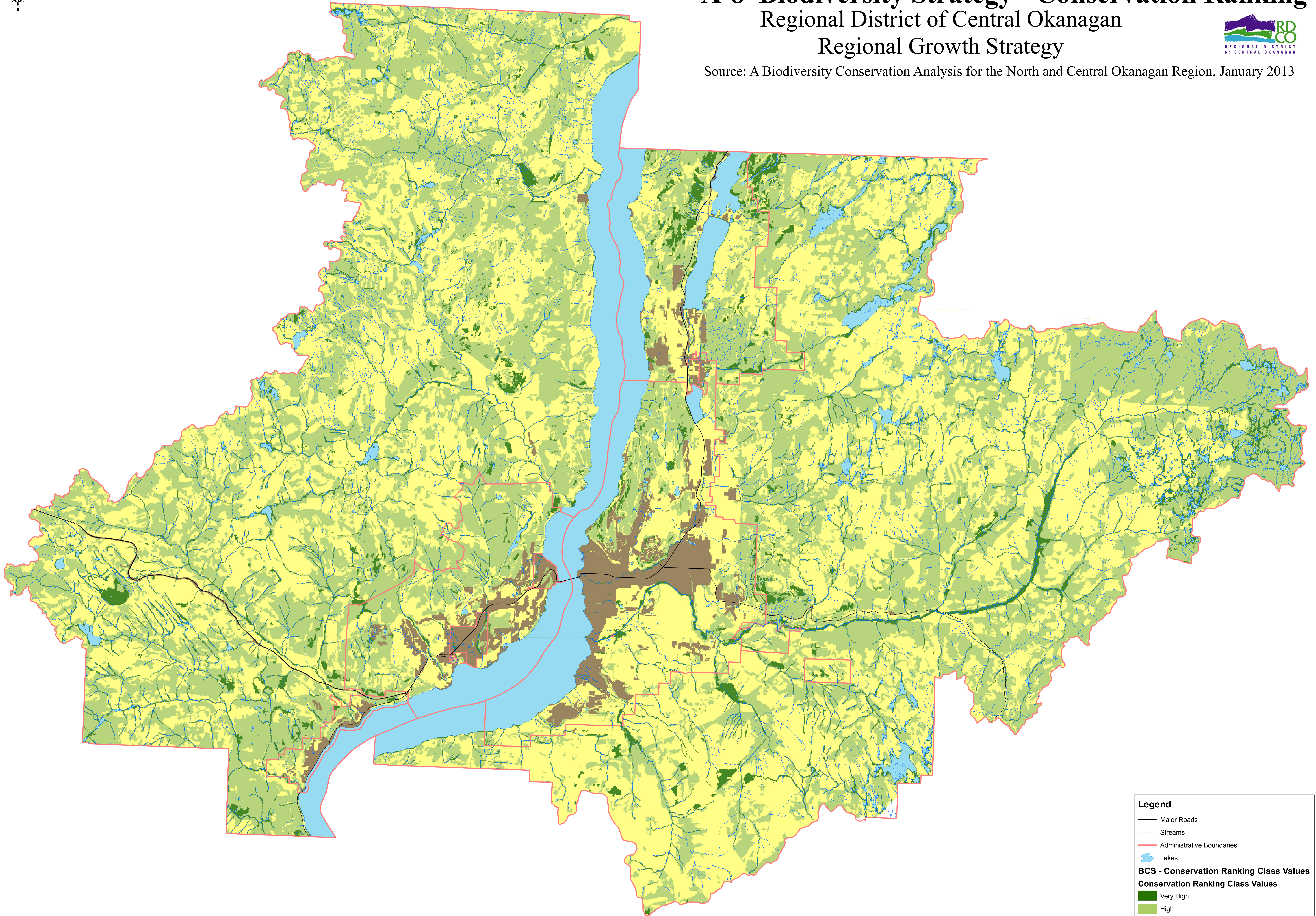
A-8 Biodiversity Strategy - Conservation Ranking

Regional District of Central Okanagan

Regional Growth Strategy



Source: A Biodiversity Conservation Analysis for the North and Central Okanagan Region, January 2013



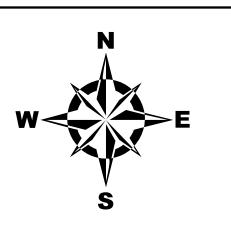
Legend

- Major Roads
- Streams
- Administrative Boundaries
- Lakes

BCS - Conservation Ranking Class Values

Conservation Ranking Class Values

- Very High
- High
- Moderate
- Low



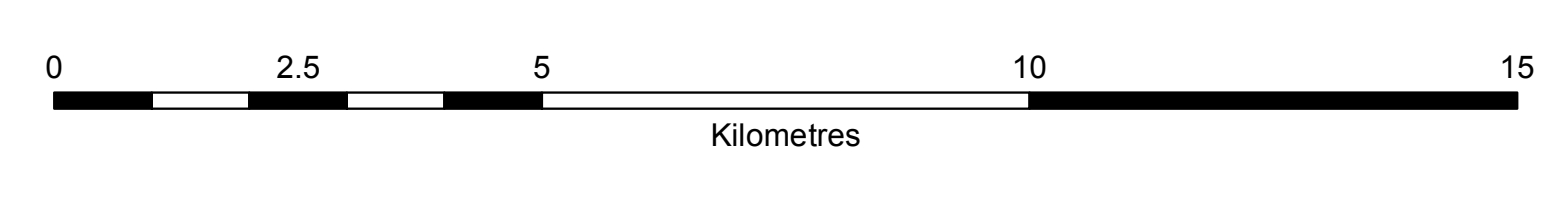
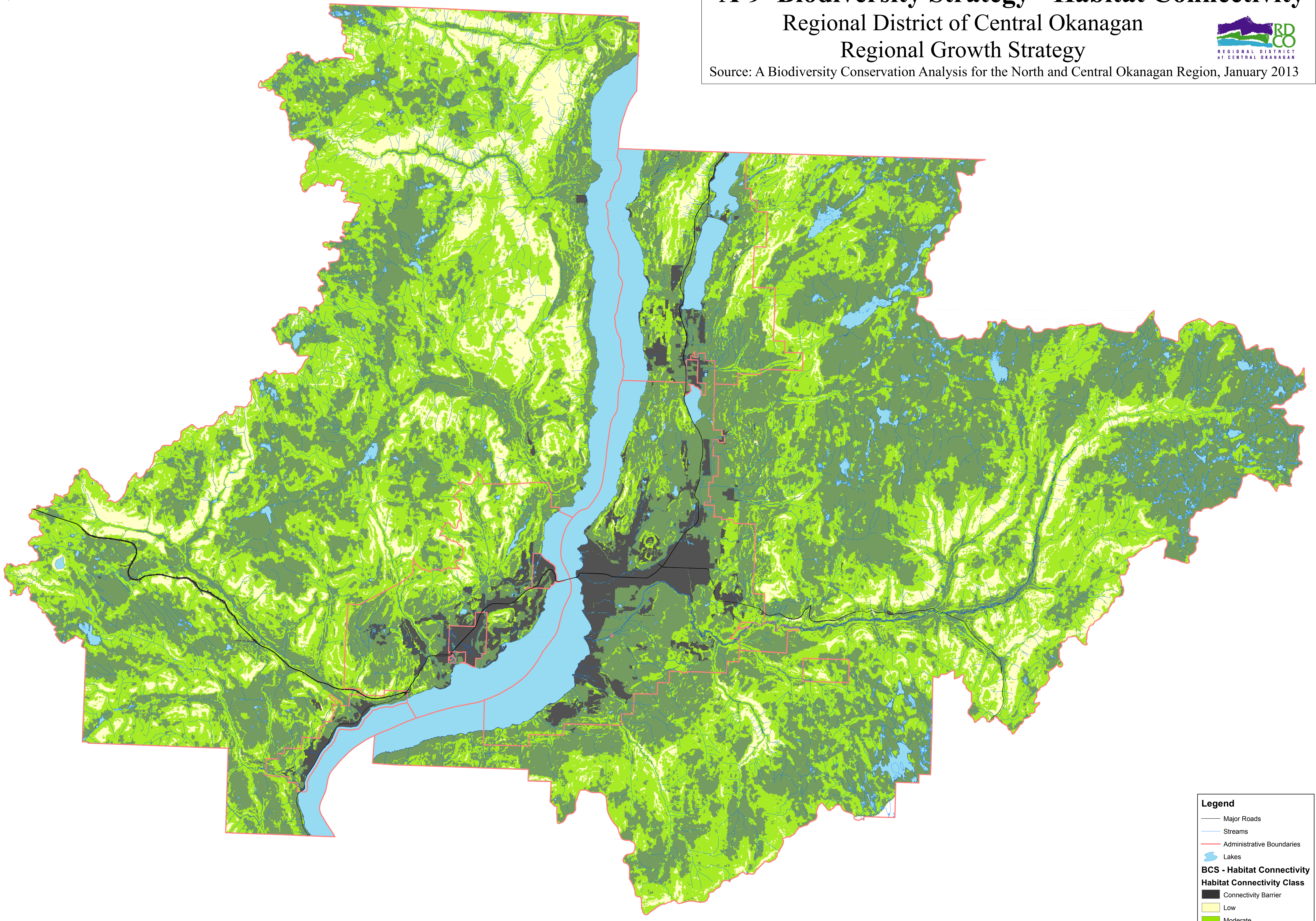
A-9 Biodiversity Strategy - Habitat Connectivity

Regional District of Central Okanagan

Regional Growth Strategy



Source: A Biodiversity Conservation Analysis for the North and Central Okanagan Region, January 2013



Legend

- Major Roads
- Streams
- Administrative Boundaries
- Lakes

BCS - Habitat Connectivity

Habitat Connectivity Class

- Connectivity Barrier
- Low
- Moderate
- High

CITY OF KELOWNA

BYLAW NO. 10950

Five Year Financial Plan 2014-2018

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. Schedule "A" attached hereto and forming part of this bylaw is hereby declared to be the Five Year Financial Plan of the City of Kelowna for the period January 1st, 2014 to and including December 31st, 2018.
2. Schedule "B" attached hereto and forming part of this bylaw is hereby declared to be the Statement of Objectives and Policies in accordance with Section 165 (3.1) of the *Community Charter*.
3. This bylaw may be cited for all purposes as the "Five Year Financial Plan Bylaw, 2014-2018, No. 10950".

Read a first, second and third time by the Municipal Council this 5th day of May, 2014.

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

Schedule "A"

Financial Plan 2014 - 2018

	<u>2014</u>	<u>2015</u>	<u>2016</u>	<u>2017</u>	<u>2018</u>	<u>2019-2030</u>
REVENUE						
PROPERTY VALUE TAX	107,697,000	113,075,817	120,151,912	126,756,800	131,318,553	1,976,499,755
LIBRARY REQUISITION	5,422,208	5,530,652	5,641,265	5,754,047	5,868,998	88,335,370
PARCEL TAXES	3,451,626	3,272,073	3,219,995	3,241,120	3,262,562	43,020,762
FEES AND CHARGES	98,185,243	98,252,183	100,791,173	103,230,002	105,755,134	1,446,765,291
BORROWING PROCEEDS	62,454,080	1,000,000	5,215,000	4,965,000	2,383,230	31,425,731
OTHER SOURCES	57,527,140	45,037,994	50,434,310	46,115,988	45,637,954	601,790,788
	<u>334,737,297</u>	<u>266,168,719</u>	<u>285,453,654</u>	<u>290,062,957</u>	<u>294,226,431</u>	<u>4,187,837,697</u>
TRANSFERS BETWEEN FUNDS						
RESERVE FUNDS	1,721,276	2,255,221	2,274,631	2,294,421	2,314,592	30,520,650
DCC FUNDS	18,683,395	20,025,508	15,069,848	17,407,661	12,000,829	177,710,688
SURPLUS/RESERVE ACCOUNTS	81,002,077	39,544,817	38,985,607	42,599,158	22,665,911	348,931,087
	<u>101,406,748</u>	<u>61,825,546</u>	<u>56,330,086</u>	<u>62,301,240</u>	<u>36,981,332</u>	<u>557,162,425</u>
TOTAL REVENUE	<u>436,144,045</u>	<u>327,994,265</u>	<u>341,783,740</u>	<u>352,364,197</u>	<u>331,207,763</u>	<u>4,745,000,122</u>
EXPENDITURES						
MUNICIPAL DEBT						
DEBT INTEREST	10,295,577	10,019,384	12,497,882	13,399,424	13,319,395	95,441,499
DEBT PRINCIPAL	11,897,321	11,699,430	13,474,621	13,901,710	15,046,052	90,128,726
CAPITAL EXPENDITURES	182,853,670	74,112,791	77,656,744	81,267,623	50,123,534	961,381,766
OTHER MUNICIPAL PURPOSES						
GENERAL GOVERNMENT	39,379,148	39,709,308	40,765,052	41,707,831	42,888,947	615,377,207
PLANNING, DEVELOPMENT &						
BUILDING SERVICES	22,047,988	20,527,862	21,152,583	21,755,260	22,370,389	322,369,653
COMMUNITY SERVICES	58,052,503	59,572,401	61,860,113	64,212,519	66,475,335	956,590,060
PROTECTIVE SERVICES	45,935,120	48,286,637	50,086,189	51,767,871	53,701,803	770,521,714
UTILITIES	17,236,300	16,892,163	17,347,583	17,827,151	18,118,083	271,781,811
AIRPORT	11,469,410	11,863,878	12,263,958	12,639,948	13,022,008	186,841,769
	<u>399,167,037</u>	<u>292,683,854</u>	<u>307,104,724</u>	<u>318,479,337</u>	<u>295,065,546</u>	<u>4,270,434,205</u>
TRANSFERS BETWEEN FUNDS						
RESERVE FUNDS	11,206,507	11,453,535	11,468,714	11,473,667	11,480,223	151,380,410
DCC FUNDS						
SURPLUS/RESERVE ACCOUNTS	25,770,501	23,856,876	23,210,303	22,411,193	24,661,994	323,185,508
	<u>36,977,008</u>	<u>35,310,411</u>	<u>34,679,016</u>	<u>33,884,860</u>	<u>36,142,217</u>	<u>474,565,917</u>
TOTAL EXPENDITURES	<u>436,144,045</u>	<u>327,994,265</u>	<u>341,783,740</u>	<u>352,364,197</u>	<u>331,207,763</u>	<u>4,745,000,122</u>

Schedule "B"
Statement of Objectives and Policies

In accordance with Section 165(3.1) of the *Community Charter*, municipalities are required to include in the Five Year Financial Plan, objectives and policies regarding each of the following:

- (a) For each of the funding sources described in Section 165(7) of the *Community Charter*, the proportion of total revenue that is proposed to come from that funding source;
- (b) The distribution of property value taxes among the property classes that may be subject to taxes; and
- (c) The use of permissive tax exemptions.

Funding Sources

Table 1 shows the proportion of total revenue proposed to be raised from each funding source in 2014. Property taxes and fees and charges are two of the largest sources of revenue. Both have advantages in that they are stable, relatively simple to administer and are generally understood by citizens. The City of Kelowna also utilizes funds from reserves and surplus as another main source of financial support. Reserve funds are closely managed to ensure and protect the current and future financial viability of the municipality. Other sources of revenue may be variable and fluctuate from year to year depending on the economic influences and capital programs undertaken by the City.

Objectives

- Investigate other potential funding sources and securing opportunities for additional revenues.
- Begin to decrease the municipality's reliance on property taxes and explore opportunities to increase the percent of total revenue received from user fees and charges and senior government grants.
- Maintain a fees and charges structure whereby increases are applied on a regular basis in line with inflation, while ensuring that service levels remain competitive and affordable.

Policies

- Pursue non-property tax revenues whenever possible through applying for government grants and charging user fees at appropriate levels.
- Perform regular reviews of revenue generating areas for appropriate application of rate increases.
 - Planning and Development Fees.
 - Recreation & Cultural Services - application of BC Consumer Price Index.
 - Utility Revenues - ensure Utilities operate as self supporting enterprise funds.
- Increase provincial and federal grant revenue through maximum utilization of the City's Grant Manager position.

Table 1: Sources of Revenue

Revenue Source	Revenue \$ (000's)	% of Revenue
Property Value Tax	107,697	25%
Library Requisition	5,422	1%
Parcel Taxes	3,452	1%
Fees & Charges	98,185	23%
Borrowing Proceeds	62,454	14%
Other Sources	57,527	13%
Reserve Funds/Accounts	101,407	23%
Total	436,144	100%

Distribution of Property Tax Rates

Table 2 outlines the council approved municipal tax distribution policy for 2014 and the relative proportion of tax revenues. Projected revenues from the combined residential, recreational and Non-Profit classes, provides the largest proportion of property tax revenue. This cumulative class represents the largest tax assessment base and hence utilizes the majority of City services.

Objectives

- Provide an effective tax change that is the same for all property classes.
- Ensure that business and light industry property tax ratios remain below the average of BC municipalities with populations greater than 75,000.
- Allow for a maximum ratio cap of 3.00:1 for the Light Industrial/Business class.

Policies

- Council will annually review and modify tax class ratios to provide an effective tax change that is the same for all classes.
- The impacts on other property classes from administering a ratio cap on the Light Industrial/Business classes will be reported to Council during the annual Tax Distribution Policy review.
- Regularly review and compare the City’s relative position in terms of distribution of taxes to other similarly sized municipalities in British Columbia.

Table 2: Tax Class Ratios and Projected Revenues

Property Class	Description	2014 Tax Class Ratios	Tax Revenue (000’s)	2013 Tax Class Ratios
01/08/03	Res/Rec/NP/SH	1.0000:1	74,845	1.0000:1
02	Utilities	5.0301:1	511	5.0475:1
04	Major Industrial	3.0908:1	360	3.0391:1
05/06	Light Ind/Bus/Other	2.0881:1	31,514	2.0822:1
09	Farm Land	0.1242:1	10	0.1279:1
91	Farm Improvements	0.4953:1	457	0.5034:1
	Total Revenues		107,697	

Permissive Tax Exemptions

The City has an existing permissive tax exemption policy which guides the administration and approval of permissive tax exemptions. Some of the eligibility criteria for permissive tax exemptions that are outlined in the policy include the following:

- The applicant must qualify for an exemption under the provisions of the Community Charter.
- The organization receiving an exemption must be a registered non-profit society or registered charity, as the support of the municipality will not be used for commercial and private gain.
- The tax exemption must demonstrate benefit to the community and residents of the City by enhancing the quality of life (spiritually, educationally, socially and culturally), while delivering services economically to the citizens within the community.

The value of tax exemptions provided by Council for 2014 (based on 2013 assessment totals and tax rates) is \$2,145,301. The following breaks down the total into various exemption categories and the exemption value for the category:

Places of Worship - \$293,306
Private schools - \$161,682
Hospitals - \$ 114,229
Special Needs Housing - \$54,540
Social Services - \$177,922
Public Park, Athletic or Recreational - \$651,246
Cultural - \$252,692
Partnering, Heritage or Other Special Exemptions Authority - \$390,783
Revitalization - \$48,901

In order to encourage the restoration and preservation of commercial, industrial and institutional building, properties that meet the criteria outlined in the Heritage Building Tax Incentive Program policy can receive a tax exemption.

The establishment of the Revitalization Tax Exemption policy allows qualifying properties within the Downtown Urban Centre and Rutland Urban Centre areas to receive a tax exemption.

Objectives

- Continue to provide permissive tax exemptions to support qualifying organizations that improve the well-being of the community.
- The municipality will continue to provide heritage and revitalization tax exemptions for qualifying properties.

Policies

- Permissive tax exemptions will be considered to encourage activities that: (a) are consistent with the quality of life objectives of the municipality; (b) provide direct access and benefit to the public; and (c) would otherwise be provided by the municipality.
- To meet the city's commitment to the ongoing restoration, preservation and maintenance of buildings and structures on its Heritage Register, eligible properties will be considered for a tax exemption.
- To support the city's revitalization program of the Downtown Urban Centre and Rutland Urban Centre, qualifying properties will be considered for a tax exemption.

CITY OF KELOWNA

BYLAW NO. 10952

Tax Structure Bylaw, 2014

WHEREAS the Letters Patent of the City of Kelowna provide that the municipality may be divided into two (2) or more taxation areas by bylaw adopted prior to the adoption of the Annual Budget Bylaw;

NOW THEREFORE, the Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. Taxation Area 1

All lands and improvement thereon classified for assessment purposes as "Farm".

2. Taxation Area 2

All lands and improvements thereon not included in Taxation Area 1.

3. This bylaw shall be applicable for the 2014 taxation year.

4. This bylaw may be cited for all purposes as "Tax Structure Bylaw, 2014 No. 10952".

Read a first, second and third time by the Municipal Council this 5th day of May, 2014.

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

CITY OF KELOWNA

BYLAW NO. 10948

Annual Tax Rates Bylaw, 2014

WHEREAS the Letters Patent dated the Twenty-fifth day of April, 1973 for the City of Kelowna provides for differing levels of taxation taking into consideration the extent of level of services being provided to different areas within the municipality.

The Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. The following rates are hereby imposed and levied for the taxation year 2014:
 - (a) For all lawful General purposes of the municipality on the assessed value of land and improvements taxable for general municipal purposes, rates appearing in column "A" of Schedule 1 of this Bylaw;
 - (b) For Debt purposes on the assessed value of land and improvements taxable for general municipal purposes, rates appearing in column "B" of Schedule 1 of this Bylaw;
 - (c) For purposes of the Okanagan Regional Library on the assessed value of land and improvements taxable for Regional Library purposes, rates appearing in column "C" of Schedule 1 of this Bylaw;
 - (d) For Hospital purposes on the assessed value of land and improvements taxable for Regional Hospital District purposes, rates appearing in column "D" of Schedule 1 of this Bylaw;
 - (e) For purposes of the Regional District of Central Okanagan on the assessed value of land and improvements taxable for Regional District purposes, rates appearing in column "E" of Schedule 1 of this Bylaw;
 - (f) For purposes of the Regional District of Central Okanagan on the assessed value of land only for the Regional District of Central Okanagan Sterile Insect Release Program, rates appearing in column "F" of Schedule 1 of this Bylaw; and
 - (g) For Local Service Area purposes on the assessed value of land and improvements taxable for general municipal purposes, rates appearing in columns "A" and "B" of Schedule 2 of this Bylaw.
2. This bylaw may be cited as "Annual Tax Rates Bylaw, 2014 No. 10948".

Read a first, second and third time by the Municipal Council this 5th day of May, 2014.

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

SCHEDULE 1

CITY OF KELOWNA

GENERAL MUNICIPAL, DEBT, HOSPITAL AND REGIONAL DISTRICT
TAX RATES - 2014

PROPERTY CLASS	TAX RATES (DOLLAR OF TAX PER \$1,000 TAXABLE VALUE)						TAXABLE LAND ONLY
	A	B	C	D	E	F	
	GENERAL MUNICIPAL	DEBT	LIBRARY	REGIONAL HOSPITAL DISTRICT	REGIONAL DISTRICT	REGIONAL DISTRICT SIR	
01 Residential	3.7500	0.0870	0.1930	0.3610	0.3293	0.0463	0.0463
02 Utilities	18.8625	0.4378	0.9710	1.2636	1.1526	0.1620	0.1620
03 Supportive Housing	3.7500	0.0870	0.1930	0.3610	0.3293	0.0463	0.0463
04 Major Industrial	11.5902	0.2690	0.5966	1.2275	1.1197	0.1574	0.1574
05 Light Industrial	7.8303	0.1818	0.4031	1.2275	1.1197	0.1574	0.1574
06 Business/Other	7.8303	0.1818	0.4031	0.8845	0.8068	0.1134	0.1134
08 Recreation/Non-Profit	3.7500	0.0870	0.1930	0.3610	0.3293	0.0463	0.0463
09 Farm:							
a) Land	0.4657	0.0108	0.0235	0.3610	0.3293	0.0463	0.0463
b) Improvements	1.8574	0.0431	0.0956	0.0000	0.0000	0.0000	0.0000

SCHEDULE 2

CITY OF KELOWNA

2014 LOCAL SERVICE AREA TAX RATES

PROPERTY CLASS	A DOWNTOWN BUSINESS IMPROVEMENT AREA	B UPTOWN RUTLAND BUSINESS IMPROVEMENT AREA
1. RESIDENTIAL	0	0
2. UTILITY	0	0
4. INDUSTRIAL - MAJOR	0	0
5. INDUSTRIAL - LIGHT	1.5950	1.3476
6. BUSINESS	1.5950	1.3476
7. TREE FARM	0	0
8. SEASONAL	0	0
9. FARM a) LAND b) IMPROVEMENT	0 0	0 0

CITY OF KELOWNA

BYLAW NO. 10949

Development Cost Charge Reserve Fund Expenditure Bylaw, 2014

WHEREAS, there is an unappropriated balance in the Development Cost Charge Reserve Fund established under Bylaw No. 7112, which has most recently been replaced by Bylaw No. 9095, of Twenty-Four Million, Eighteen Thousand, Two Hundred and One Dollars (\$24,018,201.00) as at January 1st, 2014.

AND WHEREAS, it is deemed desirable to expend a portion of the monies set aside under said Bylaw No. 7112, which has most recently been replaced by Bylaw No. 9095, for the purpose of utility, road and land improvement and additions;

NOW THEREFORE, the Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. The sum Twenty-Four Million, Eighteen Thousand, Two Hundred and One Dollars (\$24,018,201.00) is hereby appropriated from the Development Cost Charge Reserve Fund to be expended in 2014 for the following purposes:

Land for Park Purposes	\$ 1,000,000.00
Road Construction	\$11,696,485.00
Water Mains, Pump Stations & Reservoir Construction	\$ 4,097,700.00
Treatment Plant Construction & Debt Repayment	\$ 5,802,434.00
Wastewater Trunks & Debt Repayment	\$ 1,421,582.00
	<u>\$24,018,201.00</u>

2. The expenditure to be carried out by the monies hereby appropriated shall be more particularly specified and authorized by resolution of Council.
3. Should any of the above remain unexpended after the expenditures hereby authorized have been made, the unexpended balance shall be returned to the credit of the Development Cost Charge Reserve Fund.
4. This bylaw may be cited as the "Development Cost Charge Reserve Fund Expenditure Bylaw, 2014, No. 10949".

Read a first, second and third time by the Municipal Council this 5th day of May, 2014.

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

CITY OF KELOWNA

BYLAW NO. 10951

Sale of City-Owned Land Reserve Fund Expenditure Bylaw, 2014

WHEREAS, there is an unappropriated balance in the Sale of City-Owned Land Reserve Fund of Ten Million, Two Hundred Eighty-Eight Thousand, Four Hundred and Fifty Two Dollars (\$10,288,452.00) as at January 1st, 2014.

AND WHEREAS, it is deemed desirable to expend a portion of the monies set aside under said Sale of City-Owned Land Reserve Fund for the purpose of land purchases and enhancements set out below;

NOW THEREFORE, the Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. The sum of Ten Million, Two Hundred Eighty-Eight Thousand, Four Hundred and Fifty Two Dollars (\$10,288,452.00) as at January 1st, 2014 is hereby appropriated from the Sale of City-Owned Land Reserve Fund to be expended in 2014 for the following purposes:

General Land Acquisition	\$ 5,613,915.00
Parks Land	\$ 2,865,264.00
Housing Opportunity	\$ 1,809,273.00
	<u>\$ 10,288,452.00</u>

2. The expenditure to be carried out by the monies hereby appropriated shall be more particularly specified and authorized by resolution of Council.
3. Should any of the above remain unexpended after the expenditures hereby authorized have been made, the unexpended balance shall be returned to the credit of the City-Owned Land Reserve Fund.
4. This bylaw may be cited as the "Sale of City-Owned Land Reserve Fund Expenditure Bylaw, 2014, No. 10951".

Read a first, second and third time by the Municipal Council this 5th day of May, 2014.

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk